



## **AGENDA**

**FOR COUNCIL ASSESSMENT PANEL MEETING TO BE HELD ON**

**4 DECEMBER 2018 AT 6:00 PM**

**IN THE COUNCIL CHAMBER, 12 JAMES STREET, SALISBURY**

### **MEMBERS**

Mr D Wallace (Presiding Member)  
Mr R Bateup  
Ms S Johnston  
Mr J Watson

### **REQUIRED STAFF**

General Manager City Development, Mr T Sutcliffe  
Manager Development Services, Mr C Zafiropoulos (Assessment Manager)  
Team Leader – Planning, Ms A Curtis  
Development Officer – Planning, Ms K Brown  
Development Officer – Planning, Mr C Carrey  
Planning Consultant, Mr M Atkinson

### **APOLOGIES**

### **LEAVE OF ABSENCE**

### **ENDORSED MINUTES FROM PREVIOUS MEETING**

Copy of the Endorsed Minutes of the Council Assessment Panel Meeting held on 25 September 2018.

### **DECLARATIONS OF CONFLICTS OF INTEREST**

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## REPORTS

### *Development Applications*

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**CLOSE****Please note:**

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**MINUTES OF COUNCIL ASSESSMENT PANEL MEETING HELD IN THE COUNCIL  
CHAMBER, 12 JAMES STREET, SALISBURY ON**

**25 SEPTEMBER 2018**

**MEMBERS PRESENT**

Ms L Caruso (Acting Presiding Member)  
Mr R Bateup  
Mr J Watson

**STAFF**

General Manager City Development, Mr T Sutcliffe  
Manager Development Services, Mr C Zafirooulos (Assessment Manager)  
Development Officer – Planning, Mr C Carrey  
Planning Consultant, Mr M Atkinson

The meeting commenced at 6.00 pm

The Acting Presiding Member welcomed the members, staff and the gallery to the meeting.

**APOLOGIES**

Apologies were received from Mr D Wallace (Presiding Member) and Ms S Johnston.

**DECLARATIONS OF CONFLICTS OF INTEREST**

Nil

## REPORTS

### *Development Applications*

#### 5.1.1 361/631/2018/2A

Residential Development Comprising the Construction of Two Two Storey Dwellings and Retention of Existing Single Storey Dwelling at 3 Elaine Avenue, Pooraka SA 5095 for Fadak Mohammadi

## REPRESENTORS

A J Schloithe and S Parker, were invited to speak but were not present at the meeting.

M E M Klawitter, spoke to their representation.

## APPLICANT

Mr Fadak Mohammadi to their application

Mr Watson moved and the Council Assessment Panel resolved that:

- A. The proposed development is not considered to be seriously at variance with the Salisbury Development Plan – Consolidated 15 December 2016.
- B. Pursuant to Section 33 of the *Development Act 1993*, Development Plan Consent is **GRANTED** to application number 361/631/2018/2A for the construction of two (2), two-storey dwellings and the retention of a single storey dwelling, with associated retaining walls, fencing and landscaping, at 3 Elaine Avenue, Pooraka, in accordance with the plans and details submitted with the application and subject to the following conditions:

### *Development Plan Consent Conditions*

1. The development shall be carried out in accordance with the details submitted with the application and the following stamped approved plans and documents, except where otherwise varied by the conditions herein:

Drawing No.	Plan Type	Date	Prepared By
DA00	Cover	15 June 2018	1980 Home
DA01	Site Plan	15 June 2018	1980 Home
DA02	Ground Floor Plan	15 June 2018	1980 Home
DA03	First Floor Plan	15 June 2018	1980 Home
DA04	Lower Roof Plan	15 June 2018	1980 Home
DA05	Upper Roof Plan	15 June 2018	1980 Home

DA06	Elevations	15 June 2018	1980 Home
DA07	Elevations	15 June 2018	1980 Home
DA08	Landscape Plan	15 June 2018	1980 Home
N/A	Civil Plan	N/A	Unknown

\* *The approved documents referred to above are subject to change permitted by minor variations permitted through the Building Rules Consent process.*

\* *Except where otherwise stated, the development shall be completed prior to occupation/commencement of use.*

*Reason: To ensure the proposal is established in accordance with the submitted plans.*

2. The external surfaces of the building shall:
  - a. be of new non-reflective materials; and
  - b. be finished in natural tones; and
  - c. be maintained in good condition at all times.

*Reason: To ensure the building is of a high standard of appearance and complements the site and surrounds.*

3. The designated landscaping areas shall be planted in accordance with the Landscape Plan, herein approved and shall be maintained (including the replacement of diseased or dying plants and the removal of weeds and pest plants) to the reasonable satisfaction of Council. All landscaping is to be completed prior to occupation of the dwellings.

*Reason: To ensure the subject land is landscaped so as to enhance the visual and environmental amenity of the locality.*

4. Stormwater systems shall be designed and constructed to cater for minor storm flows (Residential ARI =5yrs). The design of the stormwater system shall ensure that no stormwater is discharged onto any adjoining land. Surface stormwater is to be managed in a manner that ensures no ponding of water against buildings and structures, no creation of any insanitary condition, and no runoff into neighbouring property for the major storm event ARI = 100 years.

*Reason: To ensure flood protection of the buildings.*

5. All driveways and car parking areas shall be constructed with either brick paving, concrete or bitumen to a standard appropriate for the intended traffic volumes and vehicle types. Driveways and carparking areas shall be established prior to the occupation and maintained at all times to the satisfaction of Council.

*Reason: To ensure access and carparking is provided on the site in a manner that maintains and enhances the amenity of the locality.*

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Advisory Note:

That a temporary fence be installed along the common boundary with 5 Elaine Avenue, Pooraka, for the period of construction to maintain privacy and security to the adjoining dwelling.

**5.1.2**

**361/1002/2018/3B**

Supported care facility comprising 79 beds, 8 independent living units, offices, amenities, associated facilities and services, common areas, car parking, fencing and landscaping at 16-26 Parawae Road, Salisbury Plain, SA 5109 for St Basil's Homes (SA)

**REPRESENTORS**

A P Richardson and J A Richardson, spoke to their representation.

A L O'Toole and T J O'Toole, were invited to speak but were not present at the meeting.

P P Pham and P B T Vo, were invited to speak but were not present at the meeting.

T Q Pham and K O Tran, were invited to speak but were not present at the meeting.

M J Searle and M L Searle, spoke to their representation.

K L Carter, were invited to speak but were not present at the meeting.

**APPLICANT**

Carl Reeves – Director - Pruszinski PACT Architects

Greg Vincent – Director – Masterplan

Ben Wilson – Director – Cirqa Traffic Consultants

The Acting Presiding Member called for a mover.

Mr Bateup moved and Mr Watson seconded and the Council Assessment Panel resolved that:

- A. The proposed development is not considered to be seriously at variance with the Salisbury Council Development Plan – Consolidated 15 December 2016.



- B. Pursuant to Section 33 of the *Development Act 1993*, Development Plan Consent is **GRANTED** to application number 361/1002/2018/3B for Supported care facility comprising 79 beds, 8 independent living units, offices, amenities, associated facilities and services, common areas, car parking, fencing and landscaping in accordance with the plans and details submitted with the application and subject to the following reserved matters and conditions:

Reserved Matters:

The following matter/s shall be submitted for further assessment and approval by the Manager – Development Services, as delegate of the Council Assessment Panel, as Reserved Matters under Section 33(3) of the *Development Act 1993*:

1. An Environmental Site History Report prepared by a suitably qualified person which reports on the land use history and condition of the site. The report provided must be able to identify that the land is suitable for the proposed use with the overall risk being low.

*Note: The report should be prepared in accordance with Planning Advisory Note 20 and the EPAs Implementation of the National Environment Protection (Assessment of Site contamination) Measure 1999. If the site has been used for any activities listed in Appendix 1 of PAN 20 (or any other potentially contaminating activity) or the preliminary site assessment reveals that contamination is suspected or identified, then further site assessment and investigations will be required.*

2. A detailed Landscaping Plan which shall include:
  - a) Final locations for all landscaped areas, including designated areas for trees, shrubs and groundcovers
  - b) Species to be used, which should predominantly comprise native species
  - c) Maintenance methods including irrigation
3. A Construction Environmental Management Plan (CEMP) which shall include:
  - a) Hours of operation
  - b) Arrangement for management of stormwater, noise and dust
  - c) Measures to eliminate drag out from the site during wet weather events
  - d) A Soil Erosion and Drainage Management Plan

Development Plan Consent Conditions

1. The development shall be carried out in accordance with the details submitted with the application and the following stamped approved plans and documents, except where otherwise varied by the conditions herein:

Drawing No.	Plan Type	Date	Prepared By
17022-01-02	Site Plan - Ground	Date received	PACT

Issue 2		Stamped by Council 7 August 2018	
17022-01-03 Issue 1	Site Plan – Level 1	13.06.2018	PACT
17022-01-04 Issue 1	Site Plan – Roof	13.06.2018	PACT
17022-01-03 Issue 1	Site Plan – Level 1	13.06.2018	PACT
17022-01-11 Issue 1	Materials	13.06.2018	PACT
17022-02-01 Issue 1	Building 1 – Ground Plan	13.06.2018	PACT
17022-02-02 Issue 1	Building 1 – Level 1 Plan	13.06.2018	PACT
17022-02-03 Issue 1	Building 1 – Roof Plan	13.06.2018	PACT
17022-02-04 Issue 1	Building 1 – Elevation Sheet 1	13.06.2018	PACT
17022-02-05 Issue 1	Building 1 – Elevation Sheet 2	13.06.2018	PACT
17022-02-07 Issue 1	Building 2 – Plans	13.06.2018	PACT
17022-02-08 Issue 1	Building 2 - Elevations	13.06.2018	PACT
17022-02-010 Issue 1	Building 3 – Plans	13.06.2018	PACT
17022-02-11 Issue 1	Building 3 – Roof Plans	13.06.2018	PACT
17022-02-12 Issue 1	Building 3 - Elevations	13.06.2018	PACT
17022-02-14 Issue 1	Building 4 - Plans	13.06.2018	PACT
17022-02-15 Issue 1	Building 4 - Elevations	13.06.2018	PACT
17022-02-17 Issue 3	Building 5 - Plans	13.06.2018 (Date Received Stamped by Council 6 September 2018)	PACT
17022-02-18 Issue 1	Building 5 – Roof Plans	13.06.2018	PACT
17022-02-19 Issue 3	Building 5 - Elevations	13.06.2018 (Date Received Stamped by Council 6 September 2018)	PACT
17022-02-21 Issue 1	Building 6 - Plans	13.06.2018	PACT

17022-02-22 Issue 1	Building 6 – Roof Plan	13.06.2018	PACT	Minutes of the Council Assessment Panel Meeting 25/09/2018
17022-02-23 Issue 1	Building 6 - Elevations	13.06.2018	PACT	
17022-02-25 Issue 1	Building 7 - Plans	13.06.2018	PACT	
17022-02-26 Issue 1	Building 7 - Elevations	13.06.2018	PACT	
17022-02-28 Issue 1	Building 8 - Plans	13.06.2018	PACT	
17022-02-29 Issue 1	Building 8 – Roof Plans	13.06.2018	PACT	
17022-02-30 Issue 1	Building 8 - Elevations	13.06.2018	PACT	
17022-01-12 Issue 4	Overlooking	10.09.2018 (Date Received Stamped by Council 10 September 2018)	PACT	
DT180304.01 Issue 1	Site Plan Part 1	14.06.2018 (Date Received Stamped by Council 6 September 2018)	Structural System	
DT180304.02 Issue 0	Site Plan Part 2	14.06.2018	Structural System	
DT180304.03 Issue 0	Details	14.06.2018	Structural System	
DT180304	Stormwater Management Report	14.06.2018	Structural Systems	
SP-01	Combined Services – Ground Level and Site Plan	May 2018	BESTEC	
SP-02	Combined Services – First Level	May 2018	BESTEC	
SP-03	Combined Services – Roof Level	May 2018	BESTEC	
55968/0/0	Electrical Services	31 May 2018	BESTEC	
N/A	Planning Report	June 2018	Masterplan	
17181	Traffic and Parking Report	13 June 2018	CIRQA	
N/A	Waste Management Statement	16 May 2018	PACT	
N/A	Response to Representations	Date Received Stamped by Council 6 August 2018	Masterplan	
17181/BNW	Response to Representations - CIRQA	Date Received Stamped by Council 7 August	CIRQA	

		2018	
N/A	Response to request for additional information	(Date Received Stamped by Council 6 September 2018	Masterplan

- *All plans and details approved by Council under Reserved Matters 1, 2 and 3 form part of this consent and are in addition to those plans and details listed in the table above.*
- *The approved documents referred to above may be subject to change by minor variations permitted through the Building Rules Consent process.*
- *Except where otherwise stated, the development shall be completed prior to the commencement of use. This extends to documents approved as Reserved Matters.*

*Reason: To ensure the proposal is established in accordance with the submitted plans.*

2. The Developer shall employ measures to eliminate dust emission from the site during construction period so as not to cause nuisance to adjoining residents.

*Reason: To preserve the amenity of the locality during construction work.*

3. Site work, demolition work and building work shall be carried out only between the hours of 7.00am to 7.00pm Monday to Saturday and 9.00am to 5.00pm Sunday.

*Reason: To limit the effect of construction on the amenity of the locality.*

4. Noise measured at the nearest residential property boundary shall remain within the requirements of the Environment Protection Authority (EPA) guidelines for development within a residential area.

*Reason: To ensure that noise does not cause nuisance to adjoining residential occupiers or owners.*

5. No materials, goods or containers shall be stored in the designated vehicle parking area or driveways.

*Reason: To ensure the vehicle parking areas are always available for the purpose they are designed. Further that the site be maintained in a clean and tidy state.*

6. The designated landscaping areas, approved by Council under Reserved Matter 1 shall be established within three (3) months of the approved use commencing. All landscaping shall be maintained (including the replacement of diseased or dying plants and the removal of weeds and pest plants) to the reasonable satisfaction of Council.

*Reason: To ensure the subject land is landscaped so as to enhance the visual and environmental amenity of the locality.*

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7. Outside lighting shall be installed in accordance with the Lighting Plans approved by Council under Development Plan Condition 1 and shall be directed and shaded to prevent light overspill and/or nuisance to adjacent occupiers or distraction to drivers on adjacent public roads.

*Reason: To ensure that floodlighting does not cause nuisance or danger to adjoining occupiers or road users thereby reducing the amenity of the locality and/or making road use unsafe.*

8. All waste and other rubbish shall be contained and stored pending removal in covered containers, screened from public view and kept within the designated waste storage areas identified on 'Site Plan – Ground' approved by Council under Development Plan Condition 1.

*Reason: To maintain the amenity of the locality.*

9. Waste collection shall only occur during the following periods:
- 9.00am to 7.00pm on any Sunday or public holiday; or
  - 7.00am to 7.00pm on any other day

*Reason: To manage noise and security at the boundary interface and to improve the amenity of the locality.*

10. All loading and unloading of vehicles and manoeuvring of vehicles in connection with the now approved land use shall be carried out entirely within the subject land.

*Reason: To ensure that vehicles associated with the land use does not cause disruption or danger to vehicles on adjoining public roads.*

11. All mechanical services to the building and in conjunction with the proposed use shall be designed, installed and operated in such a manner that any person or persons working within or adjacent to the site should not be subjected to any nuisance or inconvenience from noise or fumes.

*Reason: To limit the effect of the mechanical services for activities on the subject land within the site, thereby maintaining the amenity of the locality.*

Conditions recommended by Development Engineering

12. The carparking layout including car park spaces and aisle widths are to be designed and constructed to comply with AS 2890.1 – Off-street Parking Part 1 and Austroads "Guide to Traffic Engineering Practice Part 11 – Parking" and AS 2890.2 – Facilities for Commercial Vehicles.

*Reason: To ensure that the development complies with Standards and Best Engineering Practice.*

13. All driveways and car parking areas shall be constructed with either brick paving, concrete or bitumen to a standard appropriate for the intended traffic volumes and vehicle types. Individual carparking bays shall be clearly linemarked. Driveways and carparking areas shall be established prior to the approved use commencing and maintained at all times to the satisfaction of Council.

*Reason: To ensure access and carparking is provided on the site in a manner that maintains and enhances the amenity of the locality.*

14. All construction is to be completed to the reasonable satisfaction of Council. Damage sustained to any new or existing infrastructure during the course of the works is to be rectified to Council satisfaction prior to the completion of development works.

*Reason: To ensure that the development complies with Standards, best Engineering Practice and Council Policy.*

15. The site stormwater drainage system shall be designed to control the quantity and quality of stormwater discharged from the site, to minimise flooding and to prevent adverse impacts on neighbouring properties and downstream drainage systems. In particular:

- A flow path (north to south) is to be maintained within the site to enable any overland flow to pass through the site.

*Reason: To ensure appropriate flood and water management across the site*

16. All existing crossovers/inverts made redundant by this development shall be reinstated to kerb, prior to occupation of the dwelling/commencement of use, in accordance with Council's kerb design standard, to the satisfaction of Council.

*Reason: To maximise on-street parking and appropriately manage stormwater within the street water table.*

Advice Notes

1. Food for sale available on the premises, including from the restaurant and supermarket must be constructed in compliance with the *Food Act 2001* and Food Safety Standard 3.2.3.
2. If a high risk manufactured water system (warm water system) is installed within any of the buildings, ensure that it is installed, managed and maintained in accordance with the *Public Health (Legionella) Regulations*.
3. The hydrotherapy pool must be installed, managed and maintained in accordance with the *Public Health (General) Regulations*.

The motion was carried.

**5.1.3 361/1208/2017/NC**

Change of Use from a Garage to a Shop with Associated Cool Room, Canopy and Car Parking (Non-Complying) at 1333-1339 Port Wakefield Road, Waterloo Corner for Angelyn Building Designs

Mr Watson moved and the Council Assessment Panel resolved that:

- A. The proposed development is not considered to be seriously at variance with the Salisbury (City) Development Plan – consolidated 15 December 2016.
- B. Pursuant to Section 33 of the *Development Act 1993*, Development Plan Consent is **GRANTED** to application number 361/1208/2017/NC for a Change of Use from a Garage to a Shop with Associated Cool Room, Canopy and Car Parking (Non-Complying) in accordance with the plans and details submitted with the application, subject to the concurrence of the State Commission Assessment Panel (SCAP) and the following reserve matter and conditions:

Reserve Matter

- 1. A stormwater management plan shall be provided for review by Development Engineering demonstrating how water quality will be addressed prior to discharge. No detention is sought in this instance in order to allow stormwater to get away from the site ahead of headwaters from the north.

Discharge to the swale on Pt Wakefield Road via a water quality improvement process (may be as simple as an RSM Pillow in an existing sump) and an appropriate outlet would be acceptable.

Development Plan Consent Conditions

- 1. The development shall be carried out in accordance with the details submitted with the application and the following stamped approved plans and documents, except where otherwise varied by the conditions herein:

Drawing No.	Plan Type	Date	Prepared By
1 of 3	Site Plan	Received by Council dated 4 September 2018	Angelyn Building Designs
2 of 3	Floor Plan	Received by Council dated 4 September 2018	Angelyn Building Designs
3 of 3	Elevations	Received by Council dated 4 September 2018	Angelyn Building Designs

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*Reason: To ensure the proposal is established in accordance with the submitted plans.*

2. All waste and other rubbish shall be contained and stored pending removal in covered containers and waste containers must not be located within designated car parks or manoeuvring areas.

*Reason: To maintain the amenity of the locality.*

3. No materials, goods or containers shall be stored outside the building at any time.

*Reason: To ensure the car parking and emergency access areas are always available for the purpose they are designed. Further that the site be maintained in a clean and tidy state.*

DPTI Conditions

4. The access points to Port Wakefield Road shall be modified in accordance with the Angelyn Building Designs Site Plan, Job No. 21/16, dated 13 June 2018.
5. All vehicles shall enter and exit the site in a forward direction.
6. The largest vehicle permitted to access the site shall be restricted to 12.5-metre Heavy Rigid Vehicle (HRV) as per AS2890.2-2002.
7. The internal manoeuvring areas shall be designed in accordance with AS 2890.2-2002 and all passenger parking areas be designed in accordance with AS/NZS 2890.1:2004 and 2890.6:2009.
8. All electronic signage shall be located entirely within private property.
9. Longitudinal drainage shall be maintained along Port Wakefield Road in order to minimise the potential for flooding on the road. All costs associated with any upgrade/alterations shall be borne by the applicant.
10. The applicant shall ensure that all stormwater generated by the proposal is appropriately collected and disposed of without entering or jeopardising the safety of the adjacent arterial road network.

Advice Notes

1. This is not a building consent, and a satisfactory application for Building Rules Consent must be submitted and approved before the Council can issue a Development Approval.
2. Advertisements and advertising displays are not included in the consent granted. It will be necessary to make a fresh and separate application for any future proposed advertising signage.



3. Except where otherwise varied by this Consent, the conditions imposed herein shall be in addition to conditions that apply to the subject property from previous approvals that remain active.

DPTI Notes

1. The Metropolitan Adelaide Road Widening Plan shows a possible requirement for a strip of land up to 15 metres in width from the Port Wakefield Road frontage of this site for future road purposes. Due to the construction of the Northern Connector this requirement is subject to review and whilst the exact requirements have not been determined at this point in time, it is considered likely that the current 15-metre strip requirement will be significantly reduced.
2. The consent of the Commissioner of Highways under the Metropolitan Adelaide Road Widening Plan Act 1972 is required to all building works on or within 6 metres of the possible requirements. Consent can be anticipated.

## **OTHER BUSINESS**

### **5.2.1 Status of Current Appeal Matters and Deferred Items**

361/1144/2016 - United Sikhs, 701-709 Port Wakefield Road, Globe Derby Park

The ERD Court has adjourned matter until 6 December 2018 at the request of the United Sikhs of SA. The appellant has advised of their intention to proceed with the appeal but wish to initially put an amended plan to Panel and Mr Tran (Joiner to the appeal). The appellant will aim to have an amended plan prepared for the Panel's consideration at the meeting to be held in November 2018.

### **5.2.2 Policy Issues is Arising from Consideration of Development Applications**

Nil

### **5.2.3 Future Meetings & Agenda Items**

Next meeting scheduled for Tuesday 23 October 2018.

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**ADOPTION OF MINUTES**

Mr Watson moved, and the Council Assessment Panel resolved that the Minutes of the Council Assessment Panel Meeting be taken and read as confirmed.

The meeting closed at 7.04 pm.

**ACTING PRESIDING MEMBER:** Ms L Caruso

**DATE:** 25 September 2018  
(refer to email approving minutes registered in Dataworks Document Number 5047725)

<b>ITEM</b>	5.1.1
	<b>COUNCIL ASSESSMENT PANEL</b>
<b>DATE</b>	4 December 2018
<b>APPLICATION NO.</b>	361/1720/2018/3B
<b>APPLICANT</b>	AO Salisbury Pty Ltd
<b>PROPOSAL</b>	Child care centre comprising new building, signage, boundary acoustic fencing and associated car parking
<b>LOCATION</b>	158-160 and 162 Salisbury Highway, Salisbury
<b>CERTIFICATE OF TITLE</b>	5113/67 & 5400/82
<b>AUTHOR</b>	Aaron Curtis, Team Leader - Planning, City Development

### 1. DEVELOPMENT APPLICATION DETAILS

<b>Zone/Policy Area</b>	Residential Zone
<b>Application Type</b>	On-Merit
<b>Public Notification Category</b>	3
<b>Public Notification</b>	Representations received: 22 Representations to be heard: 9
<b>Referrals - Statutory</b>	Commissioner of Highways
<b>Referrals – Internal</b>	Development Engineering Landscape Design
<b>Development Plan Version</b>	Salisbury (City) Development Plan Consolidated 15 <sup>th</sup> December 2016.
<b>Assessing Officer</b>	Aaron Curtis, Team Leader - Planning
<b>Recommendation</b>	Grant Development Plan Consent subject to Reserved Matters and conditions
<b>Meeting Date</b>	4 <sup>th</sup> December 2018

### 2. REPORT CONTENTS

Assessment Report

Attachment 1:	Application Documentation
Attachment 2:	Category 3 Notice and Copy of Representations
Attachment 3:	Commission of Highways Referral Response
Attachment 4:	Internal Referral Responses
Attachment 5:	Relevant Development Plan Provisions

### **3. EXECUTIVE SUMMARY**

The applicant seeks Development Plan Consent for a child care facility comprising a new building, signage, boundary acoustic fencing and associated car parking.

This report provides a detailed assessment of the application against the relevant provisions of the Salisbury Development Plan. The assessment found that the proposal achieves a high degree of compliance with the Development Plan as follows:

- a) The proposed form of development is envisaged within the Residential Zone and the proposal is of a 'small-scale';
- b) The site is considered to be appropriately located for this facility, noting that child-care centres are commonly located on main roads;
- c) The facility will not have an unreasonable impact upon the residential amenity of existing dwellings having regard to the operating hours and projected noise levels;
- d) The building is designed to a high standard and will be of a footprint, scale and setback that is compatible with the locality;
- e) Acoustic fencing necessary to achieve the objective noise criteria will not have an unreasonable degree of overshadow impact or sense of enclosure for neighbours;
- f) Vehicular access is designed to permit simultaneous forward entry and exit for vehicles and is to the satisfaction of the Commissioner of Highways;
- g) Sufficient on-site car parking is provided for the facility;
- h) Waste management arrangements are accommodated;
- i) Stormwater management arrangements are acceptable, subject to final design;
- j) A comprehensive landscape concept plan is proposed to be implemented, prepared by a landscape architect;
- k) Signage is proposed to be employed to a high standard and which complements the overall form of development.

Some minor departures from the Development Plan policy are identified in respect to the proposed height of the signs, the depth of landscaping to the street frontage and the presence of solid fencing to portion of the Evan Avenue frontage. These elements are shortcomings in the design but are considered to be acceptable having regard to the overall positive elements of the design.

Given the above, it is recommended that Development Plan Consent be granted, subject to Reserved Matters and conditions.

### **4. SUBJECT SITE**

The subject site is located at 158-160 and 162 Salisbury Highway, Salisbury and comprises two allotments; Lot 124, Deposited Plan 22485, Certificate of Title Volume 5113 Folio 67 and Lot 122, Deposited Plan 22483, Certificate of Title Volume 5400 Folio 82.

Both sites are occupied by single storey detached dwellings. The site at 158-160 Salisbury Highway is devoid of any outbuildings whereas the site at 162 Salisbury Highway contains a number of attached structures and detached outbuildings.

The existing buildings are approved for demolition.

## ITEM 5.1.1

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The site is generally devoid of any vegetation. There are several mature street trees in Evan Avenue which provide some screening of the site from this elevation. The Salisbury Highway verge is devoid of vegetation.

There are no easements, encumbrances or land management agreements registered on the Certificate of Title. The site is level.

Site photos are provided below.

**Photo 1.**

*Looking east from  
Salisbury Highway  
toward existing house  
at number 158-160*



**Photo 2.**

*Looking south from  
Salisbury Highway  
toward existing house  
at number 162*



**Photo 3.**

*Looking south-west from Evan Avenue toward rear yard of 158-160*



**Photo 4.**

*Looking south-west from Evan Avenue toward existing house at 158-160. This is the location for the new driveway access*



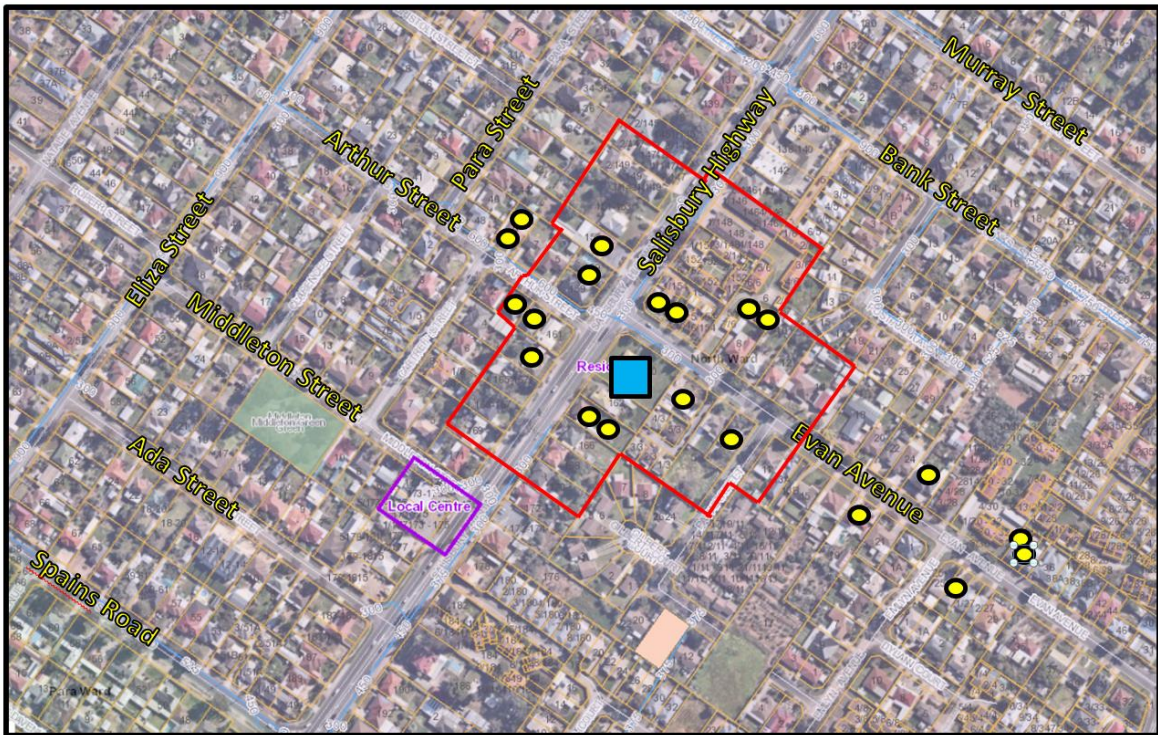
**5. LOCALITY**




The locality is characterised exclusively of residential uses. Residential development is predominantly in the form of single-storey detached dwellings on individual allotments, however, there are a number of examples of single storey units (residential flat buildings and group dwellings), for example, the sites on the opposite side of Evan Avenue.

The housing stock comprises predominantly dwellings from the era of 1960's with the dwelling at 158-160 Salisbury Highway (the subject land) dating from the 1920's – 1930's. The original land division pattern prevails, although there are a few examples of residential infill. Front fences are common but are generally low in height to allow views of front yard landscaping which is generally established.

Locality and contextual plans are provided below:

*Locality Plan – Aerial*



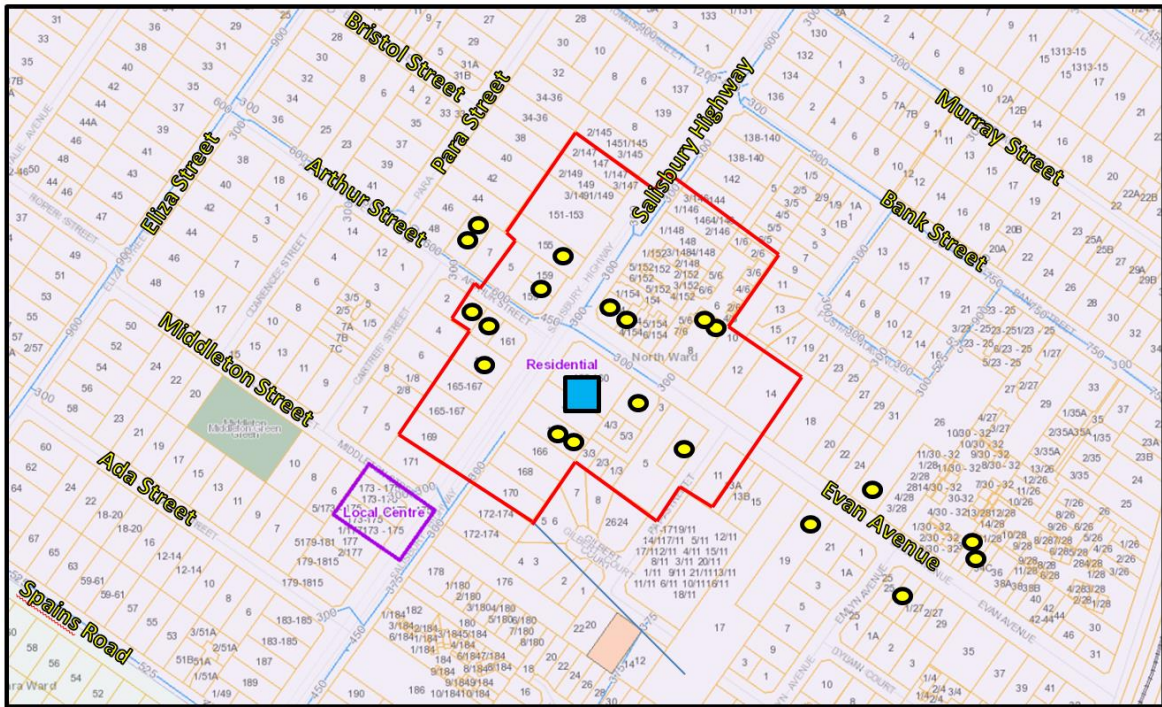
	Subject site
	Locality boundary
	Representor




*Aerial View – Enlarged*



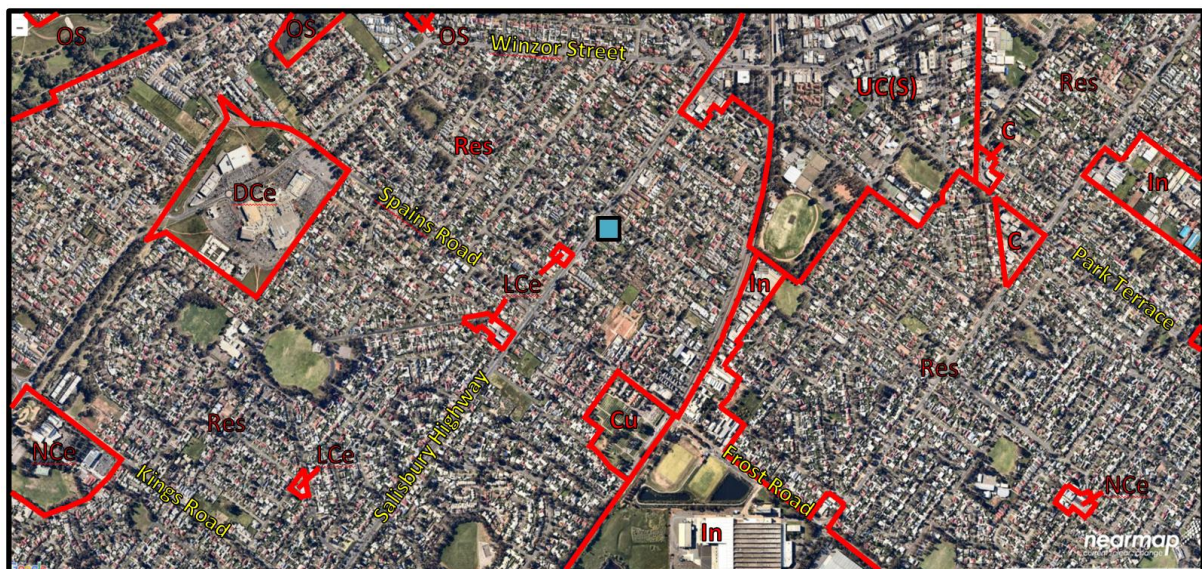
Item 5.1.1



Locality Plan – Cadastre



	Subject site
	Locality boundary
	Representor

Contextual Plan



	Subject site
	Zone Boundary



## 6. DESCRIPTION OF THE PROPOSED DEVELOPMENT

The applicant seeks Development Plan Consent for a child care facility comprising new building, signage, boundary acoustic fencing and associated car parking.

The development includes the following key elements:

- Purpose built single storey child care centre of 719m<sup>2</sup> with ancillary outdoor play areas (combined area of 823m<sup>2</sup>), shade sails and storage shed of 20m<sup>2</sup>;
- 1 x wall mounted advertisement (6m wide x 2m high);
- 2 x freestanding advertisements (1.5m wide x 4m high);
- Sealed car park having capacity for 29 spaces including 1 disabled car park;
- Enclosed bin storage area;
- Boundary acoustic fencing of variable height between 2.1m and 3m;
- Associated landscaping comprising mix of shade trees, shrubs and ground covers.

The facility will accommodate children ranging from 0 to 6 years old and have a maximum capacity of 114 children. Up to 16 staff members will be present at any one time. The facility will be open from 6:30am to 6:30pm Monday to Friday.

A copy of the application documentation is contained in Attachment 1.

## 7. CLASSIFICATION

The subject site is located in the Residential Zone, as identified within the Council's Development Plan (Consolidated 15<sup>th</sup> December 2016). In this Zone, a child-care centre is not listed as a complying or non-complying form of development and, as such, the application must be assessed as a merit form of development.

## 8. PUBLIC NOTIFICATION

The proposed development is neither listed as a Category 1 or 2 form of development within the Residential Zone or under Schedule 9 of the *Development Regulations 2008*.

Accordingly, the application must be advertised as a Category 3 form of development under Section 38 of the *Development Act 1993*.

Public notification took place between 11<sup>th</sup> and 24<sup>th</sup> October 2018. The Council received 22 representations during the notification period. Eighteen (18) of the representors were in opposition and four were in support of the proposal. The representors are listed in the table below.

Representations Received		
	<i>Representations received</i>	<i>Wish to be heard</i>
1	R Tedesco 26 Evan Avenue SALISBURY SA 5108	Yes
2	G Prince 19 Evan Avenue SALISBURY SA 5108	Yes

3	L Thomas & R Antony 16 Thorngate Drive PARALOWIE SA 5108	No
4	C Chenoweth 7 EVAN AVENUE SALISBURY SA 5108	No
5	J & S Readings 163 Salisbury Highway SALISBURY SA 5108	Yes
6	G & G Ciccone 15 Theta Street SALISBURY SA 5108	No
7	A Sargent & S Collins Unit 8 6 Evan Avenue SALISBURY SA 5108	No
8	A Lennox Unit 7 6 Evan Avenue SALISBURY SA 5108	No
9	K Woolger 164 Salisbury Highway SALISBURY SA 5108	Yes
10	N & A Bivone 159 Salisbury Highway SALISBURY SA 5108	Yes
11	M Banks & T Williams 6 Arthur Street SALISBURY SA 5108	Yes
12	L Ghimire 1 Evan Avenue SALISBURY SA 5108	Yes
13	B & V Robbie 25A Evan Avenue SALISBURY SA 5108	No
14	R & K Caruso 155 Salisbury Highway SALISBURY SA 5108	No
15	S Humpherson 34B Evan Avenue SALISBURY SA 5108	No
16	D Luppino 5 Arthur Street SALISBURY SA 5108	No
17	C Luppino 9 Arthur Street SALISBURY SA 5108	No
18	N Karandrikas 4 Arthur Street SALISBURY SA 5108	No

19	G Bivone 164 Salisbury Highway SALISBURY SA 5108	Yes
20	R Tinkler Unit 3 154 Salisbury Highway SALISBURY SA 5108	Yes
21	S Kernot 34A Evan Avenue SALISBURY SA 5108	No
22	H Vuong Unit 2 154 Salisbury Highway SALISBURY SA 5108	No

A copy of the Category 3 public notice and submissions received are contained in Attachment 2. The applicant's response is contained in Attachment 1. The contents of the representations and the applicant's response (prepared by URPS) are summarised in the table below:

<b>Summary of Representations</b>	
<i>Issues raised</i>	<i>Applicant's response (prepared by URPS)</i>
<b>Land Use</b>	
<ul style="list-style-type: none"> <li>- An arterial road is not a good location for a child-care centre.</li> <li>- The development will make good use of the site which has been empty for a long time.</li> <li>- This is a residential area, not a commercial or business zone.</li> </ul>	<ul style="list-style-type: none"> <li>- The proposed land use is specifically envisaged in the Residential Zone.</li> <li>- The facility will serve the local community, offering child care services to local working families.</li> <li>- Many child care centres are located on main roads.</li> </ul>
<b>Demand</b>	
<ul style="list-style-type: none"> <li>- There is no demand for another child care centre.</li> <li>- There are already existing child care centres in close proximity to this site.</li> </ul>	<ul style="list-style-type: none"> <li>- The Development Plan does not place onus on the applicant to demonstrate demand for a child-care centre.</li> <li>- The applicant has undertaken research to establish need and viability for the proposed facility at the site.</li> <li>- The proposal is not expected to undermine the operation of any existing child care facilities.</li> </ul>
<b>Design and Appearance</b>	
<ul style="list-style-type: none"> <li>- The development will make our street ugly and is an eyesore.</li> <li>- The building looks like a warehouse.</li> <li>- The outdoor play area should be shifted away from Evan Avenue to reduce conflict with neighbouring dog noise.</li> <li>- The development is not designed to "fit" with the character of the area.</li> </ul>	<ul style="list-style-type: none"> <li>- The proposal is designed by a leading and award winning architect and is designed to be cohesive and attractive.</li> <li>- The development incorporates a number of design features that satisfy the desired character statement for the zone and general design and appearance provisions.</li> </ul>

<i>Noise</i>	
<ul style="list-style-type: none"> <li>- <i>The proposed fencing is not adequate to mitigate noise. The fencing should be insulated to mitigate the high levels of noise from the play area.</i></li> <li>- <i>The development will increase noise activity into our lounge and bedrooms from children's play and vehicle noise.</i></li> <li>- <i>The development will disturb our sleep as we sleep during the day due to night work shift. My neighbours are elderly and would not appreciate the noise.</i></li> <li>- <i>There will be excessive noise during demolition and construction.</i></li> <li>- <i>There will be noise from air conditioners and generators.</i></li> </ul>	<ul style="list-style-type: none"> <li>- <i>The applicant has engaged an acoustic consultant to undertake an environmental noise assessment.</i></li> <li>- <i>Outdoor play areas will not be used prior to 7am.</i></li> <li>- <i>Predicted noise levels have been assessed against the recommendations under the "Guidelines for Community Noise", published by the World Health Organisation.</i></li> <li>- <i>A number of recommendations have been submitted by the acoustic consultant and which are to be adopted.</i></li> </ul>
<i>Safety</i>	
<ul style="list-style-type: none"> <li>- <i>Parents and children will cross one side of Salisbury Highway to the other without a formalised crossing in place.</i></li> <li>- <i>The development fronts a busy arterial road. It will be dangerous for children.</i></li> <li>- <i>The entry/exit gates are only single width and not designed to prevent children from entering the road.</i></li> </ul>	<ul style="list-style-type: none"> <li>- <i>The large proportion of parents are expected to access the facility by car.</i></li> <li>- <i>Parking in Salisbury Highway is prohibited. The facility provides sufficient on-site car parking, such that parents should not need to cross Salisbury Highway by foot.</i></li> <li>- <i>A pedestrian crossing is located approximately 30m south of the site.</i></li> <li>- <i>Concerns regarding children's safety is a matter principally for management of the facility.</i></li> <li>- <i>Children will not be permitted to leave the premises without a parent or guardian.</i></li> <li>- <i>A front door will be electronically controlled and not able to be opened by a child.</i></li> <li>- <i>All outdoor play areas will be fenced to prevent the escape of children.</i></li> </ul>
<i>Car Parking</i>	
<ul style="list-style-type: none"> <li>- <i>We will lose car parking in our street (Evan Avenue and Arthur Street).</i></li> <li>- <i>Insufficient parking is provided. A large child care facility needs ample car parking for staff and parents.</i></li> <li>- <i>We are concerned that parents will use our private car park to park their cars.</i></li> <li>- <i>There are several unit blocks in this area which rely on the 'on-street' parking.</i></li> </ul>	<ul style="list-style-type: none"> <li>- <i>The development provides sufficient on-site car parking in accordance with Table Sal/2.</i></li> <li>- <i>The development will retain approximately 44m of Evan Avenue frontage available for on-street parking for between 7 and 8 vehicles.</i></li> </ul>

<i>Traffic</i>	
<ul style="list-style-type: none"> <li>- <i>There will be an increase in traffic in Evan Avenue and Salisbury Highway.</i></li> <li>- <i>“No parking” signage or yellow lines should be implemented in Evan Avenue.</i></li> <li>- <i>Slip lanes and signage should be installed.</i></li> <li>- <i>Parents will park on both sides of the street, thereby increasing risk of collision with vehicles turning into Evan Avenue.</i></li> <li>- <i>The development will disrupt traffic movements in Salisbury Highway due to increased turning movements in and out of the site.</i></li> <li>- <i>Access in and out of our property will be affected.</i></li> <li>- <i>There is limited vision of traffic along Salisbury Highway which will be exacerbated by the development.</i></li> <li>- <i>There will be an increased number of accidents as a result of the development.</i></li> <li>- <i>The Evan Avenue junction is already used readily for U-turns.</i></li> </ul>	<ul style="list-style-type: none"> <li>- <i>The Evan Avenue access is 18m from the junction with Salisbury Highway, thus minimising the length of Evan Avenue in which vehicles must travel.</i></li> <li>- <i>The majority of vehicular movements will be accommodated in via Evan Avenue and out via Salisbury Highway.</i></li> <li>- <i>The majority of vehicular movements are expected during a short peak period.</i></li> <li>- <i>GTA Consultants have concluded that traffic generated by the proposed development will not compromise the safety or function of the road network.</i></li> </ul>
<i>Economic Development</i>	
<ul style="list-style-type: none"> <li>- <i>The facility will bring more employment and development to Salisbury.</i></li> <li>- <i>The facility will be good for the area.</i></li> </ul>	<ul style="list-style-type: none"> <li>- <i>The proposal will employ up to 16 staff members at any one time while also creating employment during construction. This will have a direct benefit to the local economy.</i></li> </ul>
<i>Loss of Property Values</i>	
<ul style="list-style-type: none"> <li>- <i>The development will result in a loss of property values.</i></li> </ul>	<ul style="list-style-type: none"> <li>- <i>This is not a relevant matter for assessment under the Development Plan.</i></li> </ul>

## 9. REFERRALS - STATUTORY

The application was subject to referred to the Commissioner of Highways in accordance with Schedule 8(3) of the *Development Regulations 2008* on the basis that the development will create a new access to Salisbury Highway, a Secondary Arterial Road under the Salisbury Development Plan.

The Commissioner of Highways have provided a referral response, contained in Attachment 3. The Commissioner of Highways advised it does not object in-principle to the proposed development and has recommended that conditions be attached to the approval. Some of the recommended conditions have been adopted and are contained in the recommendation.

## 10. REFERRALS - INTERNAL

Internal comments were received from the Development Engineering and Landscape Design teams. The advice is that the driveway access, car parking and stormwater system proposed are acceptable and comply with Council requirements, subject to final design detail. The landscaping is also acceptable, subject to some minor changes to landscaping species selections. A copy of the internal referral responses is contained in Attachment 4.

## 11. ASSESSMENT

Pursuant to Section 35(2) of the *Development Act 1993*, it is recommended that the Panel determine that the proposed development is not seriously at variance with the Salisbury (City) Development Plan, Consolidated 15<sup>th</sup> December 2016. Small scale child care facilities are anticipated within residential areas, subject to being appropriately sited and managed to ensure there are no unreasonable impacts on adjacent or nearby residents.

### Assessment

Detailed assessment of the application has taken place against the relevant provisions of the Salisbury Development Plan and is described below under headings.

An extract of the relevant Development Plan, Consolidated 15<sup>th</sup> December 2016, is contained in Attachment 5. The relevant provisions are also highlighted in the Attachment.

### Zoning and Land Use

Principle of Development Control (PDC) 1 of the Residential Zone envisages small scale non-residential uses. PDC 1 states (in part):

*1 The following forms of development are envisaged in the zone:*

- *Small scale non-residential uses that serve the local community, for example:*
  - *Child-care facility;*
  - *Health and welfare service;*
  - *Open space;*
  - *Primary or secondary school. (my underlining)*

Principle of Development Control 4 further states that:

*4 Non-residential development such as shops, schools and consulting rooms should be of a nature and scale that:*

- (a) Serves the local community;*
- (b) Is consistent with the character of the locality;*
- (c) Does not detrimentally impact on the amenity of nearby residents;*
- (d) Will not undermine the efficient operation of nearby centres.*

Child-care facilities are clearly envisaged within the Zone but on the provision that they are 'small-scale' and 'serve the local community'. Principle of Development Control 4 further reinforces what is anticipated in terms of non-residential development.

The Development Plan does not define what is meant by ‘*small-scale*’. The proposed child-care centre is not insignificant in terms of its planned capacity of children, however, the facility when viewed as a whole is considered to be of a ‘*small-scale*’, consistent with Principle of Development Control 1, given that:

- The scale and operations of the facility is consistent with the character of the locality;
- The location is considered to be suitable for the facility in terms of its accessibility and design which limits vehicular movements on local streets;
- The facility will not detrimentally impact upon the amenity of nearby residents;

Several of the representors expressed concern that there was a lack of demand for the facility and that there are ample facilities available within the broader area. The applicant has confirmed they have undertaken considerable research to establish need and viability for the proposed facility. However, it should be noted that commercial viability and demand are not relevant matters for assessment under the Development Plan.

Several of the representors also expressed concern that the facility was poorly located in terms of its frontage to a main road, which represents a safety risk and pollution risk for children. The applicant has advised that child-care facilities are commonly located on main roads. It is accepted that child-care facilities are commonly located on main roads.

In summary, the proposed land use is envisaged within the zone and the scale of the proposal is considered to be of a “*small-scale*” as envisaged within the zone. The external impacts of the development upon the locality are further considered under headings below.

#### Interface Between Land Uses

Under the General Section, “*Interface between land uses*” module, Objective 1 and Principle of Development Control 7 state that:

- 1 Development located and designed to minimise adverse impact and conflict between land uses.*
- 7 Development that emits noise (other than music noise) should include noise attenuation measures that achieve the relevant Environment Protection (Noise) Policy criteria when assessed at the nearest existing noise sensitive premises.*

The proposed facility will operate between the hours of 6:30am to 6:30pm, Monday to Friday. The children’s play areas will not be able to be used until 7am. The proposed operating hours are considered to be appropriate for this locality, which is subject to patterns of peak traffic movements during the morning and evening in Salisbury Highway.

The applicant has submitted an environmental noise assessment, prepared by Sonus. The report has considered noise at existing residences in the vicinity, from children playing in outdoor areas, car park activity and mechanical plant operation.

The consultant has advised that the development will achieve the relevant objective noise criteria, subject to acoustic barriers and absorption being implemented. These recommendations incorporate the following works:

- Fixing of acoustic absorption material to the full extent of the underside of the ‘south-western verandah’;
- Construction of the storage shed using minimum 0.36 BMT ‘Colorbond’ (or similar) and having an apex of not less than 2.1m high, noting that the storage shed forms part of the same acoustic barrier as the fence;
- Airtight boundary fencing using timber (pine) paling or 0.42 BMT sheet steel (Colorbond or similar) to the specified heights shown in Figure 1.

The applicant has advised that these works will be implemented as part of the approval and a condition of consent is recommended which requires implementation of all acoustic mitigation measures, prior to commencement of use. The applicant also has an ongoing obligation to ensure the recommendations are met for as long as the use continues at the site.

In respect to noise from car park activities and from mechanical plant, the consultant has made further recommendations in respect to installation of acoustic absorption material to the mechanical area wall and barriers adjacent portion of the car park. These measures shall also form part of the same recommended condition.

Subject to the above works, the modelled noise levels will comply with the objective noise criteria set under Principle of Development Control 7.

#### Building Design and Appearance

Under the General Section, “*Design and Appearance*” module, Objective 1 seeks:

- 1 Development of a high architectural standard that responds to and reinforces positive aspects of the local environment and built form.*

Further, the following Principles of Development Control are relevant:

- 1 The design of a building may be of a contemporary nature and exhibit an innovative style provided the overall form is sympathetic to the scale of development in the locality and with the context of its setting with regard to shape, size, materials and colour.*
- 3 Buildings should be designed to reduce their visual bulk and provide visual interest through design elements such as:*
- (a) Articulation;*
  - (b) Colour and detailing;*
  - (c) Small vertical and horizontal components;*
  - (d) Design and placing of windows;*
  - (e) Variations to facades.*
- 12 Buildings (other than ancillary buildings or group dwellings) should be designed so that their main façade faces the primary street frontage of the land on which they are situated.*
- 22 The setback of buildings from public roads should:*
- (a) Be similar to, or compatible with, setbacks of buildings on adjoining land and other buildings in the locality;*
  - (b) Contribute positively to the streetscape character of the locality;*



- (c) *Not result in or contribute to a detrimental impact upon the function, appearance or character of the locality.*

The development incorporates a new single storey child-care building of 719m<sup>2</sup>. The building will incorporate a foyer, reception, offices, staff room, storeroom, toilets (staff and children's), kitchen, activity spaces (6 of) which are tailored to individual age groups and laundry/cleaning. External verandahs, shade sails and a storage shed are also proposed.

The main building is to be sited 14.24m at its nearest dimension from the Salisbury Highway boundary and 19.35m at the nearest dimension from Evan Avenue. A side setback from the south-western boundary is proposed of 5.8m (excluding verandah elements) while the rear setback to the south-eastern boundary is proposed at 3m.

The building is of contemporary architectural style, having flat roof elements at 2 to 3 degrees in pitch with varying roof parapets and a mix of external materials consisting of brick (dark and red), Colorbond sheeting and high proportions of glass. The total building height will be 4.5m.

The architectural expression and form of development is considered to be appropriate for the site and locality, having regard to the above quoted Principles of Development Control. It is noted that:

- Contemporary architectural forms are contemplated, provided that the scale of development is in context with its setting in respect to shape, size, materials and colours. The scale is considered to be appropriate for its context;
- The building is designed to a high architectural standard, noting it has been designed by an experienced architect. The architectural features employed are characteristic of that envisaged by Principle of Development Control 3;
- The building setbacks are adequate to accommodate landscaping and open car parking in front. The building will achieve a street setback that is harmonious with existing development, noting that road widening does not apply to the site;
- The building achieves generous setbacks from boundaries in order to mitigate external impacts such as overshadow and visual sense of enclosure. Elements that project nearer to the boundary are open sided structures such as verandahs;
- The building incorporates a clearly articulated entry foyer and incorporates elements that encourage surveillance and address to the street, consistent with Principle of Development Control 12.

A storage shed of 20m<sup>2</sup> (5m x 4m) is proposed to be located on portion of the Evan Avenue boundary for a length of 4m. While the shed is provided with no setback or relief, the portion of wall on this boundary is limited both in length (4m) and height (2.4m) and the height is consistent with that of the accompanying side boundary fence. It is not uncommon to have structures of this kind built to the side boundary where on the secondary frontage.

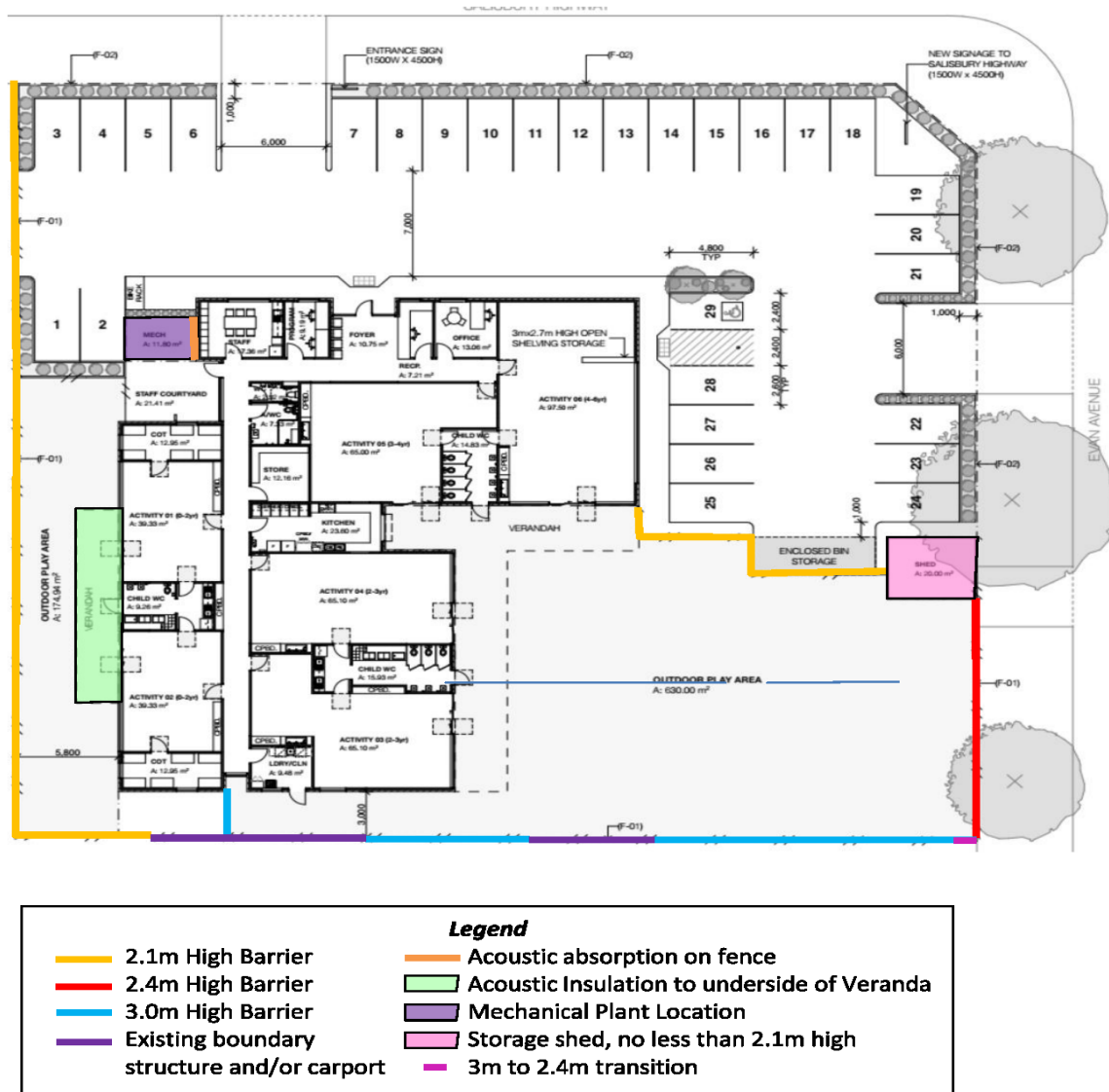
#### Overshadow Impact

Under the General Section, "*Design and Appearance*" module, Principle of Development Control 19 states that:

19 The design and location of buildings should enable direct winter sunlight into adjacent dwellings and private open space and minimise the overshadowing of:

- (a) Windows of habitable rooms;
- (b) Upper-level private balconies that provide the primary open space area for a dwelling;
- (c) Solar collectors (such as solar hot water systems and photovoltaic cells).

The development will incorporate boundary acoustic fencing which is proposed at the recommendation of the acoustic consultant as mentioned earlier. The boundary fencing is proposed as per the image below:



The proposed fencing will have some degree of visual and overshadow impact upon the adjacent properties, in particular, the 3m barrier to portion of the south-eastern side boundary and will fill in several ‘gaps’ between existing building walls on adjacent properties located on the common boundary. While of some height, this wall will not result in any unreasonable amenity impact given that:

- The portion of the fence adjacent to 1 Evan Avenue will be constructed immediately opposite an existing driveway and carport structure;

- The portion of the fence adjacent to unit 4/3 Evan Avenue will be constructed adjacent to an existing tool shed;
- The degree of overshadow cast will be minimal given that it will be during the afternoon period only.

In the case of the south-western side boundary, a 2.1m high fence is proposed to this elevation. This fence is not expected to have any unreasonable degree of impact upon the amenity of the property at 164 Salisbury Highway, noting that the neighbouring yard area is devoid of any buildings or structures and the rear yard area is in excess of 500m<sup>2</sup>. In addition, a non-masonry fence of up to 2.1m in height is not ‘development’ under the *Development Regulations 2008* and can be installed by a property owner ‘as of right’ subject to the *Fences Act 1975*.

#### Vehicular Access

Under the General Section, “*Transportation and Access*” module, Principles of Development Control 23 state that:

**23** *Development should be provided with safe and convenient access which:*

- (a) *Avoids unreasonable interfere with the flow of traffic on adjoining roads;*
- (b) *Provides appropriate separation distances from existing roads or level crossings;*
- (c) *Accommodates the type and volume of traffic likely to be generated by the development or land use and minimises induced traffic through over-provision;*
- (d) *Is sited and designed to minimise any adverse impacts on the occupants of and visitors to neighbouring properties.*

**28** *Development with access from arterial roads or roads as shown on Overlay Maps – Transport should be sited to avoid the need for vehicles to reverse on to or from the road.*

Salisbury Highway is a Secondary Arterial Road under the Salisbury Development Plan and carries in the order of 36,000 vehicles per day. Salisbury Highway is under the care and control of the Commissioner of Highways.

Evan Avenue on the other hand is a local road serving the dwellings fronting this road. Due to the alignment of the Gawler railway line, the catchment population utilising Evan Avenue (and adjoining local roads) is relatively low.

The proposal is designed to facilitate access in and out via Evan Avenue and left in and left out via Salisbury Highway (the latter movement is due to there being a central raised median along Salisbury Highway).

The driveway invert at the kerb will be flared to 6m to permit simultaneous two way movements for the access. The site has two existing crossovers, albeit these will be altered and the portion not utilised by the proposed crossovers shall be reinstated to kerb.

The Evan Avenue access is located approximately 18m from the junction with Salisbury Highway and is positioned between two mature trees which can be safely accommodate/retained in conjunction with the new driveway.

The Salisbury Highway access is located some 40m south-west of the junction with Evan Avenue and is located clear of any existing street trees or verge infrastructure.

The proposed driveway access arrangements are supported under a Traffic Impact Assessment, prepared by GTA Consultants and have been further reviewed by Council's Principal Development Engineer and the Commissioner of Highways.

In summary, the proposed vehicular access arrangements are considered to be acceptable for the following reasons:

- The number of expected vehicular movements is within the capacity of the road network, noting the majority of movements are expected via Salisbury Highway;
- The number of turning manoeuvres at the Salisbury Highway/Evan Avenue junction will increase, however, this is an existing road junction and Commissioner of Highways are satisfied this is within the safe limits of this junction;
- Safe clearance for access is achieved from the Salisbury Highway/ Evan Avenue junction and in the case of the Evan Avenue access, it is located such that the length of movements in Evan Avenue are limited in extent of intrusion into Evan Avenue from Salisbury Highway, thereby minimising disruption to residential uses in this street;
- The proposal will minimise interference with the flow of traffic in Salisbury Highway in that all movements to the site are left in and left out and the access is designed to permit simultaneous two way movements;
- All vehicles are able to enter and exit in a forward direction and the car park and manoeuvring design for the site is compliant with AS 2890 to ensure that vehicular queuing does not occur.

#### Car and Bicycle Parking

Under the General Section, "*Transportation and Access*" module, Principle of Development Control 32 states that:

- 32** *Development should provide off-street vehicle parking and specifically marked disabled car parking places to meet anticipated demand in accordance with Table Sal/2 – Off Street Vehicle Parking Requirements.*

Under Table Sal/2 of the Development Plan, a car parking rate of 1 space per 4 children is assigned.

As mentioned earlier, the facility will have a maximum capacity of 114 children. This means the facility should be served by 29 car parking spaces. The proposal is served by 29 on-site car parking spaces, one of which is marked as a disabled car parking space and is conveniently located near the entry.

This aspect of the development satisfies Principle of Development Control 32 and Table Sal/2.

Under the same "*Transportation and Access*" module, Principle of Development Control 33 further states that:

- 33** *Development should be consistent with Australian Standard 2890 Parking facilities.*

The applicant has submitted a traffic impact assessment, prepared by GTA Consultants. The consultant has advised that the proposed car park design and associated manoeuvring areas are compliant with Australian Standard 2890. Council's Principal Development Engineer and the Commissioner of Highways have reviewed this aspect of the development and advised that it complies with AS 2890.

The applicant has also advised that the proposal will provide a bicycle rack, designed to accommodate parking principally for staff.

### Crime Prevention

Under the General Section, "Crime Prevention" module, Principles of Development Control 3 and 6 state that:

- 3 *Development should provide a robust environment that is resistant to vandalism and graffiti.*
- 6 *Landscaping should be used to assist in discouraging crime by:*
  - (d) *Screen planting areas susceptible to vandalism;*
  - (e) *Planting trees or ground covers, rather than shrubs, alongside footpaths;*
  - (f) *Planting vegetation other than ground covers a minimum distance of two metres from footpaths to reduce concealment opportunities.*

The proposal incorporates an active building frontage and is designed to provide an 'open' outlook to Salisbury Highway and portion of Evan Avenue in the form of the open car parking, landscaping to the street frontage located behind and transparent fencing built at the site boundary. However, portion of the frontage to Evan Avenue will be in the form of the gable end of the storage shed (having wall height of 2.4m) and 2.4m high fencing.

In order to address potential for graffiti of this fence, the applicant has amended the fencing treatment and selected a 'natural timber' finish which the applicant advises will be durable and incorporate some visual interest by having the timber pales in steps to further articulate the fence. This approach provides for some visual interest and for easy cleaning/painting.

This element of the proposal is a shortcoming in the design in that part of the frontage to Evan Avenue is concealed, however, properties on the opposite side of the frontage overlook the street and provide for some surveillance. Further, a 2.1m high fence can be erected on the boundary without requiring a Development Plan Consent and it is not uncommon to have portion of the side boundary fenced in this manner.

### Waste Management

Under the General Section, "Waste" module, Principle of Development Control 6 states that:

- 6 *Development that involves the production and/or collection of waste and/or recyclable material should include designated collection and storage area(s) that are:*
  - (a) *Screened and separated from adjoining areas;*
  - (b) *Located to avoid impacting on adjoining sensitive environments or land uses;*

- (c) *Designed to ensure that wastes do not contaminate stormwater or enter the stormwater collection system;*
- (d) *Located on an impervious sealed area graded to a collection point in order to minimise the movement of any solids or contamination of water;*
- (e) *Protected from wind and stormwater and sealed to prevent leakage and minimise the emission of odours;*
- (f) *Stored in such a manner that ensures that all waste is contained within the boundaries of the site until disposed of in an appropriate manner.*

An enclosed waste storage area is shown on the proposal plans, next to the storage shed. The traffic impact assessment prepared by GTA Consultants has considered the manoeuvring requirements necessary to permit entry and exit to the bin enclosure via Evan Avenue by a 10m long refuse vehicle. Turning templates confirm that sufficient allowance is made for this manoeuvre. In respect to the waste collection, the applicant has advised that:

- The bin collection service will occur within the hours of 7am to 7pm, Monday to Saturday.
- One rubbish collection shall occur per week;
- The enclosure will contain two bins, a 1.5m<sup>3</sup> recycling bin (1.8m high x 0.9m long x 0.9m high) and a 1.5m<sup>3</sup> general waste bin (of the same dimensions).

The applicant intends to utilise the standard Council rubbish/recycling kerbside collection service, subject to staff wheeling the bins to the street frontage. However, the applicant can transition to a private service if they wish to seek more frequent collection service.

The proposed waste collection arrangements are considered to be acceptable.

### Stormwater Management

Under the General Section, “*Natural Resources*” module, Principles of Development Control 8, 9 and 10 state that:

- 8** *Water discharged from a development site should:*
- (a) *Be of a physical, chemical and biological condition equivalent to or better than its pre-developed state;*
  - (b) *Not exceed the rate of discharge from the site as it existed in pre-development conditions.*
- 9** *Development should include stormwater management systems to protect it from damage during a minimum of a 1-in-100 year average return interval flood.*
- 10** *Development should have adequate provision to control any stormwater over-flow runoff from the site and should be sited and designed to improve the quality of stormwater and minimise pollutant transfer to receiving waters.*

The applicant has submitted a preliminary stormwater management plan for the site which provides for:

- Finished floor level in excess of 300mm above top of kerb;
- Site levels at the boundary shall match existing levels;
- Stormwater management on-site from buildings and car parking area discharging via pipes and sumps into to a new junction box located in Evan Avenue;

- Gross pollutant trap provided prior to off-site discharge from the car park.

Council's Principal Development Engineer has reviewed the proposed stormwater management arrangements and advised that they are acceptable, subject to the final design details being worked through prior to granting of full Development Approval.

This aspect of the development satisfies Principles of Development Control 9 and 10, quoted above.

### Landscaping

Under the General Section, "*Landscaping, Fencing and Walls*" module, Principle of Development Control 2 states that:

#### 2 *Landscaping should:*

- (g) *Include the planting of locally indigenous species where appropriate;*
- (h) *Be oriented towards the street frontage;*
- (i) *Result in the appropriate clearance from powerlines and other infrastructure being maintained.*

The applicant has submitted a landscape concept plan, prepared by Birdseye Studios. The landscape concept plan incorporates the following key elements:

- Landscape strip to the street frontages, accommodating trees at regular intervals with hedge lining the full length of the frontage;
- Retention of the existing established trees in the Evan Avenue frontage;
- Shade trees, turf and shade structures within the children's play areas.

The landscaping strip to the street frontage is 700mm in depth but widens at the corner (between car park spaces 18 and 19). The applicant was requested to review the depth of this landscaping bed in order to increase the presence of the landscaping.

The applicant has maintained the original depth of landscaping but has modified the car park design to incorporate wheel stops of adjacent spaces (3-21 inclusive) to avoid conflict/damage by overhanging vehicles. The landscape scheme has also been enhanced by incorporating some taller shade trees within the landscaping bed.

It is preferred that a broader depth of landscaping be accommodated to the street frontages, however, the Zone does not provide a minimum depth of suggested landscaping. Further, the landscape concept plan has nominated appropriate species for this area and subject to being established and maintained, can be a positive element.

The landscape design also incorporates the play space. There are some structures located within the play area that can be climbed. The final location of the play structures should be located so as not to place these structures close to fence-lines, whereby noise from children playing may be transmitted over fences and/or children having views of or being visible from adjoining properties. The final location of such structures shall be designated on a final landscaping plan which is recommended for approval as a Reserved Matter.

In respect to the species selections, Council’s Team Leader – Landscape Design has reviewed the plan and advised that it is generally acceptable, subject to some minor refinements to the final selection species.

### Advertisements

Under the General Section, “Advertisements” module, Principles of Development Control 15 and 18 state that:

**15** *Freestanding advertisements and/or advertising hoardings should be:*

- (a) *Limited to only one primary advertisement per site or complex;*
- (b) *Of a scale and size in keeping with the desired character of the locality and compatible with the development on the site.*

**18** *Freestanding advertisements should not exceed the total height (measured from natural ground level) as specified within the following table:*

<b>Location of freestanding advertisement</b>	<b>Total height (in metres)</b>
<i>In all other locations</i>	<i>3</i>

The application incorporates one wall mounted advertisement having dimensions of 6m wide x 2m high and two freestanding advertisements having dimensions of 1.5m wide by 4m high. The final graphics for the sign is still to be determined, however, the applicant has advised the signage will not be illuminated, move, flash or rotate.

The wall mounted sign is of considerable dimensions, however, it does not require approval in its own right under Schedule 3 of the *Development Regulations 2008* in that wall mounted signs fixed to premises used primarily for retail, commercial, office or business are exempt from approval provided they do not move, flash, reflect light or contain illumination.

The proposed freestanding signage offends the above-mentioned Principles of Development Control in that:

- a) The number of freestanding signs is double that anticipated for the site;
- b) The overall height is 1m in excess of the maximum anticipated.

This aspect of the development represents a departure of some degree from the relevant Development Plan provisions but in the overall scheme of the development is considered to be acceptable, recognising that:

- a) The application incorporates two sites (having two individual frontages) and thus developments of the two sites could theoretically allow for two freestanding signs, as anticipated under Principle of Development Control 15(a). Further, the signs are to be sited 32m apart, thus providing some spacing between the signs;
- b) The sign heights are commensurate with the overall single storey height of the building and are well designed in terms of having a blade feature that steps above the main sign height (the plan is not dimensioned but scaling of the plan suggests it is set some 200mm or so lower at about 3.8m). They will not be ‘tall’ elements in the context of the development.



## 12. CONCLUSION

The applicant seeks Development Plan Consent for a child care facility comprising a new building, signage, boundary acoustic fencing and associated car parking.

The assessment found that the proposal achieves a high degree of compliance with the Development Plan as follows:

- a) The proposed form of development is envisaged within the Residential Zone and the proposal is of a 'small-scale';
- b) The site is considered to be appropriately located for this facility, noting that child-care centres are commonly located on main roads;
- c) The facility will not have an unreasonable impact upon the residential amenity of existing dwellings having regard to the operating hours and projected noise levels;
- d) The building is designed to a high standard and will be of a footprint, scale and setback that is compatible with the locality;
- e) Acoustic fencing necessary to achieve the objective noise criteria will not have an unreasonable degree of overshadow impact or sense of enclosure for neighbours;
- f) Vehicular access is designed to permit simultaneous forward entry and exit for vehicles and is to the satisfaction of the Commissioner of Highways;
- g) Sufficient on-site car parking is provided for the facility;
- h) Waste management arrangements are accommodated;
- i) Stormwater management arrangements are acceptable, subject to final design;
- j) A comprehensive landscape concept plan is proposed to be implemented, prepared by a landscape architect;
- k) Signage is proposed to be employed to a high standard and which complements the overall form of development.

Some minor departures from the Development Plan policy are identified in respect to the proposed height of the signs, the depth of landscaping to the street frontage and the presence of solid fencing to portion of the Evan Avenue frontage. These elements are shortcomings in the design but are considered to be acceptable having regard to the overall positive elements of the design as a whole.

Given the above, it is recommended that Development Plan Consent be granted, subject to Reserved Matters and conditions.

## 13. STAFF RECOMMENDATION

That the Council Assessment Panel resolve that:

- A. The proposed development is not considered to be seriously at variance with the Salisbury Development Plan – Consolidated 15<sup>th</sup> December 2016.
- B. Pursuant to Section 33 of the *Development Act 1993*, Development Plan Consent is **GRANTED** to application number 361/1720/2018/3B for Child care centre comprising new building, signage, boundary acoustic fencing and associated car parking in accordance with the plans and details submitted with the application and subject to the following Reserved Matter and conditions:

Reserved Matters:

The following matters shall be submitted for further assessment and approval by the Manager – Development Services, as delegate of the Development Assessment Panel, as Reserved Matters under Section 33(3) of the Development Act 1993:

1. Civil Plan, prepared by a qualified and experienced civil engineer which shall include all of the following:
  - a) Finished floor level for buildings, driveways and impervious surfaces, noting that the finished floor level of the building shall be at least 300mm above top of kerb;
  - b) Cut/fill details;
  - c) Retaining walls (if required);
  - d) Kerbing and driveway crossovers, noting that all inverts and driveway crossovers shall comply with Council Design Details;
  - e) Closure of all redundant inverts and crossovers and reinstatement with upright kerb in accordance with Council Design Details;
  - f) Pavement design details and gradients;
  - g) Car parking dimensions and aisle widths;
  - h) Water quality treatment systems including primary treatment (ie. gross pollutant transports) and secondary treatment using water sensitive design elements;
  - i) Pedestrian paths, widths, surface treatments and grades;
  - j) Stormwater management system including all sumps, drains, pipes etc., their sizes and fall to ensure appropriate stormwater management and discharge of stormwater from the site.
  
2. Final Landscaping Plan, noting the following changes should be adopted:
  - Location of all climbing structures, sited clear of fencelines so as to avoid direct overlooking into yards and/or transmission of noise over fences;
  - *Syzygium australe* should be replaced by a smaller species (to avoid overgrowth/encroachment into the verge) such as hybrids of *Syzygium australe* ‘Tiny Trev’ or ‘Winter Lights’;
  - The area dedicated for planting of *Ca – Cupaniopsis* should be considered, noting the garden area depth is less than 1m thus providing limited capacity for the trees to reach their mature size;
  - Boundary hedge plantings need to be maintained at a low height such that pedestrians and footpath users are not concealed (particularly in the immediate area adjacent driveways), especially on Salisbury Highway with the opportunity for cyclists to be on the footpath and traffic sight lines when entering Salisbury Highway.

Development Plan Consent Conditions

1. The development shall be carried out in accordance with the details submitted with the application and the following stamped approved plans and documents, except where otherwise varied by the conditions herein:

<b>Drawing No.</b>	<b>Plan Type</b>	<b>Date</b>	<b>Prepared By</b>
Ref 2018/0349 dated 07.11.18	Response to Info Request and Reps	Received by Council dated 07.11.18	URPS
Dwg No. 3522.SK01 Rev B	Location and Site Plan	Received by Council dated 07.11.18	Flightpath
Dwg No. 3522.SK02 Rev B	Proposed Site Plan	Received by Council dated 07.11.18	Flightpath
Dwg No. 3522.SK03 Rev B	Proposed Roof Plan	Received by Council dated 07.11.18	Flightpath
Dwg No. 3522.SK04 Rev B	Elevations 01	Received by Council dated 07.11.18	Flightpath
Dwg No. 3522.SK05 Rev B	Elevations 02	Received by Council dated 07.11.18	Flightpath
Dwg No. 3522.SK06 Rev B	Streetscape Elevations and Renders	Received by Council dated 07.11.18	Flightpath
Dwg No. 3522.SK07 Rev B	Signage Details	Received by Council dated 07.11.18	Flightpath
06/11/2018 Rev C	Cover Page	Received by Council dated 07.11.18	Birdseye Studios
Dwg No. 18045 SK01	Concept Plan	Received by Council dated 07.11.18	Birdseye Studios
Dwg No. 18045 SK02	Planting Plan	Received by Council dated 07.11.18	Birdseye Studios
Dwg No. 18045 SK03	Reference Images	Received by Council dated 07.11.18	Birdseye Studios
Dwg No. 18045 SK04	Plant Palette	Received by Council dated 07.11.18	Birdseye Studios
S5660C3 November 2018	Environmental Noise Assessment	Received by Council dated 07.11.18	Sonus
S159600 dated 06.11.18	Transport Impact Assessment	Received by Council dated 07.11.18	GTA Consultants
20505 20.09.18 Revision 00	Stormwater Management Report	Received by Council dated 07.11.18	PT Design
20505 20.09.18 Revision 00	Stormwater Calculations	Received by Council dated 07.11.18	PT Design
20505-C1 Issue B26.09.18	Stormwater Management Plan	Received by Council dated 07.11.18	PT Design
2018-0349 27 September 2018	Planning Statement	Received by Council dated 27.09.18	URPS

\* *The approved documents referred to above may be subject to change by minor variations permitted under the Building Rules Consent process.*

\* *Except where otherwise stated, the development shall be completed prior to the commencement of use.*

\* *Documents approved under Reserved Matters 1 and 2 also form part of this consent.*

*Reason: To ensure the proposal is established in accordance with the submitted plans.*

2. Site work, demolition work and building work shall be limited to the hours of 7:00am to 7:00pm Monday to Saturday and 9:00am to 5:00pm Sunday.

*Reason: To limit the effect of construction on the amenity of the locality.*

3. The facility shall operate between the hours of 6:30am to 6:30pm, Monday to Friday.

*Reason: To ensure the use operates in accordance with the approval.*

4. Measures shall be implemented to eliminate dust emissions from the site during the construction period so as not to cause nuisance to adjacent properties or the adjacent road network.

*Reason: To preserve the amenity of the locality during construction work.*

5. The facility shall not exceed:

- a) 114 children at any one time;
- b) 16 staff at any one time.

*Reason: To ensure the number of children and staff on-site does not exceed the intended maximum capacity for which the facility (including associated car parking and children's play areas) is designed.*

6. The advertisements and advertising structures shall be maintained in good repair at all times.

*Reason: To maintain the visual amenity of the locality.*

7. All boundary fencing shall be maintained in good repair at all times to the reasonable satisfaction of Council. Any graffiti shall be removed within 24 hours.

*Reason: To maintain the visual amenity of the locality.*

8. Except where otherwise approved, the external finishes of the building shall:

- (a) Be of new non-reflective materials; and
- (b) Be finished in colours/tones as shown on the Approved Plans; and
- (c) Be maintained in good condition at all times.

*Reason: To ensure the building achieves a high standard of external appearance.*

9. Any roof mounted plant or equipment shall be sited to avoid being visible to street view or otherwise be screened in a manner that forms an integral part of the building design.

*Reason: To ensure roof mounted plant and equipment is appropriately designed.*

10. All open sided structures (ie. verandahs) shall remain open and unobstructed at all times.

*Reason: To maintain the open appearance of these structures.*

11. All of the recommendations contained in the acoustic report, prepared by Sonus Acoustic Consultants, received by Council dated 7<sup>th</sup> November 2018, shall be implemented in full, prior to commencement of use and shall remain in place at all times thereafter.

*Reason: To ensure the number of children on-site does not exceed the intended maximum capacity for which the facility (including associated car parking and children's play areas) is designed.*

12. No materials, goods or containers shall be stored in the designated car parking area or driveways at any time.

*Reason: To ensure the car parking areas are always available for the purpose they are designed. Further, that the site be maintained in a clean and tidy state.*

13. No amplified devices of any type shall be used outside of the building on the site at any time.

*Reason: To ensure that noise does not cause nuisance to adjoining or nearby residential occupiers or owners.*

14. All waste and other rubbish shall be contained and stored pending removal in covered containers, in an area screened from public view.

*Reason: To maintain the amenity of the locality.*

15. Outside lighting shall be restricted to that necessary for security purposes only and shall be directed and shaded to prevent light overspill and/or nuisance to adjacent occupiers or distraction to drivers on adjacent public roads.

*Reason: To ensure that floodlighting does not cause nuisance or danger to adjoining occupiers or road users thereby reducing the amenity of the locality and/or making road use unsafe.*

16. All landscaping as shown on the Landscaping Plan, Approved by Council under Reserved Matter 2, shall be completed prior to commencement of use. All landscaping shall be maintained (including the replacement of diseased or dying plants and the removal of weeds and pest plants) to the reasonable satisfaction of Council.

*Reason: To ensure the site is landscaped so as to enhance the visual and environmental amenity of the locality.*

17. All loading and unloading of vehicles and manoeuvring of vehicles in connection with the now approved land use shall be carried out entirely within the site at all times.

*Reason: To ensure that vehicles associated with the land use do not cause disruption or danger to vehicles on adjoining public roads.*

18. The car parking layout including car parking spaces and aisle widths shall be designed and constructed to comply with AS 2890.1 – Off-street parking Part 1 and Austroads “Guide to Traffic Engineering Practice Part 11 – Parking”.

*Reason: To ensure that the development complies with Standards and Best Engineering Practice.*

19. All driveways and car parking areas as shown on the Approved Site Plan, prepared by Flightpath, received by Council dated 7<sup>th</sup> November 2018, shall be constructed with either brick paving, concrete or bitumen to a standard appropriate for the intended traffic volumes and vehicle types. Individual car parking bays shall be clearly line marked. Driveways and car parking areas shall be established prior to the commencement of use and shall be maintained at all times thereafter to the satisfaction of Council.

*Reason: To ensure access and car parking is provided on the site in a manner that maintains and enhances the amenity of the locality.*

20. The stormwater system shall be designed and constructed to cater for minor storm flows (Residential ARI = 5 years). The design of the stormwater system shall ensure that no stormwater is discharged onto any adjoining land. Surface stormwater shall be managed in a manner that ensures no ponding of water against buildings and structures, no creation of any insanitary condition and no runoff into neighbouring property for the major storm event ARI = 100 years.

*Reason: To ensure flood protection of the development.*

21. The new stormwater infrastructure (junction box in Evan Avenue) shall be vested in Council following installation and shall be completed to the reasonable satisfaction of Council.

*Reason: To ensure the stormwater infrastructure satisfies Council requirements.*

***Commissioner of Highways Conditions***

22. All vehicles shall enter and exit the site in a forward direction at all times.
23. All on-street car parking areas shall be designed in accordance with AS/NZS 2890.1:2004 and AS/NZS 2890.6:2009. Additionally, clear sightlines, as shown in Figure 3.3 'Minimum Sight Lines for Pedestrian Safety' in AS/NZS 2890.1:2004, shall be provided at the property line to ensure adequate visibility between vehicles leaving the site and pedestrians on the adjacent footpath.
24. The largest vehicle permitted on-site shall be a 10 metre rigid truck, which shall only be permitted to access the site outside of business operating hours.

***Advice Notes***

1. Except where otherwise approved, no other advertisements or advertising displays shall be displayed on or about the site.
2. Waste collection arrangements for the site by the Northern Adelaide Waste Management Authority (NAWMA) does not extend to on-site collection. Should on-site collection be desired, an agreement must be signed between the applicant and NAWMA to facilitate this service.
3. The applicant is reminded of its general environmental duty, as required by Section 25 of the *Environment Protection Act 1993*, to take all reasonable and practicable measures to ensure that the activities on the whole site, including during construction, do not pollute the environment in a way which causes or may cause environmental harm.
4. EPA information sheets, guidelines documents, codes of practice, technical bulletins etc. can be accessed on the following web site: <http://www.epa.sa.gov.au>.

**CO-ORDINATION**

Officer: GMCiD MDS  
Date: 19.11.18 15.11.18

**ATTACHMENTS**

This document should be read in conjunction with the following attachments:

1. Application Documentation
2. Category 3 Notice and Copy of Representations
3. Commission of Highways Referral Response
4. Internal Referral Responses
5. Relevant Development Plan Provisions



***ATTACHMENT 1***  
***APPLICATION DOCUMENTATION***



### DEVELOPMENT APPLICATION FORM

361/ 2018/

Please use BLOCK LETTERS and Black or Blue Ink

I wish to apply for (tick only one): <input checked="" type="checkbox"/> Development Plan Consent <input type="checkbox"/> Building Rules Consent	
<input type="checkbox"/> Full Development Approval (consists of both consents, which is required prior to any work commencing)	
APPLICANT: COMPANY / FIRST NAME AO Salisbury Pty Ltd	SURNAME -
POSTAL ADDRESS:	
EMAIL:	
OWNER NAME: (This must be completed) <input type="checkbox"/> as above Dinh Ngoc Bich Le (158-160) and Thi Xuan Dao Luong (162)	
OWNER POSTAL ADDRESS: 158-160 = 209-211 WHITES ROAD, PARALOWIE, SA, 5109 <input type="checkbox"/> as above 162 = 209-211 WHITES ROAD, PARALOWIE, SA, 5109	
OWNER PHONE NO:	OWNER EMAIL:
CONTACT PERSON FOR FURTHER INFORMATION <input type="checkbox"/> as above	
NAME: Mr Phil Harnett (URPS)	TELEPHONE (W): (08) 8333 7999 (M): 0451 118 577
EMAIL: philp@urps.com.au	Information from Council will be given by electronic communication to the nominated email address.
BUILDER NAME: TBC	BUILDERS EMAIL:
BUILDER POSTAL ADDRESS:	CONTACT NO.:
	LICENCE NO.:
CURRENT USE OF PROPERTY:	
DESCRIPTION OF PROPOSAL:	DEVELOPMENT COST \$ 1.1 Million
LOCATION OF PROPOSAL	
Street No: 158-160 162	Street: Salisbury Highway
	Suburb: Salisbury
Lot No: 124 122	Section:
Plan:	Volume: 5113/89 5460
	Folio: 67 82
OFFICE USE ONLY	
Registration Date: 1 / 2018	Zone: Ward:
BUILDING RULES CLASSIFICATION	
Classification sought:	
If Class 5, 6, 7, or 9 classification is sought, state the proposed number of employees	
Male: Female:	
If Class 9a classification is sought, state the number of persons for whom accommodation is provided:	
If Class 9b classification is sought, state the proposed number of occupants of the various spaces at the premises:	

I acknowledge that copies of this application and supporting documentation may be provided to interested persons in accordance with the Development Regulations, 1993. Developments requiring public notification will be made available to the public for comment via Council's web site at [www.salisbury.sa.gov.au](http://www.salisbury.sa.gov.au)

SIGNATURE: Phil Harnett (URPS) DATE: 27/9/18  
(PHIL HARNETT)

Ref: 2018-0349

7 November 2018

Mr Aaron Curtis  
Team Leader - Planning  
City of Salisbury  
12 James Street  
SALISBURY SA 5108



Suite 12  
154 Fullarton Road  
ROSE PARK SA 5067

08 8333 7999  
www.urps.com.au  
ABN 55 640 546 010



Dear Aaron

**Response to Additional Information Request – Development Number 361/1720/2018/3B – Child Care Centre**

Thank you for your letter dated 29 October 2018 requesting some additional information and advising of the Category 3 representations received for the child care centre proposed at 158-160 and 162 Salisbury Highway, Salisbury (Development Number 361/1720/2018/3B).

Following the request of AO Salisbury Pty Ltd, I provide you with our response to the additional information request, followed by a response to the provided representations.

I have addressed each matter of your letter in corresponding order below.

**1. Civil Site Works and Drainage Plan**

You have requested particular details to be shown on the Civil Site Works and Drainage Plan. Each of these matters is addressed below as per our subsequent email correspondence dated 05 November 2018:

- I. The proposed finished floor level (27.85) is shown on the Stormwater Management Plan (Drawing Number 20505-C01 - issue B) prepared by PT Design as attached.
- II. The land has a gradual fall of 300-500mm diagonally across the property. This fall spans over an approximate distance of 68 metres. The car parking area will be almost flat with only minor falls throughout for drainage purposes. Any levels will be designed to suit the proposed finished floor level and existing boundary levels with safe overland flow during a major storm event.
- III. Any cut and fill will be marginal with no retaining walls expected. If required retaining walls will not be high (i.e. a 200mm high concrete plinth beneath fences).
- IV. Above ground storage within the proposed car parking area will provide stormwater detention. The attached Stormwater Management Report prepared by PT Design dated 20/09/2018 provides calculations to justify the proposed detention volume. Given the associated cost, we kindly request on our client's behalf that stormwater pipe and pit size details be provided once we have the guarantee and comfort of planning consent. We are happy to provide this information as a response to a condition of consent or reserved matter, if necessary.

shaping great communities

- V. The proposed car parking area will have upright perimeter concrete kerbing that ranges from 100mm to 150mm in height.
- VI. Only minor perimeter paving is proposed around the building. Once again, we please request that this information be provided at detailed design stage, after the guarantee of planning consent. We are happy to provide this information as a response to a condition of consent or reserved matter if necessary.

## 2. Landscaping Concept Plan

Thank you for confirming that the submitted Landscape Concept Plan is acceptable in principle. Please refer to the amended 'Landscape Concept Plans' prepared by Birdseye Studios which now show:

- The final plant mix/schedule.
- The species and types of plants to be used.
- A mix of shade trees, shrubs and ground covers.

The proposal is considered to provide amenity and visual relief to the proposal and the car parking area.

## 3. Crime Prevention

With reference to General Section, Crime Prevention Principle 6, you have raised the following matters:

- The fence fronting Evan Avenue may be susceptible to vandalism.
- There is scope to accommodate the fence within the site (i.e. stepping fence back 1m for example) to achieve the required acoustic attenuation while also providing landscaping in front to deter graffiti of the fence to and provide a high-quality presentation to Evan Avenue.

Thank you for your suggestions however it is preferred that the fence fronting Evan Avenue not be stepped into the property because:

- It will reduce the space available for the outdoor play area.
- Legislation requires that a particular amount of outdoor play area be provided based upon the maximum number of children accommodated.

General Section, Crime Prevention Principle 6 states:

- 6 Landscaping should be used to assist in discouraging crime by:**
- (a) Screen planting areas susceptible to vandalism**
  - (b) Planting trees or ground covers, rather than shrubs, alongside footpaths**
  - (c) Planting vegetation other than ground covers a minimum distance of two metres from footpaths to reduce concealment opportunities.**

Instead of stepping the fence into the property as you suggested, we have decided to choose a type of fencing that will have a natural timber appearance (as pictured in Image 2 below). This fencing will:

- Not reflect light to distract motorists.
- Be subtle in the streetscape and durable (i.e. unlike say glass) so as not to attract vandalism.
- Have stepped timber slats to be less attractive to graffiti (i.e. the fence is not a flat surface).
- Allow for easy cleaning or painting if the fence is ever vandalised by graffiti.
- Not allow areas for concealment within Evan Avenue.

- Continue to have the required acoustic properties.

Image 1: Indicative boundary fencing along Evan Avenue.



Further, and in any case, the subject fence sits adjacent two units with clear lines of sight which may deter such activity occurring. In other words, the presence of these dwellings and the open nature of Evan Avenue will likely deter criminal activity.

It must also be acknowledged that a 2.1 metre high fence could be built on the Evan Avenue boundary without any planning consent in accordance with the Development Regulations 2008.

#### 4. Landscaping to Salisbury Highway and Evan Avenue

You have stated:

*“The proposed site plan incorporates a 1m landscaping strip adjacent to the Salisbury Highway boundary and portion of the Evan Avenue boundary, however, the landscaping strip is only 1m in depth and due to the limited depth of the car parks (4.8m), vehicles will overhang into the landscaping strip, thus compromising what is a limited depth landscaping strip this compromising the plantings. Accordingly, it is suggested that the depth of the car parks be increased and inclusion of wheel stops to avoid encroachment into the landscaping bed. It is also suggested the landscaping strip should be increased in depth to enable more substantive plantings to be accommodated. It is noted that the 7m aisle could possibly be reduced in width in order to partly accommodate the increased car parking depth and increased depth of the landscaping bed”.*

Thank you for your suggestions. In response to these, we have made the following amendments:

- Reduced the driveway aisle from 7 metres to 6.8 metres. This aisle width still satisfies relevant Australian Standards.
- Increased the length of car parking spaces from 4.8 metres to 5.4 metres. This increased length satisfies the relevant Australian Standards.
- Provided wheel stops at the end of each car parking space. These wheel stops will prevent parked vehicles from overhanging into nearby landscaping strips and damaging respective trees.
- Reduced the Salisbury Highway landscaping strip from 1 metre to 0.7 metres. The reduced width of the landscaping allows meaningful landscaping given the species selected as per the amended 'Landscaping Concept Plans' prepared by Birdseye Studios (Revision C). The provision of wheel stops (as explained above) ensures that the available landscaping area is more suitable to achieve successful plantings, despite the width of the landscaping strip being reduced. This is because vehicles will not overhang into the landscaping area subsequently damaging plants.

You have also stated:

*"... car parking spaces 25-29 inclusive have a depth of 4.8m, thus meaning they will encroach into the adjacent footpath. It is suggested that wheel stops be incorporated and the depth of the car parking spaces be increased in order to avoid conflict with the footpath".*

This matter has been reviewed however not amended. This is because the adjacent footpath in front of car parking spaces 25 to 29 is 1.65 metres wide. Any vehicle overhang will still allow pedestrians to safely and conveniently utilise the footpath given its width.

In connection with the above you have referenced General Section, Landscaping, Fences and Walls, Principle 2 which states:

**2 Landscaping should:**

- (a) include the planting of locally indigenous species where appropriate**
- (b) be oriented towards the street frontage**
- (c) result in the appropriate clearance from powerlines and other infrastructure being maintained.**

The proposal satisfies Principle 2 as per the amended 'Landscaping Concept Plans' prepared by Birdseye Studios (Revision C).

## 5. Enclosed Bin Storage

Each matter raised is addressed in corresponding order below:

- a) Proposed operating hours are limited to 6:30am to 6:30pm Monday to Friday. Notwithstanding this, the bin collection will occur within the hours of 7am to 7pm Monday to Saturday.
- b) 1 rubbish collection will be required per week.
- c) The enclosure will contain two bins, 1 x 1.5m<sup>3</sup> recycling bin (measuring 1.8m high x 0.9m long x 0.9m high) and 1 x 1.5m<sup>3</sup> general waste bin (measuring 1.8m high x 0.9m long x 0.9m high). It is anticipated that operations will commence using the standard council rubbish/recycling kerbside collection service and if/when required transition to a private service will be made.

### Response to Representations

The following representations were received after Category 3 public notification.

Number	Representor	Address	Wishes to be heard by CAP?
1	GM Prince	19 Evan Avenue, Salisbury	Yes (Joe Bivone)
2	R Tedesco	26 Evan Avenue, Salisbury	Yes (Joe Bivone)
3	K Woolger	164 Salisbury Highway	Yes (Joe Bivone)
4	N Bivone & A Bivone	159 Salisbury Highway	Yes (Joe Bivone)
5	LN Ghimire	1 Evan Avenue, Salisbury	Yes (Rupa Ghimire)
6	RJ Tinkler	Unit 3/154 Salisbury Highway, Salisbury	Yes
7	G Bivone	40 Immanuel Drive, Salisbury	Yes (Joe Bivone)
8	NJ Karandrikas	4 Arthur Street, Salisbury	No
9	TM Williams & M Banks	6 Arthur Street, Salisbury	Yes
10	C Luppino	9 Arthur Street, Salisbury	No
11	R Caruso & K Caruso	155 Salisbury Highway, Salisbury	No
12	B Robbie & V Robbie	25a Evan Avenue, Salisbury	No
13	DF Luppino	9 Arthur Street, Salisbury	No
14	SE Humpherson	34b Evan Avenue, Salisbury	No
15	S Kemot	34a Evan Avenue, Salisbury	No
16	HP Vuong	Unit 2/154 Salisbury Highway, Salisbury	No
17	CJ Chenoweth	7 Evan Avenue, Salisbury	Unspecified
18	L Thomas & R Antony	16 Thorngate Drive, Paralowie	No (supports the development)

19	G Ciccone & G Ciccone	15 Theta Street, Salisbury	No (supports the development)
20	JH Readings & SJ Readings	163 Salisbury Highway, Salisbury	Yes
21	SP Collins & A Sargent	Unit 8/6 Evan Avenue, Salisbury	No (supports the development)
22	AJ Lennox	Unit 7/6 Evan Avenue, Salisbury	No (supports the development)

Instead of addressing each representors concerns individually, I have summarised the issues and provided a response across all of the representations, under the subheadings below.

The map below shows the location of nearby representors. When using the map it is important to note the following:

- Some of the representors are not situated within the locality and therefore are not shown on the map. Some representors are situated in other suburbs.
- The yellow lines delineate the subject land.
- Green dots represent some of the representors that support the proposal.
- Red dots represent some of the representors that oppose the proposal or have some concerns.





#### *Need for the Proposed Use*

One of the representors has stated "There is no need for another Child Care Centre" with another stating "There are already Child Care Centres in close proximity".

The Development Plan does not require one to demonstrate that there is adequate demand for their development to warrant planning consent. The decision on whether to apply for the proposal is based upon the applicant's own personal preference for their business. In any case I note AO Salisbury Pty Ltd has undertaken considerable research to ensure the need and viability of the proposed development on the subject land. The applicant has subsequently purchased the properties at considerable expense which is a decision not taken lightly.

In addition, it is unlikely that the proposal will be capable of undermining the operation of any nearby existing child care centres.

The proposal also does not offend centre zone policies. The nearest centre is a small Local Centre Zone situated approximately 100 metres south of the subject land further along Salisbury Highway. A large Urban Core Zone that accommodates the Salisbury Railway Station, Salisbury Cinemas and a variety of shops, offices and restaurants is situated approximately 400 metres north of the subject land. The proposed land use will not jeopardise the efficient operation of these centres because:

- The nearby Local Centre Zone is small and only accommodates 2 or 3 shops. This particular Local Centre Zone does not comprise a land use that would compete with the proposed child care centre.
- The subject land is sufficiently distanced from the Urban Core Zone so as not to compete with land uses within it. In addition, the majority of existing land uses within the Urban Core Zone provide substantially different services to the childcare centre proposed.

#### *Zoning and Land Use*

Some of the representations have indicated that the use is unsuitable for a residential zone. I disagree.

The Desired Character of the Residential Zone states:

**In addition to housing and open space, the zone will also contain a variety of community related, non-residential land uses that contribute to the creation of pleasant, connected, well serviced and socially inclusive neighbourhoods.**

Principle 1 of the zone states:

**The following forms of development are envisaged in the zone:**

- ...
- **small scale non-residential use that serves the local community, for example:**  
- child care facility--

The proposed land use is specifically envisaged in the zone. There can be no question that such uses are contemplated in the subject zone.

The only question is whether or not the proposal is appropriate when considered in the context of PDC 4 which states:

**Non-residential development** such as shops, schools and consulting rooms **should be of a nature and scale that:**

- (a) serves the local community**
- (b) is consistent with the character of the locality**
- (c) does not detrimentally impact on the amenity of nearby residents**
- (d) will not undermine the efficient operation of nearby centres.**

The proposed child care centre will certainly be a use that serves the local community, offering child care services to local working families with children aged 0 to 6 years old. As such, matter (a) is satisfied.

The proposal's compliance with items (b), (c) and (d) is demonstrated below.

#### *Design and Appearance*

Some of the representations have stated that *"It will make our street ugly"*.

Conversely, another representor has stated *"Good that the land is used for something because the current house there on that land has been empty for a long time"*.

The proposal has been architecturally designed by a leading and award-winning firm (Flightpath) who have designed a building that is cohesive and attractive, and satisfies the desired character of the zone and general design and appearance provisions of the Development Plan in that it features:

- **Broad front, side and rear setbacks that provide substantial spacing around the building for car parking and landscaping.**
- **A single storey and low scale built form with eave heights ranging from 3.6 to 3.9 metres and a total height up to 4.5 metres. This total height is lower than many residential buildings situated within the locality.**
- **A mix of flat and low-pitched roof elements that provide interesting roof features yet keep the building low-scale and respectful of surrounding dwellings.**
- **Windows and doors that face Salisbury Highway, Evan Avenue and the proposed car parking area. These windows and doors are proportionate to the building and will be glazed to enhance casual surveillance during opening hours.**
- **Horizontal and vertical components that provide design balance and enhance building articulation and visual interest.**
- **Materials and finishes which are common of residential buildings including brick feature walls. Proposed materials are also low-light reflective to prevent glare.**
- **Proportionate and modest advertisements that do not dominate the proposed building or its locality.**

Furthermore, street fronting fencing along Evan Avenue will have a natural timber appearance (refer image 1 earlier in this correspondence) and will only have a length of 14 metres (27% of the Evan Avenue frontage). The remainder of the Avenue frontage will remain open with landscaping and car parking areas.

### On-Site Car Parking

One of the representations has indicated that the proposal does not supply enough car parking spaces.

General Section, Transportation and Access Principle 32 states:

**32** Development should provide off-street vehicle parking and specifically marked disabled car parking places to meet anticipated demand in accordance with Table Sal/2 - Off Street Vehicle Parking Requirements.

Table Sal/2 guides that a child care centre should have 1 car parking space per 4 children.

The proposal will have a maximum capacity of 114 children, as such 29 car parking spaces are required (i.e.  $114/4 = 28.5$ ).

The proposal satisfies car parking requirements as per the Development Plan with 29 on-site car parking spaces proposed, 1 of which is marked as a disabled car parking space.

General Section, Transportation and Access Principle 33 states:

**33** Development should be consistent with Australian Standard AS 2890 Parking facilities.

The proposal is consistent with relevant Australian Standards.

### On-Street Parking

Some of the representations have stated "We will lose parking in our street". I understand that this is with reference to Evan Avenue given that on-street parking on Salisbury Highway is prohibited by way of a 'yellow line' marking across the entire frontage of the subject land.

General Section, Residential Development Principle 36 states:

**36** The number of driveway crossovers should be minimised and appropriately separated to optimise the provision of on-street visitor parking and preserve and enhance street character including opportunities for landscaping, tree planting and fences.

The subject land has frontage to Evan Avenue of approximately 50.2 metres and currently has a vehicle crossover extending from Evan Avenue with a width of 3 to 4 metres. The remainder of the Evan Avenue frontage is currently available for on-street parking, along with the opposite side of Evan Avenue.

The existing vehicle crossover from Evan Avenue to the subject land will be re-instated to upright kerb with a new crossover proposed that would have a width of 6 metres. As such, the available length of the Evan Avenue frontage for on-street parking will be reduced by approximately 2 to 3 metres. Approximately 44.2 metres of the Evan Avenue frontage will remain available which is the equivalent of 7 to 8 cars parked on-street. On this basis, the proposal:

- Certainly minimises driveway crossovers to Evan Avenue with only 1 crossover proposed.
- Clearly optimises on-street visitor parking by retaining approximately 44.2 metres of the Evan Avenue frontage, the equivalent of 7 to 8 cars.

In addition:

- On-street parking on the opposite side of Evan Avenue from the subject land will not be modified and will continue to be available.

- The proposal provides sufficient on-site car parking in accordance with the Development Plan.
- Child care centres generate very few trips during the day between peak periods. Hence any traffic impact will be during the morning and evening when children are dropped off and picked up.
- GTA Consultants have concluded that traffic generated by the proposed development could not be expected to compromise the safety or function of the surrounding road network.

Overall, in my view, the proposal will not have a detrimental impact upon the availability of on-street car parking.

#### Traffic

Some of the representations have stated “Too much traffic” and “lots of disruptions to traffic coming in and out of our street”. Once again, I understand that this is with reference to Evan Avenue.

The Evan Avenue access point to the subject land is approximately 14 metres from the intersection of Salisbury Highway. As such, vehicles accessing the property via Evan Avenue, from Salisbury Highway will only need to travel a short distance along Evan Avenue before accessing the subject land. Traffic accessing the property in this manner is unlikely to disrupt nearby residences.

For the following reasons, we are of the opinion that any traffic generated from the proposal is acceptable:

- The proposal is situated on the corner of Salisbury Highway and Evan Avenue. Parents from the wider locality will likely drive to the proposal via Salisbury Highway preventing the need to travel further along Evan Avenue.
- The majority of parents will leave the proposal via Salisbury Highway because it provides enhanced connectivity to other areas, including employment. The south-eastern end of Evan Avenue abuts the northern railway corridor in which vehicle access across the railway line is not situated nearby. This means that the south-eastern end of Evan Avenue provides limited and inconvenient connectivity to other areas, and, as a result, is unlikely to be used significantly.
- The proposal provides sufficient on-site car parking in accordance with the Development Plan.
- Child care centres generate very few trips during the day between peak periods. Hence any traffic impact will be during the morning and evening when children are dropped off and picked up.
- GTA Consultants have concluded that traffic generated by the proposed development could not be expected to compromise the safety or function of the surrounding road network.

Some of the representors have raised concern about parents accessing the proposal from the opposite side of Salisbury Highway and crossing the road. Some residents have indicated that this may result in a need for additional traffic lights and potentially a pedestrian crossing on Salisbury Highway. In response to this, I note the following:

- Several parents will drive to the property and drop off/pick up their children before travelling elsewhere. This means that no parents and children will need to walk across Salisbury Highway.
- It is rare that a nearby resident would walk their child across Salisbury Highway and return home on foot. This is because most parents are travelling somewhere other than their home after dropping their child off, hence the need for child care.
- Parking on-street on Salisbury Highway is prohibited by way of a ‘yellow line’.
- The proposal provides sufficient on-site car parking to enable parents to safely drop off and pick up their children.

- An existing pedestrian crossing is situated approximately 30 metres south of the subject land that allows pedestrians to safely and conveniently cross Salisbury Highway.

#### *Safety*

Some of the representors have raised concern about the safety of children given the location of the proposal on Salisbury Highway. This is, respectfully, a matter for the proponent and is not a town planning matter.

The following management and design considerations have been applied to the proposal to minimise risk:

- No child will be able to leave the premises without a parent or registered guardian.
- One front door to the building is proposed adjacent a reception area which is permanently staffed during opening hours. This means that no child will be able to leave the premises unaccompanied by a parent or registered guardian.
- The front door is electronically controlled and cannot be opened by a child.
- The front door of the building is intentionally inset to enable a sheltered pedestrian area rather than children walking directly into the car parking area.
- Low level, open fencing is proposed along the Salisbury Highway frontage along with landscaping. This will minimise the chance of a child running directly onto Salisbury Highway if they manage to escape their guardian after leaving the building.
- All outdoor play areas as well as the staff courtyard will be appropriately fenced to prevent child access to surrounding roads. All fencing will be maintained to a suitable standard to prevent risk to children.
- Up to 16 staff members will be present on-site at any one time. The number of staff members present will be in line with the relevant legislative requirements.

Safety of children and parents is of utmost importance and the proposal has been designed, and will be managed, accordingly. It follows that many child care centres in South Australia and other states in Australia do occur on main roads.

#### *Noise*

Some of the representations have raised concerns about noise from the proposal.

General Section, Interface between Land Uses, Principle 7 states:

- 7 Development that emits noise (other than music noise) should include noise attenuation measures that achieve the relevant Environment Protection (Noise) Policy criteria when assessed at the nearest existing noise sensitive premises.**

Any noise generated from the site will be limited to:

- Children playing in the outdoor play areas.
- Car parking activity (people talking as they vacate or approach their vehicles, the opening and closing of vehicle doors, vehicles starting, vehicles idling, and vehicles moving into and accelerating away from their park position).
- Mechanical plant equipment (air conditioners).

The applicant has engaged Sonus (acoustic engineers) to undertake an Environmental Noise Assessment.

The Environmental Noise Assessment notes the following:

- Outdoor play areas will not be used prior to 7am.
- Noise from children is specifically excluded from the Environment Protection (Noise) Policy 2007 which Principle 7 refers to. Predicted noise levels from children playing have therefore been compared against the recommendations of the Guidelines for Community Noise published by the World Health Organisation (WHO) with regard to annoyance during the day (7am and 10pm).

To ensure compliance with relevant noise criteria, the following acoustic treatments are recommended by Sonus and will be applied to the development:

- Acoustic absorption material be applied to the south-western veranda in the 'outdoor play area'.
- Acoustic insulation be applied to particular walls of the building.
- Boundary fencing to 'outdoor play areas' be airtight sealed at all junctions.
- Install particular barriers/boundary fences to particular heights.
- The storage shed be constructed from minimum BMT Colorbond.
- Mechanical equipment be located adjacent the south-western corner of the building.

On this basis, Sonus concludes:

*"An environmental noise assessment has been made for the proposed early learning centre located at 158-162 Salisbury Highway, Salisbury. The assessment has considered noise at existing residences in the vicinity, from children playing in outdoor areas, car park activity and mechanical plant operation.*

*Relevant assessment criteria have been established based on the Salisbury Council Development Plan, Environment Protection (Noise) Policy 2007 and the World Health Organisation recommendations to protect against annoyance. Acoustic barriers and absorption have been recommended in order to achieve these criteria.*

*...the development will minimise adverse impact and conflict between land uses and not detrimentally affect the amenity of the locality or cause unreasonable interference through noise, thereby achieving the relevant provisions of the Salisbury Council Development Plan".*

Therefore, the proposal satisfies Principle 7 and the relevant Australian Standards.

#### *Property Values*

Some of the representors have indicated that the proposal will reduce property values.

While the representors are entitled to their own views about property values, section 38(6)(d) of the Development Act 1993 provides that any representation "must be limited to... what should be the decision of the relevant authority as to development plan consent."

The issue of property value is not a matter addressed by the Development Plan. Further, the Environment, Resources and Development Court held that a fear of, or actual reductions in, property value are rarely relevant in deciding whether or not to grant development plan consent to a development application. In this respect, the comments made by Commissioner Hutchings in *Meyer v Northern Areas Council & Broughton Hills* are instructive:

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*“The issue of property value is one brought to the Court from time to time... The history of town and country planning is marked by numerous attempts to relate changes in property values to planning initiatives... The development plans applicable to the regions and districts of South Australia do not refer to land values except in the most oblique and inferential way here and there and demonstration of any link, if it is possible, could only be made by very specialised evidence from a range of experts including planners, valuers, land economists and the like.”*

There are no specific references to property or land value in the Development Plan, nor have the representors provided specialised evidence from experts such as planners, valuers and/or economists on the matter.

In the circumstances, any comment in relation to impact on property values in this matter should not be factored into the planning assessment.

#### *Economic Benefit*

One of the representors have stated *“Another child care centre will be good for this area and excellent to see that more employment is coming for Salisbury”*.

As previously explained, the proposal will employ up to 16 staff members at any one time while also providing additional jobs during the construction phase. This will have a direct benefit to the local economy.

#### *Conclusion*

I trust I have addressed the concerns raised by the representors in sufficient detail. I confirm that I will be available to appear at the CAP (Council Assessment Panel) meeting to respond to the verbal representations.

In the meantime, we look forward to your support acknowledging that this application displays, in my view, a high degree of planning merit in order to warrant Development Plan Consent.

Please call me on 8333 7999 if you have any questions in respect of this matter.

Yours sincerely



Philip Harnett  
Associate





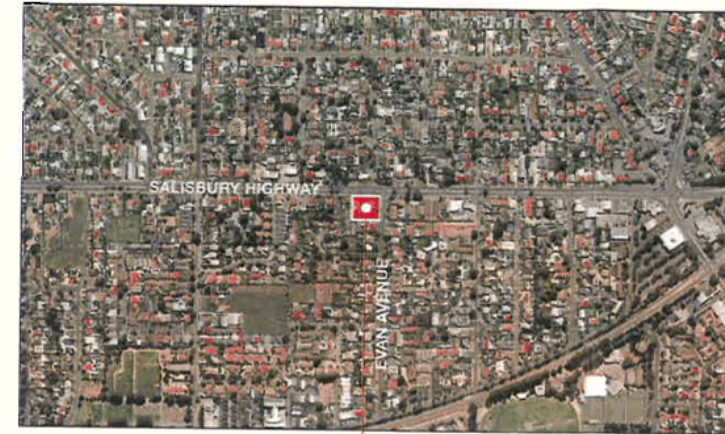
3522

# SALISBURY EARLY LEARNING CENTRE

## DRAWING SCHEDULE

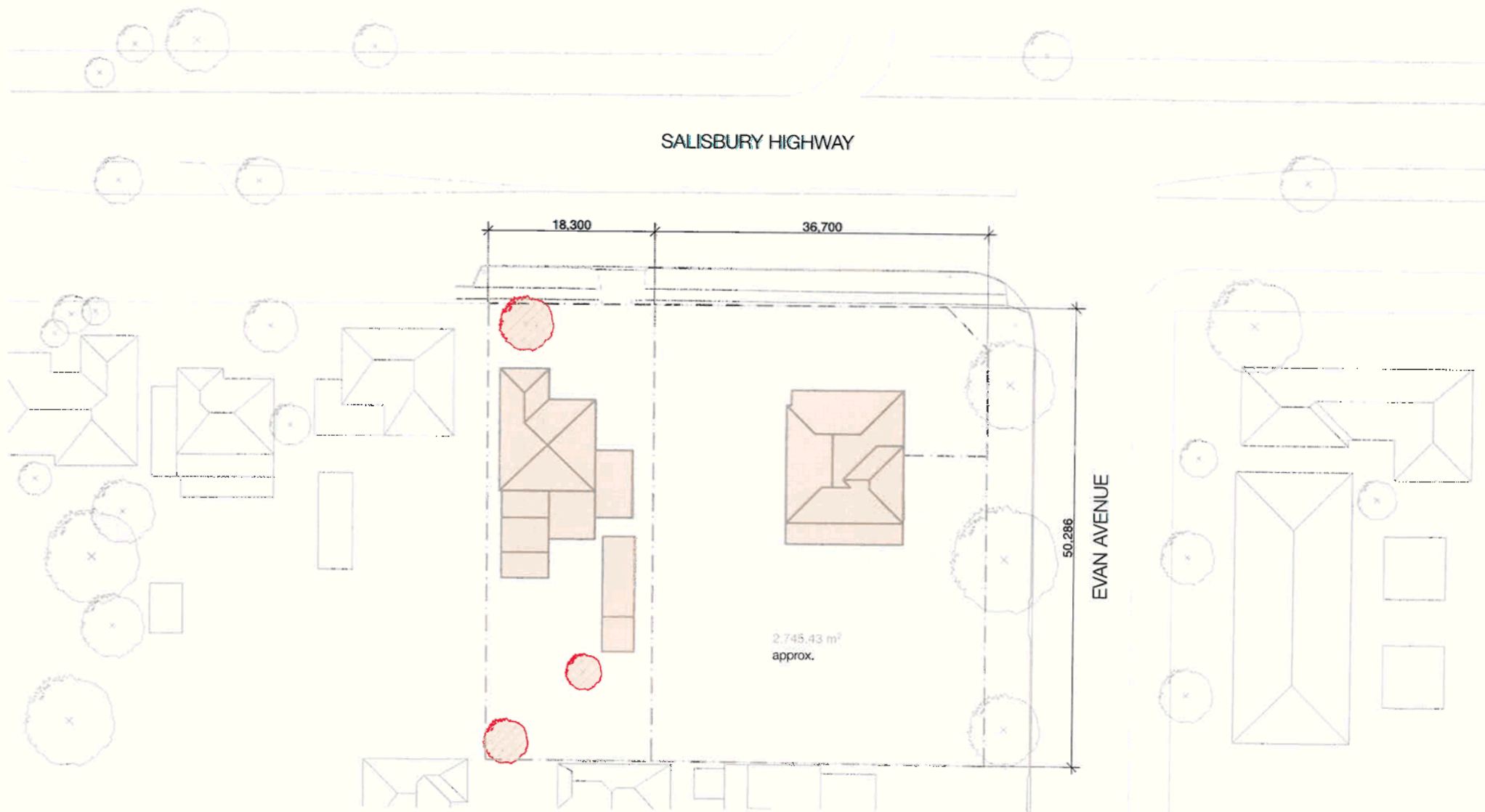
**ARCHITECTURAL** PREPARED BY **FLIGHTPATH ARCHITECTS**

- SK01 B LOCATION AND SITE PLAN
- SK02 B PROPOSED SITE PLAN
- SK03 B PROPOSED ROOF PLAN
- SK04 B ELEVATIONS 01
- SK05 B ELEVATIONS 02
- SK06 B STREETScape ELEVATIONS AND RENDERS
- SK07 B SIGNAGE DETAILS



158-162 SALISBURY HIGHWAY, SALISBURY

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**SITE PLAN LEGEND**

	EXISTING TREES TO BE RETAINED
	EXISTING TREE TO BE REMOVED
	EXISTING SITE EXTENT: 158-162 SALISBURY HIGHWAY
	DENOTES DEMOLITION

**FOR PLANNING**

B	PLANNING AMENDMENT	SKD	06/11/18
A	PLANNING APPLICATION	SKD	26/09/18
	Issue	checked	date

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project  
**SALISBURY EARLY LEARNING CENTRE**  
address  
158-162 Salisbury Highway, Salisbury,  
Adelaide SA 5106 Australia

scale  
As shown @ A3 drawn  
KM

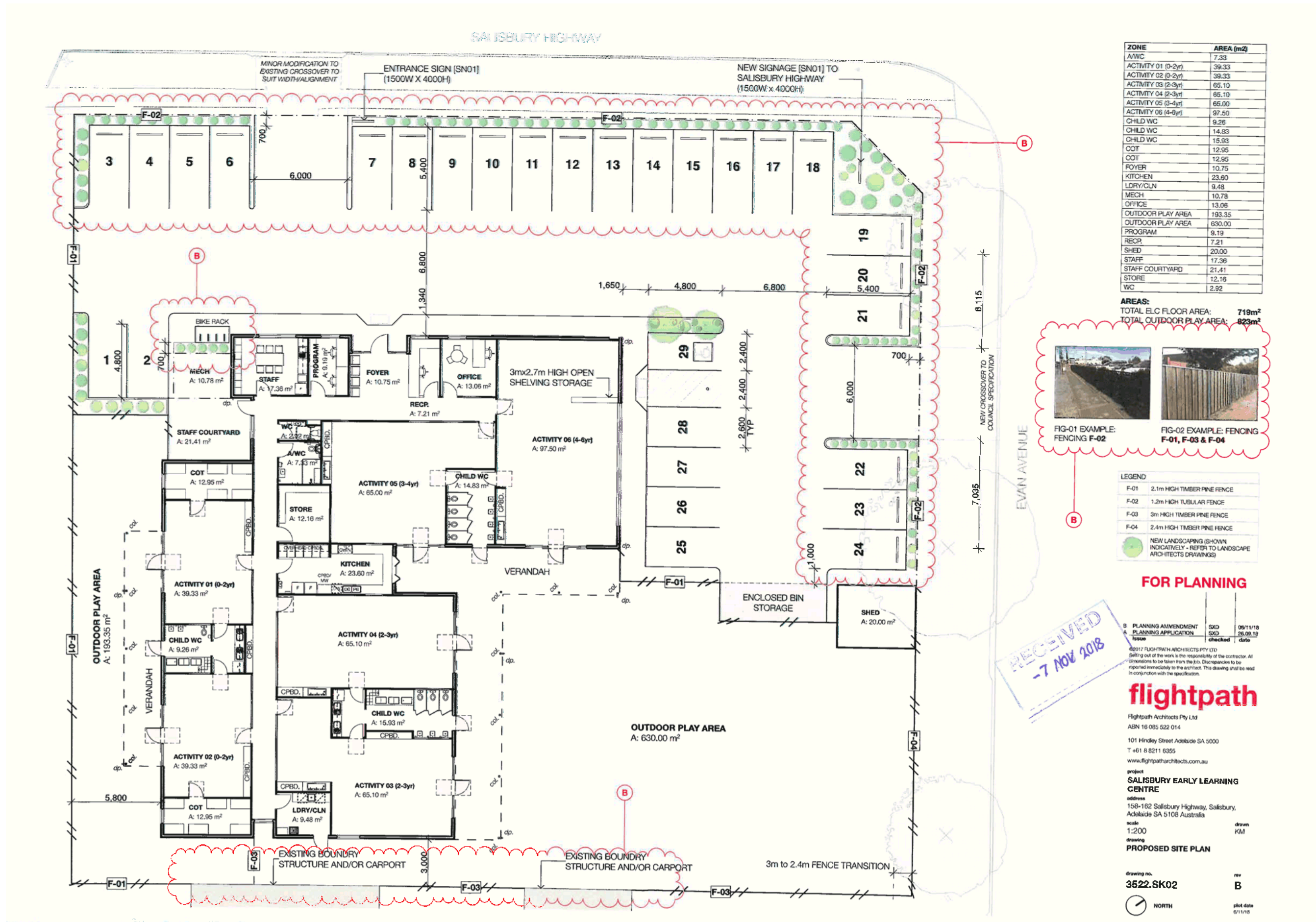
drawing  
**LOCATION AND SITE PLAN**

drawing no.  
**3522.SK01** rev  
**B**

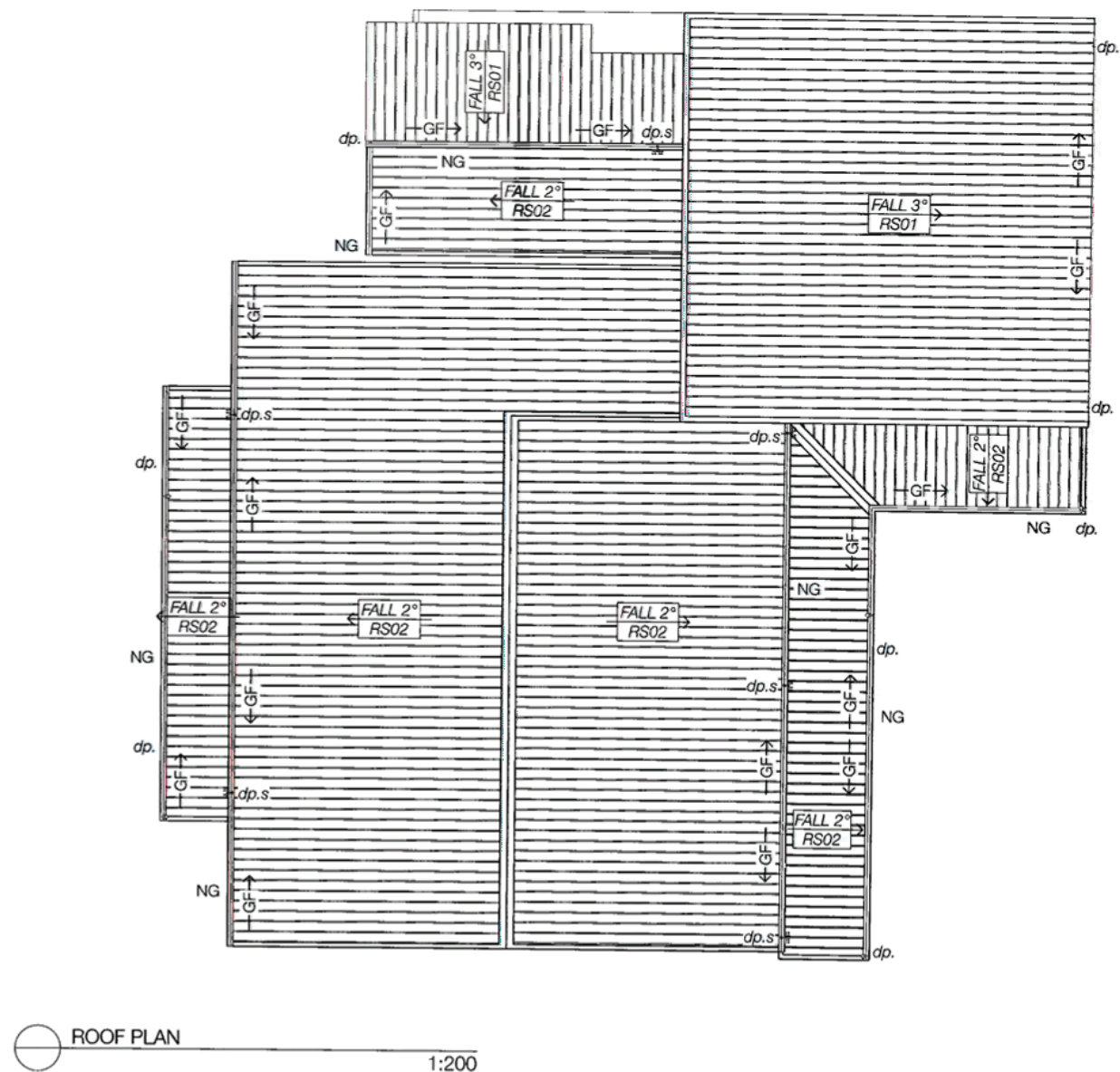
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01/18

DEMOLITION: SITE PLAN 1:500

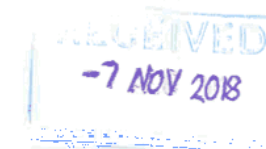








ROOF PLAN 1:200



ROOF PLAN LEGEND	
FALL 3°	ROOF PITCH
RS01	ROOF MATERIAL
RS01	COLORBOND STEEL PROFILED CLADDING, COLOUR: MONUMENT
RS02	REVOLUTION ROOFING - REV 5 ROOF SHEETING, COLOUR: WOOLAND GREY
col.	COLUMN, REFER ENGINEERING.
dp.	DOWNPIPE
dp.s	DOWNPIPE WITH SPREADER
NG	NEW COLORBOND GUTTER
GF	DIRECTION OF GUTTER FALL

**FOR PLANNING**

issue	checked	date
B PLANNING AMENDMENT	SKD	06/11/18
A PLANNING APPLICATION	SKD	26.09.18

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project  
**SALISBURY EARLY LEARNING CENTRE**  
 address  
 158-162 Salisbury Highway, Salisbury, Adelaide SA 5108 Australia

scale  
 1:200  
 drawing  
**PROPOSED ROOF PLAN**

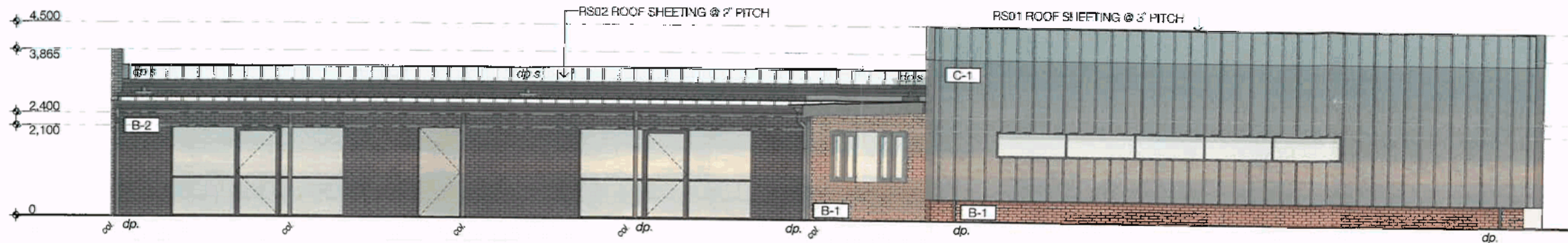
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**3522.SK03**

rev  
**B**

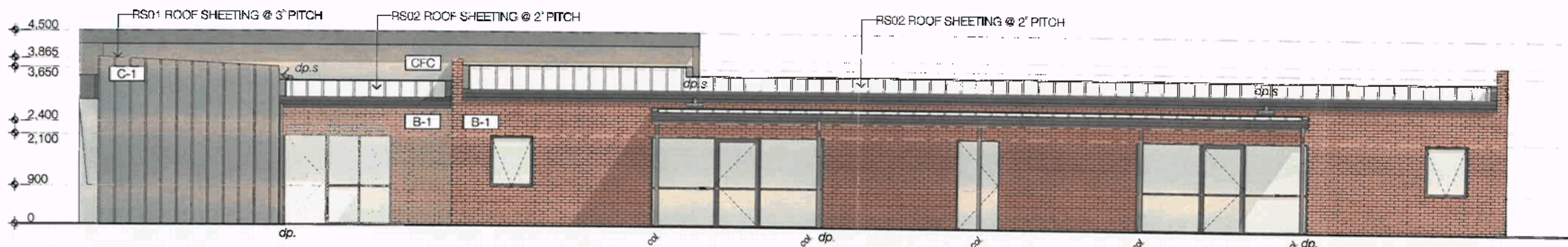
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 01/11/18







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LEGEND

B-1	RED BRICK
B-2	DARK BRICK
C-1	COLORBOND STEEL PROFILED CLADDING, COLOUR: MONUMENT
CFC	CEMENT FIBRE SHEETING, COLOUR: WHITE
RS01	COLORBOND STEEL PROFILED CLADDING, COLOUR: MONUMENT
RS02	REVOLUTION ROOFING - REV 5 ROOF SHEETING, COLOUR: WOODLAND GREY
dp.	DOWNPIPE
dp.s	DOWNPIPE WITH SPREADER
col.	COLUMN, REFER ENGINEERING.

FOR PLANNING

B	PLANNING AMENDMENT	SKD	06/11/18
A	PLANNING APPLICATION	SKD	25/09/18
	Issue	checked	date

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project  
**SALISBURY EARLY LEARNING CENTRE**  
address  
158-162 Salisbury Highway, Salisbury, Adelaide SA 5108 Australia

scale  
As shown @ A3 KM  
drawing  
**ELEVATIONS 01**

drawing no.  
**3522.SK04** rev  
**B**

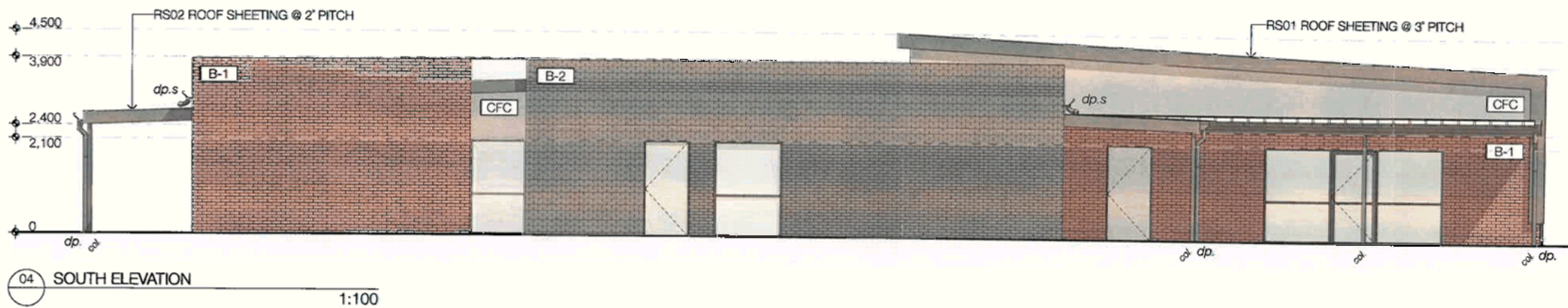
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LEGEND

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B-2	DARK BRICK
C-1	COLORBOND STEEL PROFILED CLADDING, COLOUR: MONUMENT
CFC	CEMENT FIBRE SHEETING, COLOUR: WHITE
RS01	COLORBOND STEEL PROFILED CLADDING, COLOUR: MONUMENT
RS02	REVOLUTION ROOFING - REV 5 ROOF SHEETING, COLOUR: WOODLAND GREY
dp.	DOWNPIPE
dp.s	DOWNPIPE WITH SPREADER
col.	COLUMN, REFER ENGINEERING.

FOR PLANNING

B	PLANNING AMENDMENT	SKD	06/11/18
A	PLANNING APPLICATION	SKD	28.09.18
	Issue	checked	date

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project  
**SALISBURY EARLY LEARNING CENTRE**  
address  
158-162 Salisbury Highway, Salisbury, Adelaide SA 5108 Australia

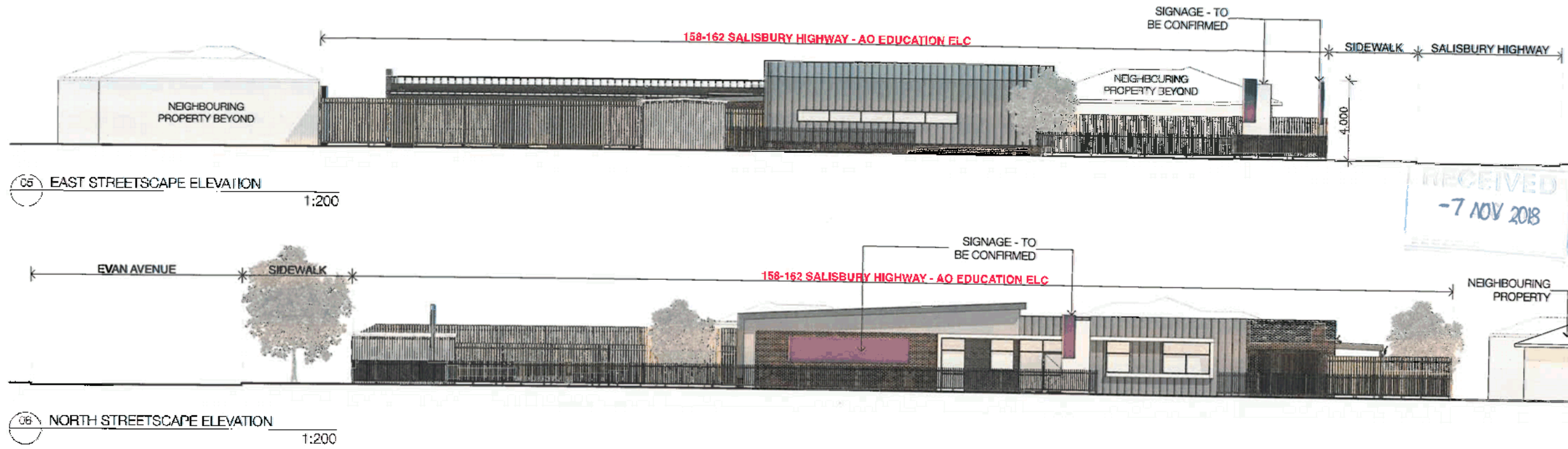
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drawing  
**ELEVATIONS C2**

drawing no.  
**3522.SK05** rev  
**B**

plot date  
8/11/18







VIEW FROM SALISBURY HIGHWAY



VIEW FROM SALISBURY HIGHWAY

**FOR PLANNING**

B PLANNING AMENDMENT	SKD	09/11/18
A PLANNING APPLICATION	SKD	25/09/18
Issue	checked	cbte

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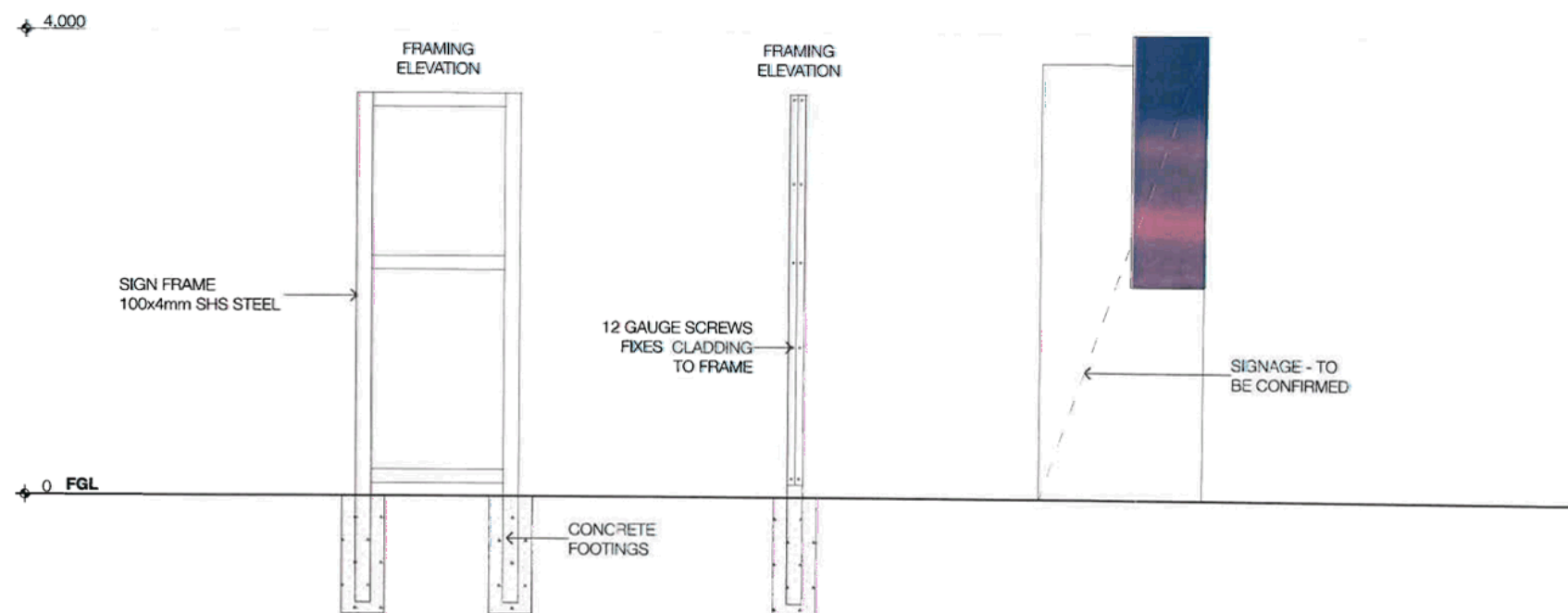
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project  
**SALISBURY EARLY LEARNING CENTRE**  
 address  
 158-162 Salisbury Highway, Salisbury, Adelaide SA 5108 Australia  
 scale  
 As shown @ A3 KM  
 drawing  
**STREETSCAPE ELEVATIONS AND RENDERS**

drawing no.  
**3522.SK06** rev  
**B**

NORTH  
 plot date  
 5/11/18





01 ELEVATION [SN01] 1:50

**NOTE**  
FINAL SIGNAGE SELECTIONS TO BE CONFIRMED.

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-7 NOV 2018

**FOR PLANNING**

B PLANNING AMENDMENT	SKD	06/11/18
A PLANNING APPLICATION	SKD	26/09/18
Issue	checked	date

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Setting out of the work is the responsibility of the contractor. All dimensions to be taken from the job. Discrepancies to be reported immediately to the architect. This drawing shall be read in conjunction with the specification.

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**project**  
SALISBURY EARLY LEARNING CENTRE  
**address**  
158-162 Salisbury Highway, Salisbury, Adelaide SA 5108 Australia

**scale**  
As shown @ A3 KM  
**drawing**  
SIGNAGE DETAILS

**drawing no.**  
3522.SK07 **rev**  
B

**plot date**  
01/11/18





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# SALISBURY EARLY LEARNING CENTRE 06/11/2018 REV C

## LANDSCAPE CONCEPT PLANS

- 18045 SK01 CONCEPT PLAN
- 18045 SK02 PLANTING PLAN
- 18045 SK03 REFERENCE IMAGES
- 18045 SK04 PLANT PALETTE



BIRDSEYE : STUDIOS







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**SALISBURY EARLY LEARNING CENTRE  
CONCEPT PLAN**

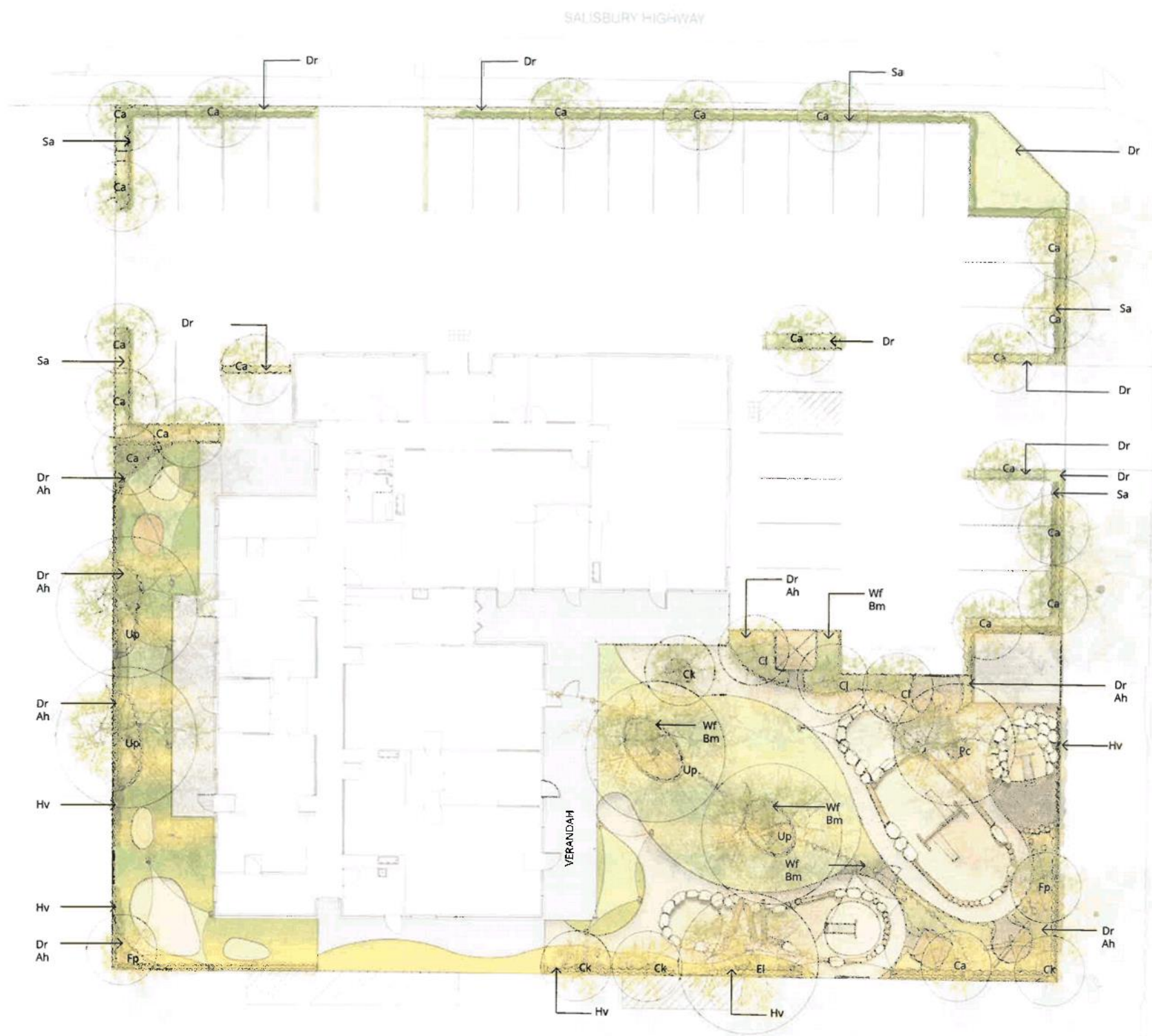
BIRDSEYE STUDIOS LANDSCAPE ARCHITECTS ACN 603 475 332 contact@birdseyestudios.com.au birdseyestudios.com.au

**BIRDSEYE : STUDIOS**

0 5 10M  
1:250 @ A3







PLANT SCHEDULE

KEY	BOTANICAL NAME	COMMON NAME	INSTALLATION SIZE
<b>TREES</b>			
Ca	Cupaniopsis anacardioides	Tuckeroo	45 Litre
Ck	Callistemon 'Kings park special'	Bottlebrush	45 Litre
Cl	Citrus Lemon 'Meyer'	Meyer Lemon	45 Litre
Ei	Eucalyptus leucoxylon ssp. leucoxylon	SA Blue gum	45 Litre
Fp	Ficus platypoda	Rock Fig	45 Litre
Pc	Pistacia chinensis	Chinese pistachio	45 Litre
Up	Ulmus parvifolia	Chinese elm	45 Litre
<b>SHRUBS AND GROUNDCOVERS</b>			
Bm	Brachyscome multifida	Cut Leaf Daisy	140mm
Dr	Dianella revoluta	Spreading Fax Lily	140mm
Ah	Anigozanthos 'Bush gem'	Kangaroo paw	140mm
Sa	Syzygium australe	Lilly Pilly	140mm
Wf	Westringia fruticosa 'Jervis gem'	Native Rosemary	140mm
<b>CLIMBER</b>			
Hv	Hardenbergia violacea	Native Lilac	140mm

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SALISBURY EARLY LEARNING CENTRE  
PLANTING PLAN

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1:250 @ A3



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Shade Sail over Sand Pit



Organic Fence



Timber Post - To be placed along Fence & through Playground



Stepping Logs



Timber Logs



Timber Teepee



Rock mound with slide



Synthetic turf

SALISBURY EARLY LEARNING CENTRE  
REFERENCE IMAGES

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TREES



Callistemon 'Kings park special'



Cupaniopsis anacardioides



Eucalyptus leucoxylon ssp. leucoxylon

SALISBURY EARLY LEARNING CENTRE  
PLANT PALETTE

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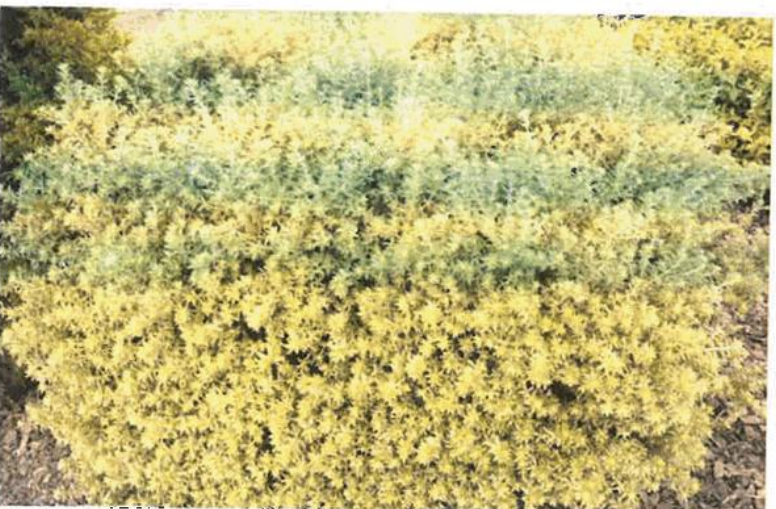
SHRUBS & GROUND COVERS



Dianella revoluta



Syzygium australe



Westringia fruticosa



Hardenbergia violacea



Anigozanthos 'Bush gem'



Brachyscome multifida

BIRDSEYE : STUDIOS

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# Early Learning Centre

158-162 Salisbury Highway, Salisbury

Environmental Noise Assessment

November 2018

S5660C3

# sonus.

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**INTRODUCTION**

An environmental noise assessment has been made of the proposed early learning centre located at 158-162 Salisbury Highway, Salisbury.

Preschools, schools, childcare centres and playgrounds are often located immediately adjacent to residences and the sound of children playing during the day is rarely of concern. However, in some situations, where adjacent residents are sensitive to the sound of children’s voices, the noise can be annoying and can exceed objective noise criteria. For the purposes of this assessment, it has been assumed that the residences in the vicinity of the proposed development are sensitive to the sound of children’s voices.

The closest noise sensitive receivers to the development are the residences located to the south of the subject site. Residences are also located to the east on the opposite side of Evan Avenue and to the north on the opposite side of the Salisbury Highway. Appendix A shows the relative location of the nearby residences.

The assessment considers noise levels at the surrounding residences from children playing in outdoor areas, car park activity and mechanical plant operation.

The assessment has been based on the following;

- *Flightpath Architects* drawings for “SALISBURY EARLY LEARNING CENTRE” (project number “3552”) including “SK01” through “SK07” revision “B” dated 5 November 2018;
- The number of children as provided in email correspondence “Early Learning Centre” dated 28 August 2018; and
- Outdoor play areas not being used prior to 7am.



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#### DEVELOPMENT PLAN

The proposed development and surrounding residences are located within a Residential Zone of the Salisbury Council Development Plan<sup>1</sup>. The Development Plan has been reviewed and particular regard given to the following relevant provisions.

#### **General Section – Interface Between Land Uses**

- Objective 1: Development located and designed to minimise adverse impact and conflict between land uses.*
- Objective 2: Protect community health and amenity from adverse impacts of development.*
- Objective 3: Protect desired land uses from the encroachment of incompatible development.*

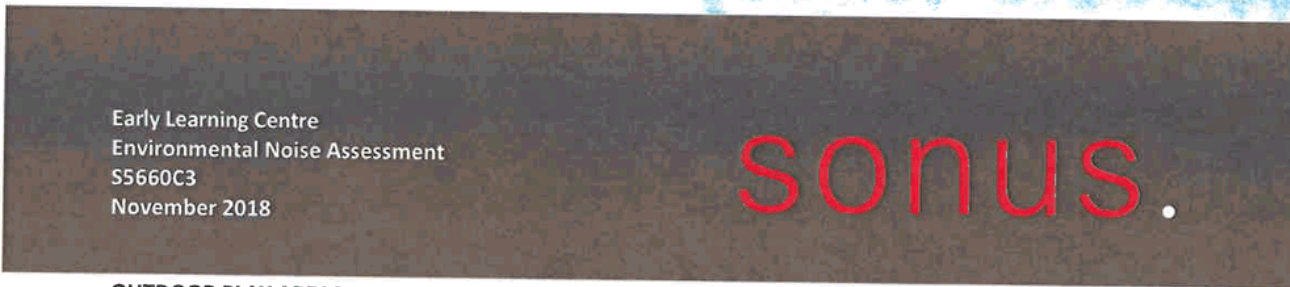
#### PRINCIPLES OF DEVELOPMENT CONTROL

1. *Development should not detrimentally affect the amenity of the locality or cause unreasonable interference through any of the following:*  
  
....  
*(b) Noise*  
  
....
2. *Development should be sited and designed to minimise negative impact on existing and potential future land uses desired in the locality.*
5. *Sensitive uses likely to conflict with the continuation of lawfully existing developments and land uses desired for the zone should be designed to minimise negative impacts.*
6. *Non-residential development on land abutting a residential zone should be designed to minimise noise impacts to achieve adequate levels of compatibility between existing and proposed uses.*

#### *Noise Generating Activities*

7. *Development that emits noise (other than music noise) should include noise attenuation measures that achieve the relevant "Environment Protection (Noise) Policy" criteria when assessed at the nearest noise sensitive premises.*
8. *Development with the potential to emit significant noise (e.g. industry) should incorporate noise attenuation measures that prevent noise from causing unreasonable interference with the amenity of noise sensitive development.*

<sup>1</sup> Consolidated 15th December 2016



**OUTDOOR PLAY AREAS**

**Criteria**

The Development Plan makes reference to the *Environment Protection (Noise) Policy 2007* (the Policy); however, for the reasons outlined above, the noise from children playing is specifically excluded from assessment under the Policy. Predicted noise levels from children playing have therefore been compared against the recommendations of the *Guidelines for Community Noise* published by the World Health Organisation (WHO) with regard to annoyance during the day (7am and 10pm).

The WHO guidelines include:

*“To protect the majority of people from being seriously annoyed during the daytime, the sound pressure level on balconies, terraces and outdoor living areas should not exceed 55 dB L<sub>Aeq</sub> for a steady continuous noise. To protect the majority of people from being moderately annoyed during the daytime, the outdoor sound pressure level should not exceed 50 dB L<sub>Aeq</sub>.”*

Based on the above, it is proposed that the average L<sub>Aeq</sub> noise levels over the WHO day time period from children playing at the centre be no greater than 50 dB(A) at surrounding noise sensitive locations.



**Assessment**

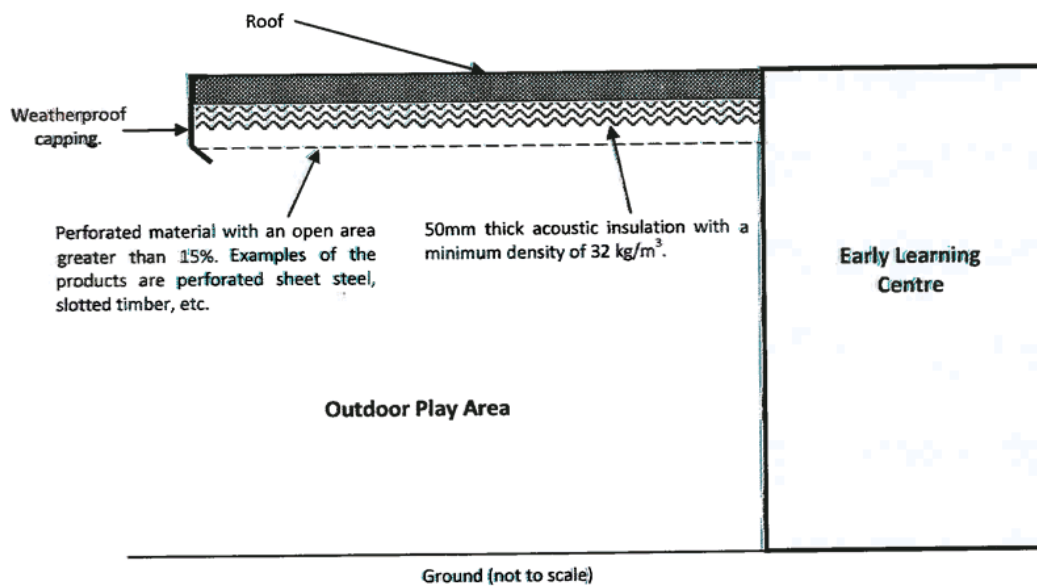
The noise from children of different age groups in outdoor areas has previously been measured at similar early learning centres. Based on these measurements, noise from the proposed early learning centre has been predicted.

The sound power levels for children of different age groups playing are provided as Appendix B.

Based on the predictions, in order to achieve the WHO assessment criterion, the following acoustic treatment measures are recommended;

0 to 2 year old verandah

- Ensure the south-western verandah is installed with acoustic absorption material, such as insulation of minimum thickness 50mm and density 32kg/m<sup>3</sup> to the full extent of the underside in accordance with the following details. Alternatively, a proprietary weather proof acoustic absorption product may be used, such as “Pyrotek Reapor” or “Stratocell Whisper”.



Storage Shed

- The Storage shed is recommended to be constructed from minimum 0.36 BMT 'Colorbond' (or similar) with an apex of no less than 2.1m high for the extent shown in the current drawings of the proposal.



Boundary Fence to outdoor play areas

- Ensure an airtight seal is achieved at all junctions, including at the ground, the continuation from other fences, with the early learning centre, and with other existing structures for the proposed fence construction comprising timber (pine) paling. It is noted that to ensure an airtight construction, an overlapping system with multiple screw fixings is expected. Alternatively, a minimum 0.42 BMT sheet steel ("Colorbond" or similar) or a material with the same or greater surface density ( $\text{kg/m}^2$ ) can be installed. Barriers should be constructed to the specified heights as detailed in Figure 1 for the extent shown. Existing structures along the southern site boundary may be used to form part of the barrier for the extent highlighted in PURPLE in Figure 1 from an acoustic perspective (although other aspects of these junctions might need to be confirmed by others). It is noted that a transition piece from 3m to 2.4m can be used in the south-eastern corner. The transition piece should not extend more than 1m into the 3m high fence, as shown in Figure 1 in PINK.

With the above recommendations in place, predictions indicate that the equivalent noise level from children playing at the centre will be no greater than 48 dB(A) at the nearby residences when assessed against the day time WHO assessment period. The noise will therefore achieve the WHO guidelines criterion.

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#### CAR PARK ACTIVITY & MECHANICAL PLANT

##### Criteria

Principle of Development Control 7 from the Development Plan references the *Environment Protection (Noise) Policy 2007*, which provides goal noise levels to be achieved at residences from general activity at a site.

The Policy is based on the World Health Organisation Guidelines to prevent annoyance, sleep disturbance and unreasonable interference on the amenity of an area when adjusted to account for the particular noise source and its location. Therefore, for noise generated by car park activity and the operation of mechanical plant, compliance with the Policy is considered to be sufficient to satisfy all provisions of the Development Plan relating to environmental noise from these sources.

##### General Activity

The Policy provides goal noise levels to be achieved at residences based on the principally promoted land use of the Development Plan Zones in which the noise source (early learning centre) and the noise receivers (the residences) are located. Based on the residential zone and the "development" nature of the project, the following goal noise levels are provided by the Policy to be achieved at residences:

- an average ( $L_{eq}$ ) noise level of 47 dB(A) during the day (7am to 10pm);
- an average ( $L_{eq}$ ) noise level of 40 dB(A) during the night (10pm to 7am); and,
- a maximum ( $L_{max}$ ) noise level of 60 dB(A) at night.

When measuring or predicting noise levels for comparison with the Policy, penalties may be applied to the average goal noise levels for each characteristic of tone, impulse, low frequency and modulation of the noise source. To apply a penalty, the characteristic must be considered dominant in the existing acoustic environment. In some instances the modulating nature of car park activity would result in a 5 dB(A) penalty being applied to the site, however given the influence of Salisbury Highway traffic at the closest residences to the car park, a penalty is not considered applicable.

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#### Assessment

The noise level at the nearby residences from the proposed site activity other than children playing has been predicted based on a range of previous noise measurements which include:

- general car park activity such as people talking as they vacate or approach their vehicles, the opening and closing of vehicle doors, vehicles starting, vehicles idling, and vehicles moving into and accelerating away from their park position;
- vehicle movements on site; and,
- mechanical plant serving the building.

The sound power levels for the above activities and equipment are provided as Appendix B.

The predictions have been based on the following operational assumptions for the level of activity in any 15-minute<sup>2</sup> period:

- Day Time (7am to 10pm)
  - Car parking including doors closing and conversation in 15 car parks;
  - 10 vehicle movements into or out of the car park; and,
  - Continuous operation of mechanical plant.
- Night Time (before 7am)
  - Car parking including doors closing and conversation in 8 car parks;
  - 5 vehicle movements into or out of the car park; and,
  - Continuous operation of mechanical plant.

As is typical at the development application stage of a project, the proposed mechanical plant has not yet been designed or selected. The assessment of the mechanical plant has therefore been based on previous noise measurements and manufacturer data for plant and equipment at similar facilities. The subsequent overall sound power level data for two air conditioning units are provided in Appendix C. The noise level and any acoustic treatment associated with mechanical plant should be reviewed during the detailed design phase, should the final equipment selections have different sound power levels or should a different number of units be proposed to those specified within this report.

Based on the predicted noise levels, the following acoustic treatments are recommended to achieve the goal noise levels of the Policy:

<sup>2</sup> Default assessment period of the Policy.

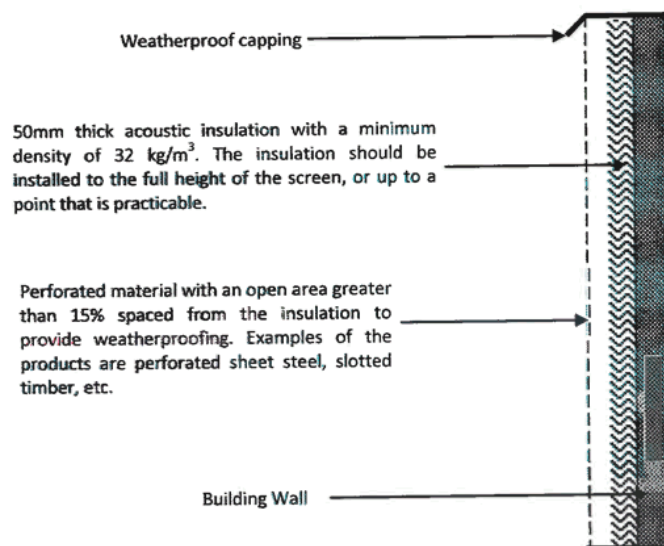


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Acoustic absorption on mech area wall

- Locate all mechanical plant within the designated mechanical plant area shown in **PURPLE** in Figure 1.
- Install 50mm thick acoustic insulation with a minimum density of  $32 \text{ kg/m}^3$  to the early learning centre building wall for the extent shown in Figure 1 as **ORANGE**. The insulation should extend to a minimum height of 1.0m above the tallest mechanical plant unit. The installation should be in accordance with the following detail;



Boundary Fence to car park

- Construct a 2.1m barrier along the south-western boundary of the car park for the extent detailed in Figure 1.
- Ensure an airtight seal is achieved at all junctions, including at the ground for the proposed fence construction of airtight timber (pine) overlapping paling. Alternatively, a minimum 0.42 BMT sheet steel ("Colorbond" or similar) or a material with the same or greater surface density ( $\text{kg/m}^2$ ) can be installed.

With the recommended acoustic treatments in place, the predicted noise levels at residences in the vicinity due to car park activity and mechanical plant operation achieve the Policy goal noise levels. That is, noise levels are predicted to be no more than 44 dB(A) during the day and 39 dB(A) at night.

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The instantaneous maximum noise levels from vehicle activity on the site have been predicted at all residences within the Residential Zone. Predicted maximum noise levels have been based on measurements at a variety of similar sites. Maximum noise levels from the assessed activities are no greater than 60 dB(A) at all dwellings within the vicinity, therefore satisfying the Policy requirements.



**CONCLUSION**

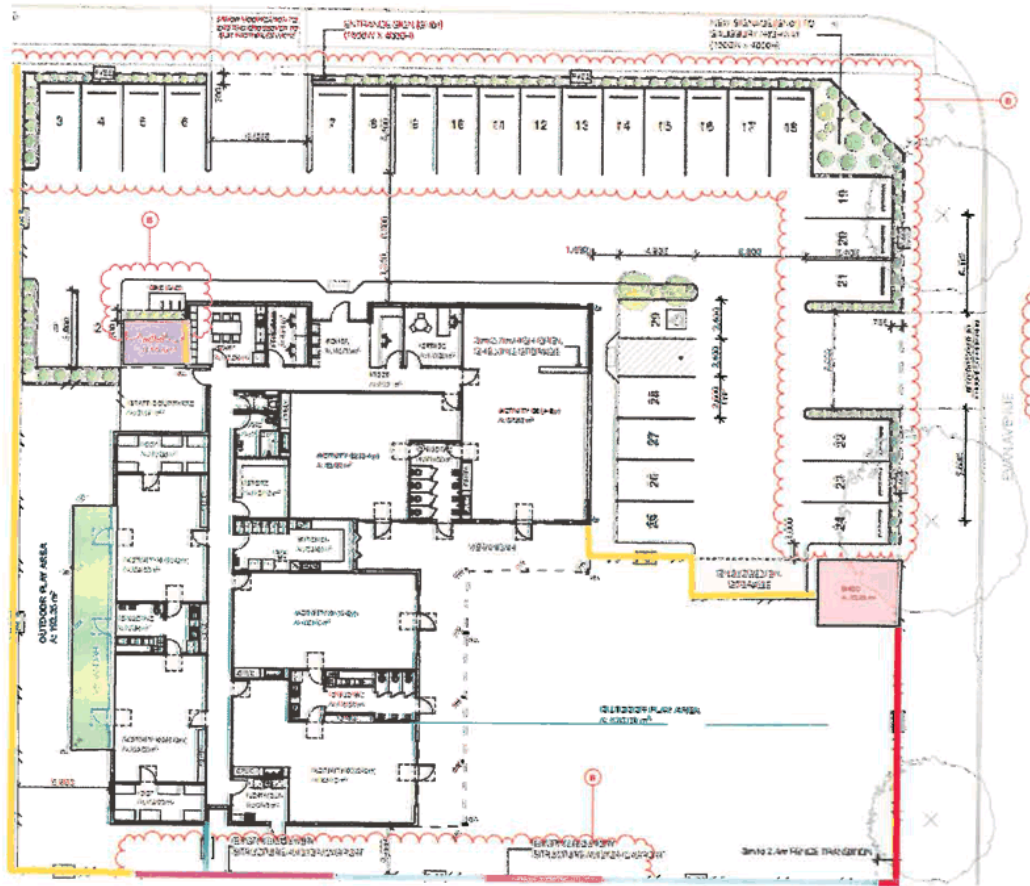
An environmental noise assessment has been made for the proposed early learning centre located at 158-162 Salisbury Highway, Salisbury. The assessment has considered noise at existing residences in the vicinity, from children playing in outdoor areas, car park activity and mechanical plant operation.

Relevant assessment criteria have been established based on the Salisbury Council Development Plan, *Environment Protection (Noise) Policy 2007* and the World Health Organisation recommendations to protect against annoyance. Acoustic barriers and absorption have been recommended in order to achieve these criteria.

Based on the above, it is considered that the development will *minimise adverse impact and conflict between land uses and not detrimentally affect the amenity of the locality or cause unreasonable interference through noise*, thereby achieving the relevant provisions of the Salisbury Council Development Plan.



Figure 1: Site plan with recommended acoustic treatments.

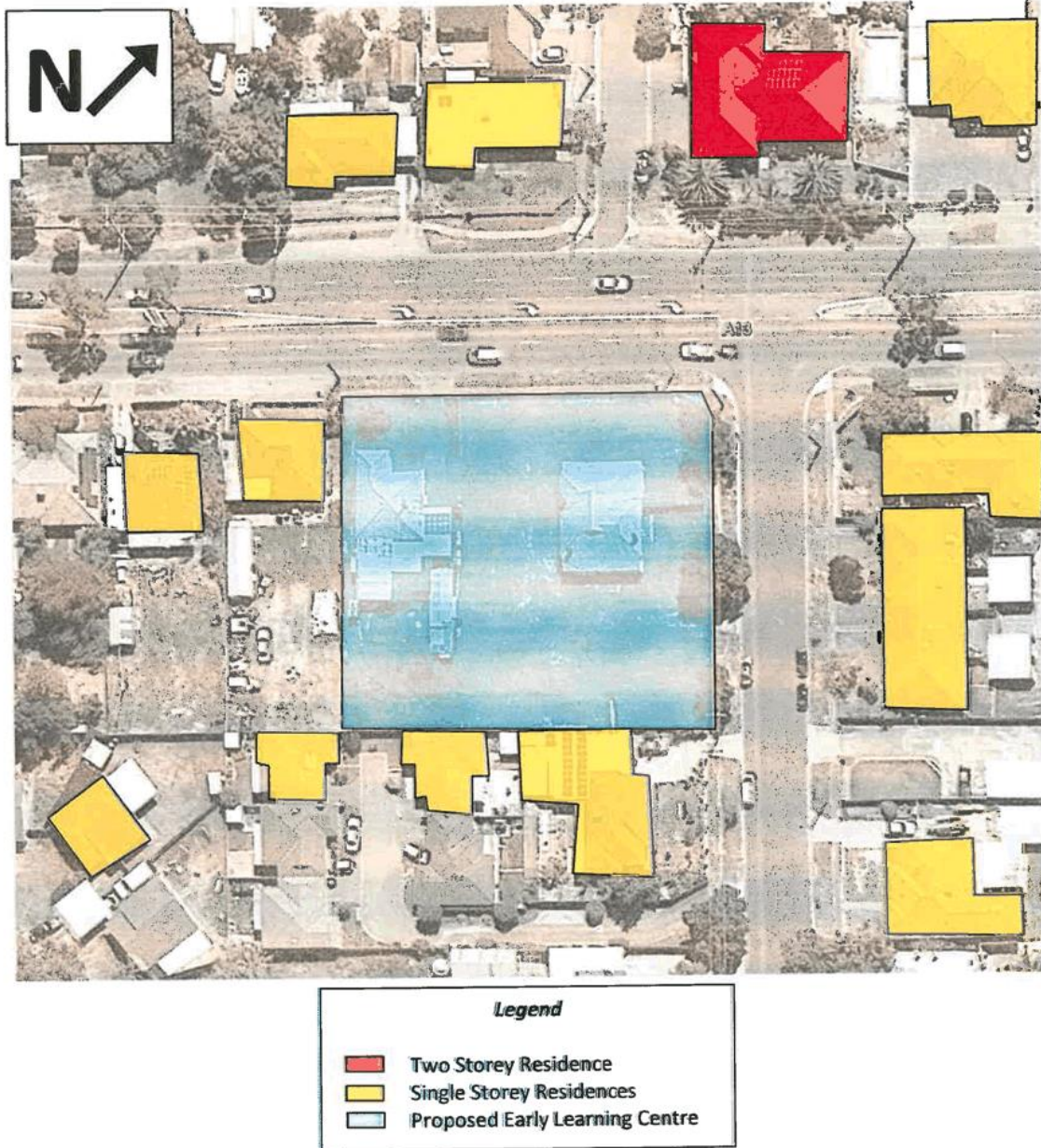


Legend			
	2.1m High Barrier		Acoustic absorption on fence
	2.4m High Barrier		Acoustic Insulation to underside of Veranda
	3.0m High Barrier		Mechanical Plant Location
	Existing boundary structure and/or carport		Storage shed, no less than 2.1m high
			3m to 2.4m transition

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APPENDIX A: Site Locality and Surrounding Residences

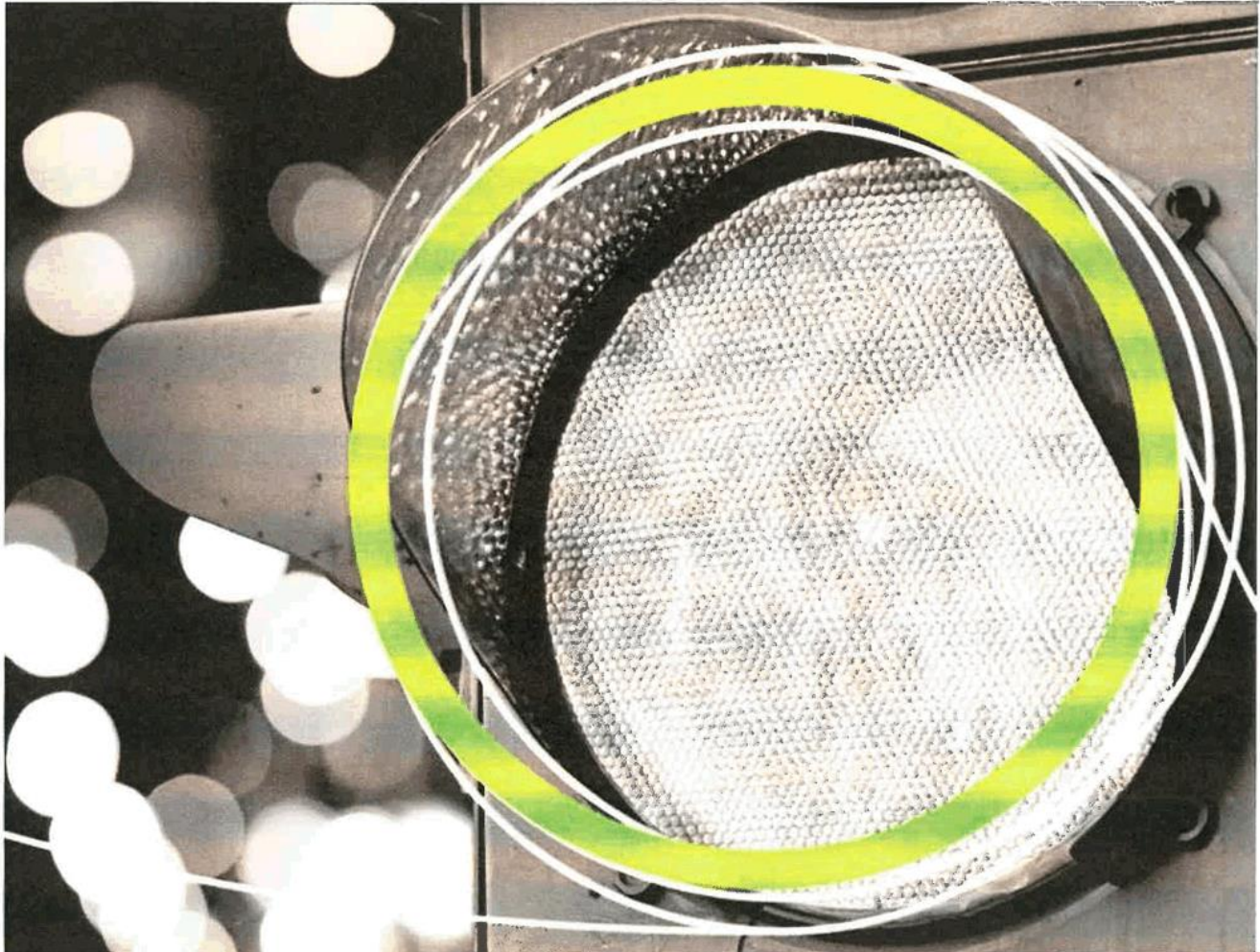




**APPENDIX B: Sound Power Levels**

	Activity	Sound Power Level dB(A)
Outdoor Play Area	Under 2yrs	74
	2yrs - 3yrs	78
	3yrs - 5yrs	80
Car Park	General activity	93
	Vehicle moving	94
Mechanical Plant	A/C Unit	76

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Proposed Childcare Centre  
158-162 Salisbury Highway, Salisbury  
Transport Impact Assessment


**Client //** Flightpath Architects  
**Office //** SA  
**Reference //** S159600  
**Date //** 6/11/2018

Proposed Childcare Centre  
 158-162 Salisbury Highway, Salisbury  
 Transport Impact Assessment

Issue: B 6/11/2018

Client: Flightpath Architects  
 Reference: S159600  
 GTA Consultants Office: SA

**Quality Record**

Issue	Date	Description	Prepared By	Checked By	Approved By	Signed
A	18/09/2018	Final	Ian Bishop	David Kwong	David Kwong	DK
B	6/11/2018	Final – amended	Ian Bishop	David Kwong	David Kwong	

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Proposed Childcare Centre, 158-162 Salisbury Highway, Salisbury



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## 1. Introduction

### 1.1 Background

A child care centre is proposed on a site located at 158-162 Salisbury Highway, Salisbury. The proposed development comprises of a 114 place child care centre and associated staff and visitor car parking within the site.

GTA Consultants was engaged by Flightpath Architects to undertake a transport impact assessment of the proposed development to accompany a development application.

### 1.2 Purpose of this Report

This report sets out an assessment of the anticipated transport implications of the proposed development, including consideration of the following:

- i existing traffic and parking conditions surrounding the site
- ii parking demand likely to be generated by the proposed development
- iii suitability of the proposed parking in terms of supply (quantum) and layout
- iv traffic generation characteristics of the proposed development
- v proposed access arrangements for the site

### 1.3 References

In preparing this report, reference has been made to the following:

- o City of Salisbury Development Plan, consolidated 15 December 2016
- o Roads & Maritime Services (RMS) Technical Direction – TDT2013/04a – Guide to Traffic Generating Developments – Updated Traffic Surveys
- o Australian Standard/ New Zealand Standard, Parking Facilities, Part 1: Off-Street Car Parking AS/NZS 2890.1:2004
- o Australian Standard / New Zealand Standard, Parking Facilities, Part 6: Off-Street Parking for People with Disabilities AS/NZS 2890.6:2009
- o plans for the proposed development prepared by Flightpath Architects
- o various technical data as referenced in this report
- o other documents as nominated.

## 2. Existing Conditions

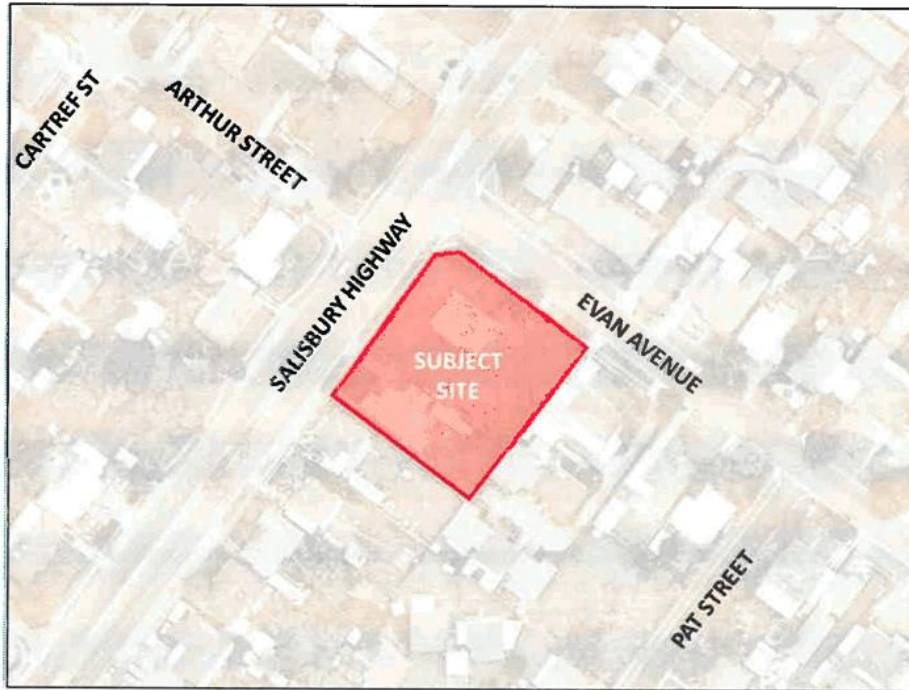
### 2.1 Subject Site

The subject site is located on lots 158 and 162 Salisbury Highway, Salisbury. The subject site is approximately 2,800 square metres and has a frontage of approximately 50 metres to Salisbury Highway and 45 metres to Evan Avenue.

The site is located within a residential zone and is currently occupied by a residential dwelling. The adjacent properties are primarily residential.

The location of the subject site and the surrounding environs is shown in Figure 2.1.

Figure 2.1: Subject Site and its Environs



(PhotoMap courtesy of NearMap Pty Ltd)

There is an existing crossover onto Salisbury Highway at the western side of the site from Lot 162 and an existing crossover at the eastern side of the site from lot 158 onto Evan Avenue.

### 2.2 Road Network

#### 2.2.1 Adjoining Roads

##### Salisbury Highway

Salisbury Highway functions as a two-way arterial road aligned in a northeast-southwest direction, configured with a dual carriageway comprising of 2-lanes in either direction. The carriageway,

including central median is approximately 24 metres set within a road corridor approximately 30 metres wide. Traffic volumes sourced from the Department for Planning, Transport and Infrastructure (DPTI) suggests the two-way annual average daily traffic (AADT) volume in 2014 was 37,900 vehicles per day (VPD). Salisbury Highway is subject to a posted speed limit of 60 km/h.

#### Evan Avenue

Evan Avenue functions as a local road aligned in a northwest-southeast direction, configured with a carriageway comprising of one lane in either direction. The carriageway is approximately 9.5 metres wide set within a road corridor approximately 19 metres wide. Evan Avenue is subject to the urban default limit of 50 km/h.

### 2.2.2 Surrounding Intersections

The following intersections are located in the vicinity of the site:

- Salisbury Highway / Evan Avenue (unsignalised)
- Salisbury Highway Arthur Avenue (unsignalised)

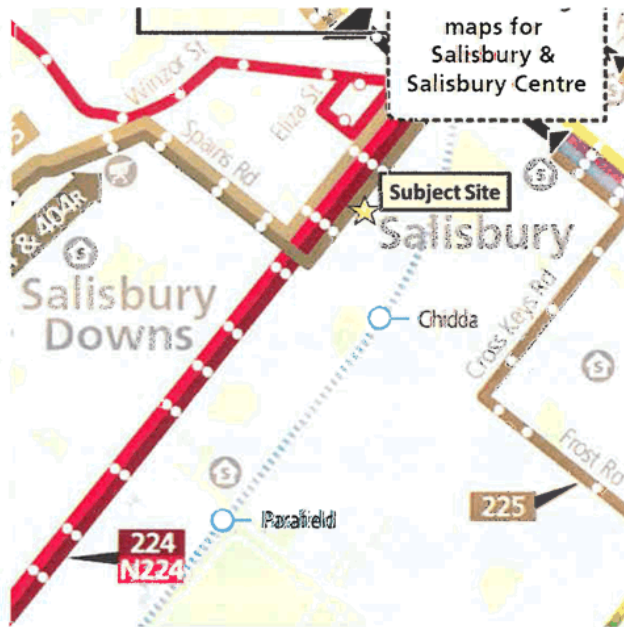
## 2.3 Sustainable Transport Infrastructure

### 2.3.1 Public Transport

Figure 2.2 shows the subject site in relation to existing public transport routes within its vicinity. Bus stops are located on Salisbury Highway approximately 185 metres north of the site and approximately 170 metres south of the site, serviced by the following routes:

- 405 Salisbury to Paralowie Clockwise Loop
- 404 & 404R Salisbury to Paralowie Anti-clockwise Loop
- 224 Elizabeth Interchange to City
- 224N After Midnight Saturday PM - Sunday AM Gawler to City

Figure 2.2: Public Transport Map



### 2.3.2 Pedestrian Infrastructure

A paved footpath, approximately 1.2 metres wide is provided on the eastern and western sides of Salisbury Highway, connecting the site to the broader footpath network. Footpaths are not available on Evan Avenue.

### 2.3.3 Cycle Infrastructure

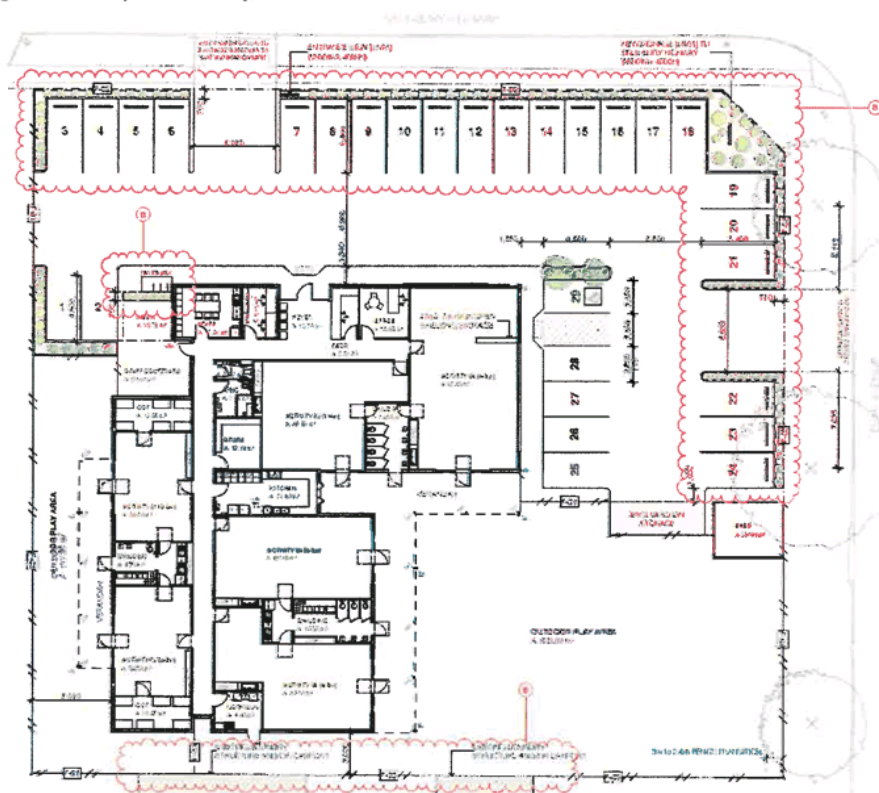
There is no formal cycle infrastructure provided on the surrounding road network however the site is accessible to cyclists via the road network or alternatively by using the footpath network provided.

### 3. Development Proposal

#### 3.1 Land Uses

The proposed development comprises a new child care centre with a gross floor area of approximately 720 square metres, an outdoor activity area of approximately 805 square metres and associated parking facilities. The child care centre will accommodate up to 114 children. A plan of the proposed development is shown in Figure 3.1 below.

Figure 3.1: Proposed Site Layout



#### 3.2 Car Parking

The proposed development includes the provision of 29 on-site car parking spaces, including one parking space for people with disabilities.

#### 3.3 Vehicle Access

Vehicle access to the car parking area is to be provided via an existing crossover from the site onto Salisbury Highway which will be increased in width to 6 metres. The crossover on Salisbury Highway will permit left in and left out turning movements.

The existing crossover on Evan Avenue will be closed and a new 6-metre-wide crossover will be provided onto Evan Avenue which will permit all turning movements. The proposed crossover will be located approximately 19 metres south of the intersection with Salisbury Highway and is located outside of the prohibited zone around intersections in accordance with AS2890.1:2009.

### 3.4 Cycling Facilities

Plans indicate a bicycle rack within the site, providing bicycle parking for staff and visitors accessing the site.

### 3.5 Pedestrian Facilities

Pedestrian access is available from the footpath on Salisbury highway via the car park crossover.

### 3.6 Loading Areas

Loading is proposed to be undertaken in the general car park area and will likely use light vehicles which can use the existing parking spaces when undertaking deliveries to the child care centre.

An enclosed bin storage area is to be located at the south-eastern corner of the car park, adjacent to a shed however refuse collection will likely be undertaken outside of regular business hours.



## 4. Car Parking

4

### 4.1 Development Plan Car Parking Requirements

Car parking rates for new developments located within the City of Salisbury are set out in table Pal/2 of their Development Plan. The car parking rates applicable to the proposed development are as follows:

- *Child Care Centre – 1 per 4 children*

Based on the above rate, the proposed development has a development plan parking requirement of 29 car parking spaces.

### 4.2 Adequacy of Parking Supply

The proposed development will supply 29 onsite parking spaces including one parking space for people with disabilities.

The proposed parking supply therefore meets the requirements of the development plan.

### 4.3 Car Parking Layout

The car park has been designed in accordance with Australian / New Zealand Standards for Off Street Car parking (AS/NZS2890.1:2004) and Parking for People with Disabilities (AS/NZS2890.6:2009) and is characterised by the following:

- Parking spaces are 2.6 metres wide by 4.8 metres long (with 600mm overhang) or 5.4m long set within an aisle 6.8 metres meeting and in most cases exceeding the requirements of a Class 3 parking facility.
- One parking space for people with disabilities that is 2.4 metres wide by 4.8 metres long (with 600mm overhang) and has an associated shared area of the same dimensions, meeting the requirements of AS2890.6:2009.
- Blind aisle extensions of 1 metre have been provided at the end of blind aisle in accordance with AS2890.1:2004.

It is recommended that minimum 300mm clearances be provided where there are any vertical obstructions. This includes a clearance being provided to the north of parking space number 2 adjacent to the proposed mechanical area.

## 5. Sustainable Transport Infrastructure

5

### 5.1 Bicycle End of Trip Facilities

Off-street bicycle parking rates are set out in Table Sa1/3 of the Development Plan. The Development Plan does not include specific bicycle parking rates for childcare centres.

The number of bicycle trips to the proposed development would be limited due to most children being delivered by private motor vehicle, and some by public transport. Some staff may ride to work, and some children may be delivered by bicycle however overall numbers would be low.

A bicycle rack is proposed to be provided towards the southern side of the building, adjacent parking space number 2. GiTA considers the provision of bicycle parking to be adequate for the proposed development.

### 5.2 Cycling Network

As previously outlined, there are no formal cycling facilities within the vicinity of the development site. As cyclists are now permitted to ride on the footpath, cyclists may access the site via the road or footpath network.

### 5.3 Public Transport

The site is accessible by public transport as discussed in Section 2.3.1.

## 6. Loading & Refuse Collection

### 6.1 Deliveries & Refuse Collection

Loading is proposed within the car park during regular business hours but will likely occur outside of peak traffic periods. Deliveries would generally be provided by light vehicles such as courier vans, which will use the car parking spaces for loading and unloading.

To minimise conflicts, waste collection will occur outside of business operating hours when the car park will be empty (i.e. early morning or weekends).

This type of arrangement is typical to many child care centres around the metropolitan area.

Turn paths for a 10-metre refuse vehicle have been undertaken and are shown in Figures 6.1 to 6.2. The turn paths show that the vehicle will be able to enter the site in a forward direction, collect refuse, reverse and then exit in a forward direction.

Figure 6.1: 10m Refuse Collection Vehicle – Forward Entry, Front Collection

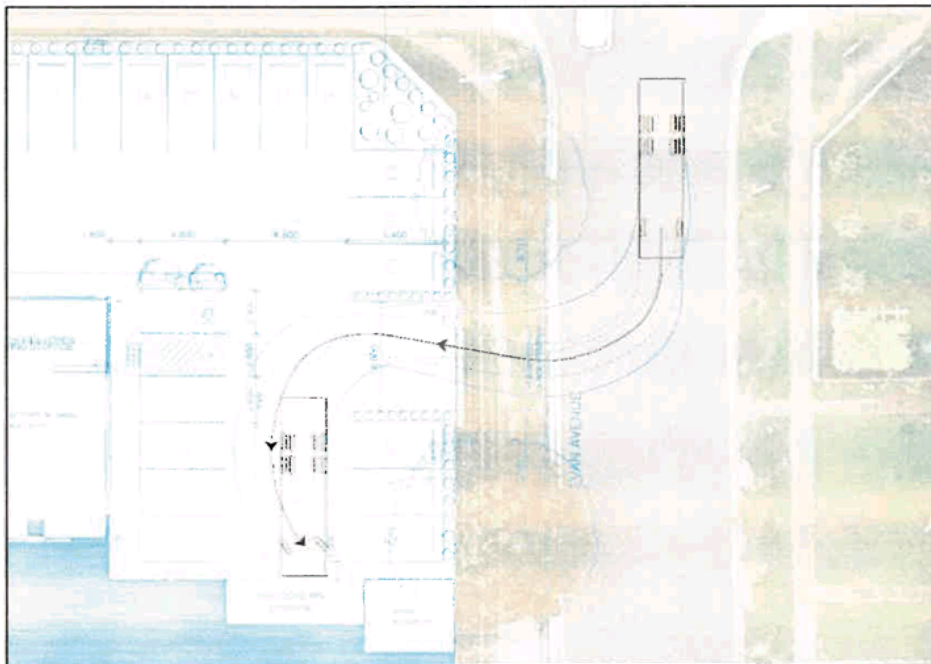
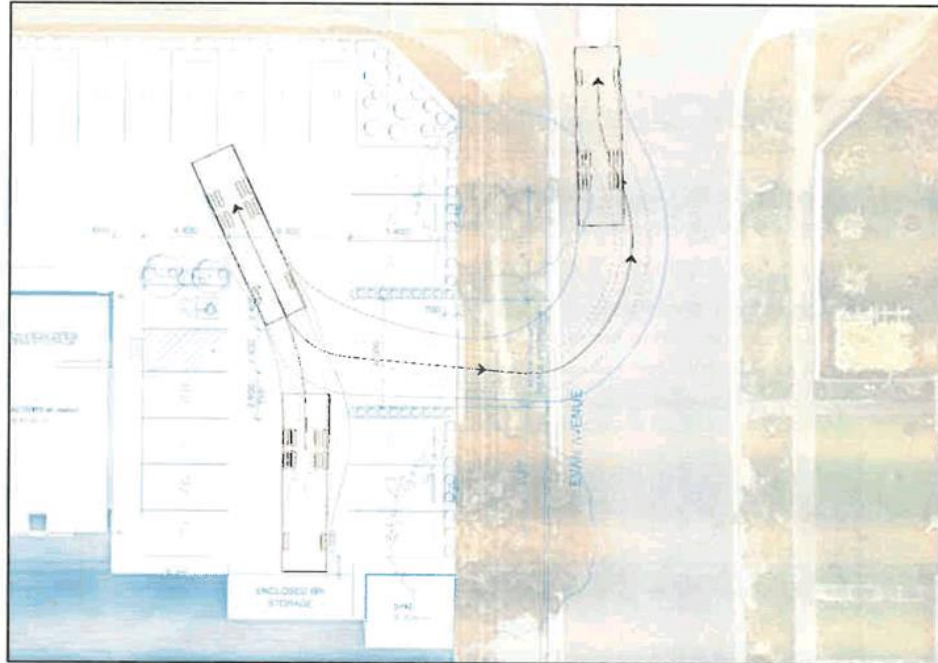


Figure 6.2: 10m Refuse Collection Vehicle – Front Collection, Forward Exit



## 7. Traffic Impact Assessment

### 7.1 Traffic Generation

#### 7.1.1 Design Rates

Traffic generation rates have been sourced from the Roads and Maritime Services of New South Wales (formerly RTA) in the "Guide to Traffic Generating Developments" published in 2002 (henceforth referred to as the RMS Guide). The applicable rates for a child care centre are as follows:

**Table 7.1: RTA Child Care Trip Rates**

Centre Type	Peak Vehicle Trips / Child		
	07:00 – 09:00	14:30 – 16:00	16:00 – 18:00
Long-day Care	0.8	0.3	0.7

Assuming 70% of trips occur during the peak hour, this would equate to a rate of 0.6 trips per child in the AM peak (8-9am) and 0.5 trips per child in the PM peak (5-6pm).

Based on the capacity of 114 children, the proposed childcare centre would generate 68 trips during the AM peak (8-9am) and 57 trips during the PM peak (5-6pm).

#### 7.1.2 Distribution and Assignment

The directional distribution and assignment of traffic generated by the proposed development will be influenced by several factors, including the:

- i configuration of the arterial road network in the immediate vicinity of the site;
- ii existing operation of intersections providing access between the local and arterial road network;
- iii distribution of households in the vicinity of the site;
- iv configuration of access points to the site;

Having consideration to the above, for the purposes of estimating vehicle movements, the following directional distributions have been assumed:

- Salisbury Highway Access – 40%
- Evan Avenue Access – 60%
- Salisbury Highway northbound 50%
- Salisbury Highway southbound 50%

In addition, the directional split of traffic (i.e. the ratio between the inbound and outbound traffic movements) is assumed to be 50:50 (50% inbound, 50% outbound).

Based on the above, Figure 7.1 and Figure 7.2 have been prepared to show the estimated marginal increase in turning movements in the vicinity of the subject property following full site development.

Figure 7.1: AM Peak Hour Site Generated Traffic Volumes

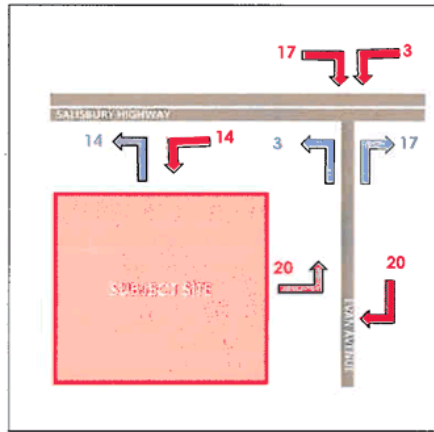
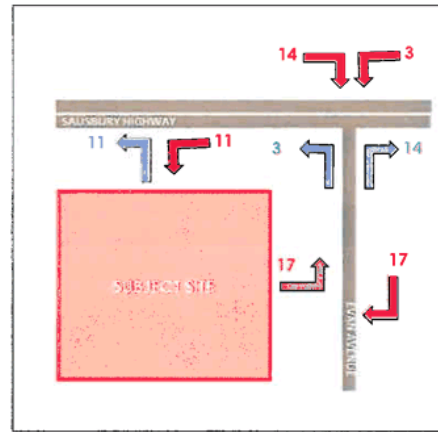


Figure 7.2: PM Peak Hour Site Generated Traffic Volumes



## 7.2 Traffic Impact

Against existing traffic volumes in the vicinity of the site, the additional traffic generated by the proposed development could not be expected to compromise the safety or function of the surrounding road network.

A high proportion of the customers of the centre will likely be from the surrounding residential areas and hence already be using the roads around the centre to travel to and from other destinations (e.g. commuting for work). This is known as passing trade and would typically reduce the net traffic impact of the proposed development by up to 30%.

Childcare centres generate very few trips during the day between peak periods, hence the traffic impact will generally be during the morning and evening child delivery and collection periods respectively. Hence, the impact of traffic on the adjacent streets is expected to be low.

## 8. Conclusion

Based on the analysis and discussions presented within this report, the following conclusions are made:

- i The proposed development generates a development plan parking requirement of 29 parking spaces.
- ii The proposed supply of 29 spaces within the site, including one space for people with disabilities, meets the development plan requirements and is considered to be appropriate.
- iii The proposed parking layout is consistent with the dimensional requirements as set out in the Australian/New Zealand Standards for Off Street Car Parking (AS/NZS2890.1:2004 and AS/NZS2890.6:2009).
- iv Waste collection can be undertaken onsite. A refuse vehicle can enter the site in a forward direction, collect refuse and exit the site in a forward direction.
- v The proposed bicycle parking provision is considered to be appropriate given the nature of the proposed development.
- vi The site is expected to generate up to 68 and 57 vehicle movements in the morning and afternoon peak hour respectively.
- vii As childcare centres generate few trips during the day with the peak traffic occurring during morning and evening child delivery and collection, the impact of traffic on adjacent streets are expected to be low.

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## STORMWATER MANAGEMENT REPORT

---

Early Learning Centre Salisbury

**Prepared by:**

**PT Design** ABN 35 008 116 916  
141-149 Ifould Street, ADELAIDE SA 5000  
Tel: (08) 8412 4300

Project No: 20505  
Revision: -00-  
Date of Issue: 20/09/2018

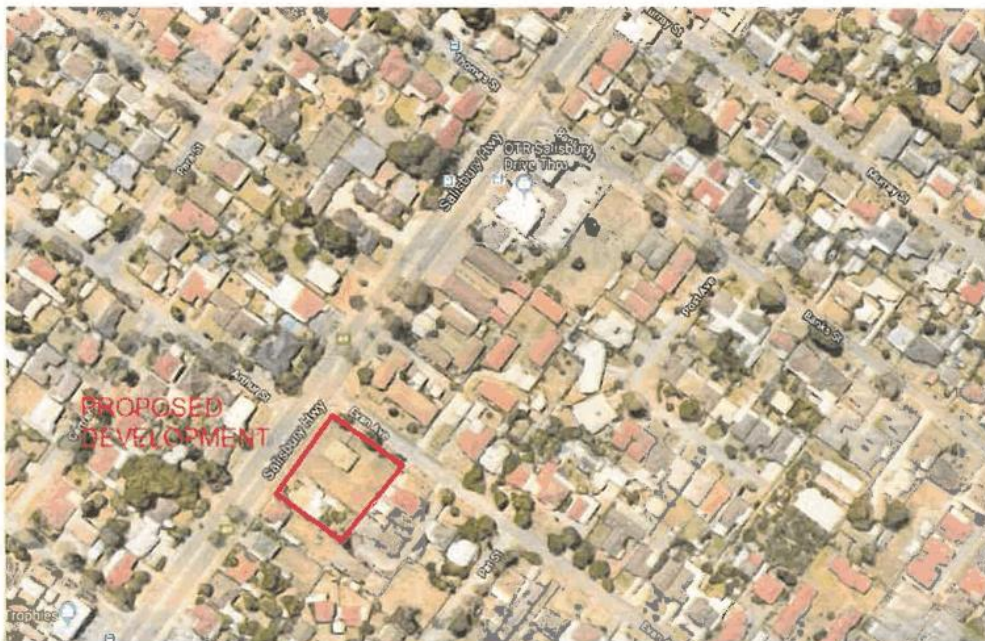
## INTRODUCTION

A new Early Learning Centre is proposed to be constructed on the corner of Salisbury Highway and Evan Avenue, Salisbury. The site is located in a typically residential area, however, given this site is on a main road, similar commercial style buildings are located nearby. This report considers the stormwater management including detention in accordance with council's requirements.

## THE SITE

The site is a rectangular piece of land 2744m<sup>2</sup> in area, bordered by roads to the north and west and residential properties to the south and east. The predevelopment allotment/s are covered by domestic buildings (and out buildings) and minor extents of paving. The site falls approx. 0.5m from the north to the south. Council has advised that the site is subject to minor flooding during major storm events.

There are no side entry pits or stormwater connection points adjacent the site to allow for direct connection of the stormwater, however, an existing stormwater pipe runs in the street along Evan Avenue. Council has advised that direct connection of the stormwater to this pipe is considered acceptable.



### **STORMWATER DETENTION**

The City of Salisbury requires that we detain the post-development critical 1 in 100 year ARI event to no more than the pre-development minor rain event.

Calculations show that the pre-development flow from the site is 24.5 L/sec. Calculations show that we must then provide 26.6 kL of detention storage. Refer to Appendix A for the supporting calculations.

Detention will be provided by above ground storage in the front carpark. The final detention details will be determined during detailed design in the future.

### **STORMWATER DESIGN**

The FFL of the building will be set at 27.85. This is 300mm above the nearest adjacent highest top of kerb level. Council has advised that this is the minimum height required to reduce the risk of inundation of flood waters from the road during a major storm event. All other levels will be set to provide an overland flow path and protection to the building during a major storm event. Final paving levels will be determined during detail design.

All roof and paved catchment areas within the site will be directed to the stormwater detention system within the carpark. Final stormwater pipe layout and sizes will be determined in accordance with AS3500.3 during detailed design development stage in the future.

No harvesting of roof stormwater is proposed for this development.

Given limited areas of landscaping around the carpark and the need to maximise play areas around the building, Water Sensitive Urban Design measures cannot be achieved. A proprietary Gross Pollutant Trap and Oil and Grease Arrestor will be provided to the carpark areas not undercover prior to discharging from the site in an effort to reduce any potential pollutants. A suitably sized and selected treatment device will be specified once we get into detailed design development.

# **APPENDIX A**

## **STORMWATER DETENTION CALCULATIONS**



## STORMWATER CALCULATIONS

---

Early Learning Centre Salisbury

Prepared by:

**PT Design** ABN 35 008 116 916  
141-149 Ifould Street, ADELAIDE SA 5000  
Tel: (08) 8412 4300

Project No: 20505  
Revision: -00-  
Date of Issue: 20/09/2018



Project: 158-162 Salisbury Highway	Project # 20505
Salisbury	Date 20.09.2018
Design By: MP	Page 1

**CRITICAL 1 IN 100 YEAR DETENTION VOLUME**

**PRE DEVELOPMENT FLOW (MINOR STORM)**

Time of Concentration	5 mins
Rainfall Intensity	94.2 mm/hr

Catchment Area	C	Area (m <sup>2</sup> )	
Roof	0.9	491	11.6
Impervious	0.75	78	1.5
Pervious	0.2	2175	11.4
		<b>Total</b>	<b>24.5</b> L/sec

**POST DEVELOPMENT FLOW (MAJOR STORM)**

Time of Concentration	t mins (critical TBC)
Rainfall Intensity	<sup>100</sup> i <sub>t</sub> mm/hr

Catchment Area	C	Area (m <sup>2</sup> )	
Roof	0.9	857.9	0.21
Impervious	0.75	1287.6	0.27
Pervious	0.2	598.5	0.03
		<b>Total</b>	<b>0.52</b> <sup>100</sup> i <sub>t</sub>



Project: 158-162 Salisbury Highway	Project # 20505
Salisbury	Date 20.09.2018
Design By: MP	Page 2

**CRITICAL STORAGE VOLUME**

$$\frac{Q \text{ in}}{0.52} = 100 I_t$$

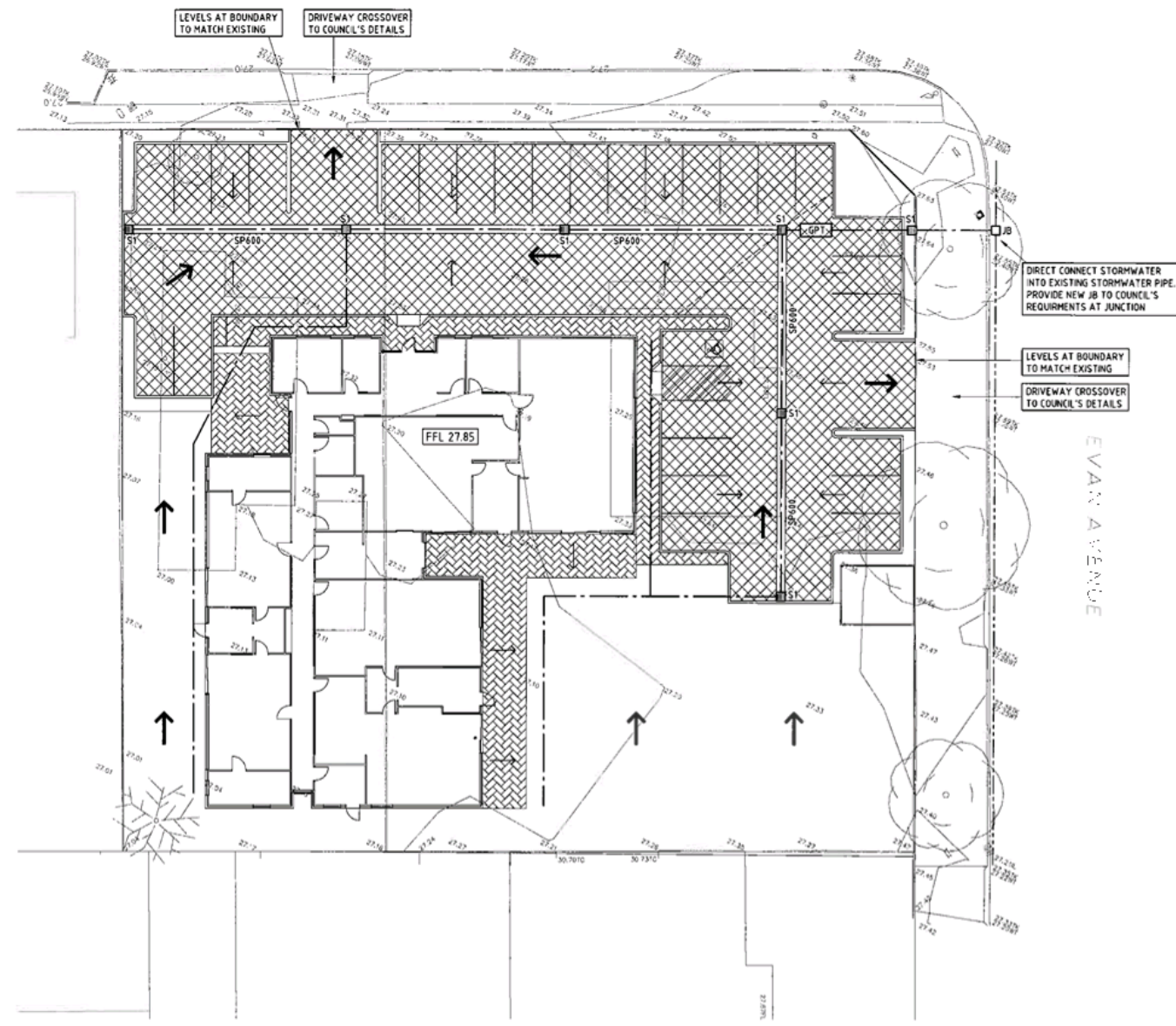
$$\frac{Q \text{ out}}{24.5} = \text{L/sec}$$

Tc (mins)	Intensity, I (mm/hr)	Q in (L/sec)	Pump initiated t (mins)	V total (L <sup>3</sup> )
5	192	99.1	1.2	16849
6	177	91.3	1.3	18692
10	140	72.2	1.7	23801
20	96.4	49.7	2.5	26587
30	75.4	38.9	3.1	23248
60	47.7	24.6	5.0	448
120	29.3	15.1	8.1	-62834
180	21.9	11.3	10.8	-133743
360	13.3	6.9	17.8	-361612
720	7.9	4.1	30.0	-844535
1440	4.7	2.4	50.5	-1838456

**PEAK STORAGE REQUIRED** 26587 L<sup>3</sup>





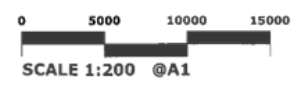


- LEGEND**
- STORMWATER PIPE TO FUTURE DETAILS
  - - - - - APPROX. LOCATION OF EXISTING STORMWATER PIPE CONFIRM LOCATION & DEPTH ON SITE
  - JB JUNCTION BOX TO FUTURE DETAILS
  - SI STORMWATER SUMP TO FUTURE DETAILS
  - GPT SOLID POLLUTANT FILTER / OIL AND GREASE ARRESTOR TO FUTURE DETAILS
  - PS PRE-PACKAGED PUMP STATION TO PUMP MANUFACTURERS DETAILS PUMP RATE 20.0 L/sec
  - PRM PUMP RISING MAIN TO PUMP MANUFACTURERS DETAILS
  - GRADE LINE
  - ← DIRECTION OF SURFACE FALL
  - ← DIRECTION OF OVERLAND FLOW PATH DURING MAJOR STORM EVENT
  - [Cross-hatched] BITUMEN PAVING TO FUTURE DETAILS
  - [Diagonal-hatched] PAVING TO FUTURE DETAILS

**PUMP NOTES:**  
 PUMP SHALL BE DUAL PUMP. THE PUMP CONTROLS SHALL BE SET UP TO ENABLE ALTERNATE PUMP OPERATION AT EACH START. IN THE EVENT THAT A PUMP FAILS TO OPERATE WHEN THE WATER LEVEL IN THE WELL REACHES THE PUMP START, THE OTHER PUMP SHALL BE ACTIVATED AND A VISIBLE ALARM INITIATED. IN THE EVENT THAT BOTH PUMPS FAIL TO OPERATE, AN AUDIBLE ALARM SHALL BE INITIATED. PROVIDE BACK-UP POWER SUPPLY IN CASE OF POWER FAILURE.

**NOTE:**  
 DRAWINGS SHALL BE READ IN CONJUNCTION WITH ALL OTHER CONSULTANTS DRAWINGS AS A PACKAGE. REFER TO ARCHITECTS DRAWINGS FOR ALL SETOUT DIMENSIONS.  
 ALL LEVELS SHALL BE CONFIRMED ON SITE PRIOR TO CONSTRUCTION. SHOULD ANY DISCREPANCY OCCUR THE CONTRACTOR SHALL CONTACT THIS OFFICE IMMEDIATELY FOR FURTHER INSTRUCTION.

**CONTRACTORS NOTES:**  
 COVER LEVELS GIVEN FOR PITS ARE NOMINAL ONLY. COVER LEVELS SHALL MATCH FINISHED PAVING LEVELS.  
 THE CONTRACTOR IS RESPONSIBLE FOR CHECKING LOCATION OF ALL UNDERGROUND SERVICES PRIOR TO COMMENCING ANY EXCAVATION WORK. ANY DAMAGE CAUSED TO ANY SERVICES SHALL BE REPORTED IMMEDIATELY TO THE SUPERINTENDENT & SHALL BE REPAIRED BY THE APPROPRIATE AUTHORITIES. ALL COSTS ASSOCIATED WITH REPAIRS SHALL BE AT THE CONTRACTOR'S EXPENSE. PHONE 0810 BEFORE YOU DIG (1100) FOR ASSISTANCE.  
 WHERE PROPRIETARY ITEMS ARE SPECIFIED, ALTERNATE EQUIVALENT PRODUCTS MAY BE ADOPTED WITH THE PRIOR WRITTEN APPROVAL OF THIS OFFICE.



FOR APPROVAL

26.09.2018	ISSUED FOR PLANNING APPROVAL	B
24.09.2018	PRELIMINARY ISSUE	A
Date	Revision	Issue

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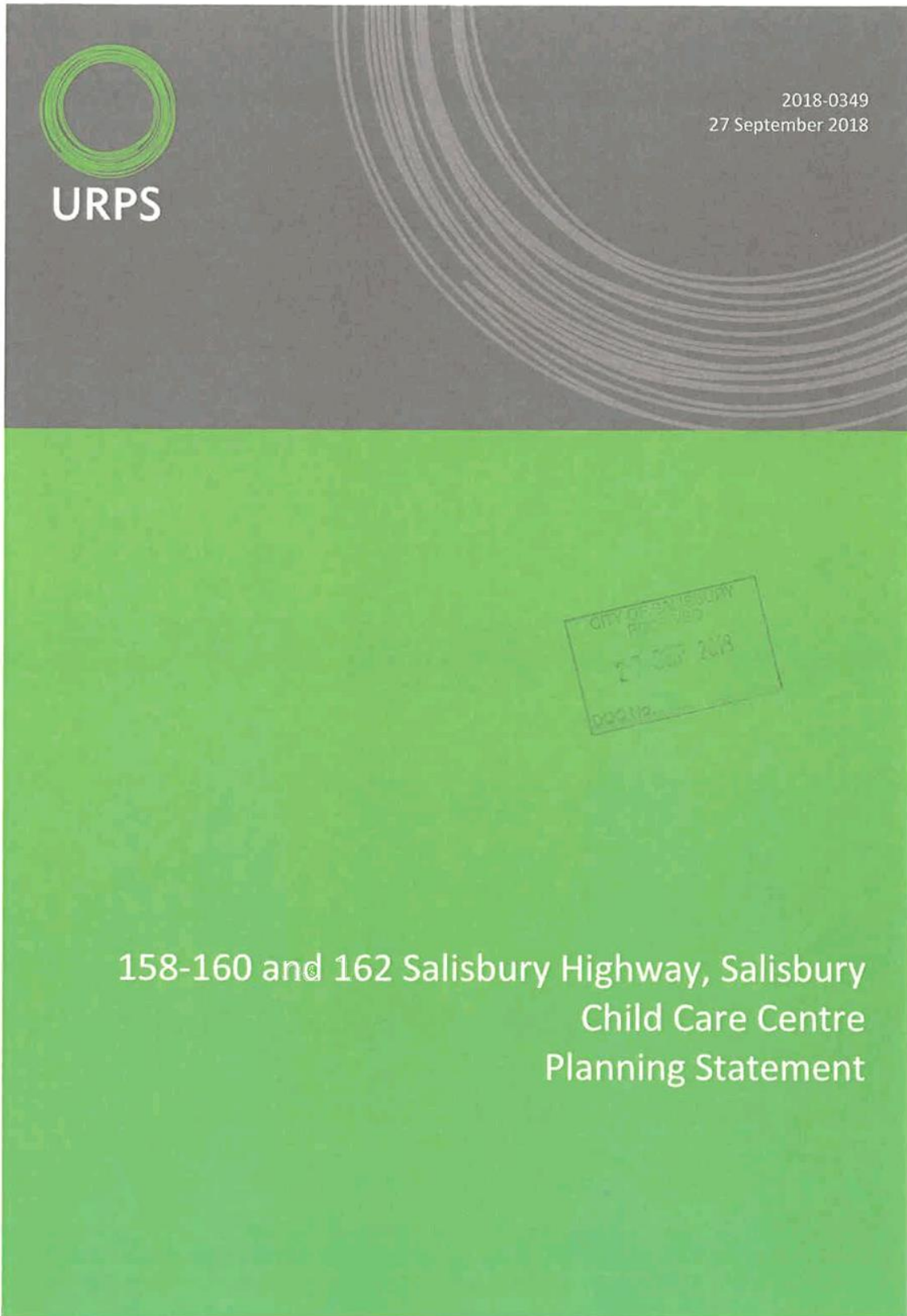
Designed <b>MP</b>	Drawn <b>MP</b>
Approved	Date <b>SEPT '18</b>
CIVL	Sheet 1 of 1

Project  
**EARLY LEARNING CENTRE  
 158-162 SALISBURY HIGHWAY  
 SALISBURY**

Client <b>FLIGHTPATH ARCHITECTS</b>	N
Drawing Title <b>STORMWATER MANAGEMENT PLAN</b>	Scale 1:200
Drawing Number <b>20505-C01</b>	Issue <b>B</b>

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Item 5.1.1 - Attachment 1 - Application Documentation



# Planning Report

27 September 2018

**Lead consultant** URPS

**Prepared for** AO Salisbury Pty Ltd

**Consultant Project Manager** Matthew King, Director  
Phil Harnett, Associate

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**URPS Ref** 2018-0349

Revision	Date	Reviewed	Approved	Details
V1	24.9.18	MK	MK	Draft

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## Executive Summary

### Child Care Centre – 158-160 and 162 Salisbury Highway, Salisbury

<b>Applicant</b>	AO Salisbury Pty Ltd
<b>Description of land</b>	<ul style="list-style-type: none"> <li>158-160 Salisbury Highway, Salisbury (CT 5113/67).</li> <li>162 Salisbury Highway, Salisbury (CT 5400/82).</li> </ul>
<b>Site area</b>	2,745.43 square metres
<b>Council</b>	City of Salisbury
<b>Development Plan</b>	Salisbury Council Development Plan (consolidated 15 December 2016)
<b>Zone</b>	Residential Zone
<b>Policy Areas</b>	N/A
<b>Current land use</b>	Residential
<b>Description of development</b>	<ul style="list-style-type: none"> <li>Construction of purpose built child care centre.</li> <li>Ancillary outdoor play areas, shade sails, storage shed, bin enclosure, car parking and landscaping.</li> <li>1 x wall mounted advertisement (6m wide x 2m high).</li> <li>2 x freestanding advertisements (1.5m wide x 4m high).</li> </ul>
<b>Assessment Pathway</b>	Merit
<b>Public notification</b>	Category 3
<b>Relevant Authority</b>	Salisbury Council
<b>Referrals</b>	Department of Transport, Planning and Infrastructure (DPTI)
<b>Related applications</b>	N/A
<b>Contact person</b>	Mr Phil Harnett

## 1.0 Introduction

URPS has been engaged by AO Salisbury Pty Ltd, the applicant for this application, to provide planning advice, liaise with the relevant authority and prepare this supporting planning statement.

In addition to this report, the following is attached:

- Certificate of Title (attachment 1).
- Plans prepared by Flightpath Architects (attachment 2).
- Landscaping plans prepared by Birdseye Studios (attachment 3).
- Transport Impact Assessment prepared by GTA Consultants (attachment 4).
- Environmental Noise Assessment prepared by Sonus (attachment 5).
- Stormwater Management Report and Plan prepared by PT Design (attachment 6).



## 2.0 Subject Land and Locality

### 2.1 Subject Land

The subject land is comprised of:

- 158-160 Salisbury Highway, Salisbury (CT 5113/67).
- 162 Salisbury Highway, Salisbury (CT 5400/82).

The land is situated on the corner of Salisbury Highway and Evan Avenue. Salisbury Highway is prescribed as a 'Secondary Arterial Road'.

Together, the allotments have an approximate area of 2,745.43 square metres and a frontage to Salisbury Highway and Evan Avenue of 55 metres and 50.2 metres respectively.

Both allotments are generally flat and presently contain single storey detached dwellings. 162 Salisbury Highway also contains a garage, outbuilding and verandas.

The certificate of title for each allotment forms **attachment 1**.

### 2.2 Locality

The locality is largely characterised by the traffic associated with Salisbury Highway and the single storey dwellings which front this arterial road and nearby streets.

Existing dwellings in the locality predominantly date from the 1960's to 1980's era of architecture with some earlier and later dwellings also present.

Allotments vary in size however are predominantly rectangular in shape and Torrens Titled in ownership. Some small scale Community Title land divisions also exist however are less frequent throughout the locality by comparison.

Evan Avenue is a typical suburban street with regular street trees, some of which are well established.

A small Local Centre Zone with some small specialty shop and ancillary car parking is approximately 100 metres south of the subject land further along Salisbury Highway.

A locality plan forms **Image 1** below.



Subject Land and Locality



<b>LEGEND</b>	<b>ZONES</b>
Site boundary	<b>LCe</b> Local Centre
Cadastre	<b>R</b> Residential
Zone boundary	

Image 1: Locality Plan



## 3.0 Proposed Development

The proposal is for:

- Construction of purpose built child care centre with ancillary outdoor play areas, shade sails, storage shed, bin enclosure, car parking and landscaping.
- 1 x wall mounted advertisement (6m wide x 2m high).
- 2 x freestanding advertisements (1.5m wide x 4m high).

The child care centre will cater for children ranging from 0 to 6 years old and will accommodate up to 114 children at any one time. Up to 16 staff members will be present at any one time to monitor and care for the children.

The child care centre will be single storey, have a total floor area of 719 square metres and will be open from 6:30am to 6:30pm Monday to Friday.

29 car parking spaces, 1 of which is a disabled access park, will be available on-site.

The proposed plans prepared by Flightpath Architects forms **Attachment 2**.

The associated landscaping plans prepared by Birdseye Studios form **Attachment 3**.

## 4.0 Procedural Matters

### 4.1 Zone

The subject land is situated within the Residential Zone as per the Salisbury Council Development Plan (consolidated 15 December 2016).

The land is not situated within a policy area or precinct.

### 4.2 Assessment Pathway

A child care centre within the Residential Zone is neither “complying” nor “non-complying”. The proposed use will therefore be assessed on its “merits”.

The zone further prescribes advertisements as non-complying except where:

- It does not move, rotate or incorporate flashing lights.
- It does not wholly or partly consist of bunting, streamers, flags, wind vanes and the like.
- If attached to a building, it does not wholly or partly extend above the top of the wall or the fascia.

The proposed advertisements satisfy these exceptions and therefore will also be assessed on their “merits”.

### 4.3 Public Notification Category

The Residential Zone does not prescribe public notification categories. As such, categories for public notification are prescribed in Schedule 9 of the Development Regulations 2008.

A child care centre is not listed as Category 1 or 2 and therefore defaults to Category 3 for public notification purposes.

### 4.4 Referrals

In relation to Salisbury Highway, a ‘Secondary Arterial Road’, the proposal will:

- Alter an existing access.
- Create a new access.

As such, the proposal must be referred to DPTI (Department of Planning, Transport and Infrastructure) and the council must have regard to their comments. A response to the referral from DPTI should be received within 4 weeks.

On behalf of the applicant, GTA Consultants have had preliminary discussions with DPTI regarding this proposal. DPTI have indicated their “in principle” support. Matters concerning DPTI will be discussed further throughout this report.



## 5.0 Development Assessment

On my consideration of the relevant Salisbury Council Development Plan, the following matters are most relevant to the application:

- Land Use.
- Siting, Design and Appearance.
- Advertisements.
- Transportation and Access.
- Car and Bicycle Parking.
- Interface between Land Uses.
- Waste Storage and Collection.
- Stormwater Management.
- Orderly and Sustainable Development.

I address the proposal's performance against the above matters, with reference to the most pertinent Development Plan provisions, as follows.

### 5.1 Land Use

The Desired Character of the Residential Zone states:

**In addition to housing and open space, the zone will also contain a variety of community related, non-residential land uses that contribute to the creation of pleasant, connected, well serviced and socially inclusive neighbourhoods.**

Principle 1 of the zone states:

**The following forms of development are envisaged in the zone:**

- ...
- **small scale non-residential use that serves the local community, for example:**
  - **child care facility...**

The proposed land use is specifically envisaged in the zone and will contribute to achieving the zone's desire for a pleasant and well serviced neighbourhood. So as to provide ease of access for families, child care services are best situated amongst the residential land uses they generally service.

The appropriate establishment of non-residential uses in the Residential Zone is guided by Principle 4 which states:

**Non-residential development such as shops, schools and consulting rooms should be of a nature and scale that:**

- (a) serves the local community**
- (b) is consistent with the character of the locality**
- (c) does not detrimentally impact on the amenity of nearby residents**
- (d) will not undermine the efficient operation of nearby centres.**

The proposed child care centre will certainly be a use that serves the local community, offering child care services to local working families with children aged 0 to 6 years old. The proposed child care centre will accommodate up to 114 children. On this basis, Clause (a) is satisfied.

The proposed building design is consistent with the low-rise residential character of the locality, being respectful of scale and materials and therefore Clause (b) is satisfied.

With specific regard to the proposal, amenity impacts turn on height and setbacks, scale of the facility, operating hours and noise. In this respect I note:

- As demonstrated by attachment 2 the building height will be commensurate to the nearby single storey dwellings.
- Setbacks at the residential interface will range between 3.0 - 5.8 metres. As such, setbacks satisfy General Section, Residential Development Principles 16 and 17 in that the proposal will provide:
  - > Adequate visual privacy.
  - > Minimise visual impact.
  - > Minimise the overshadowing of adjoining properties.
- The combined building height and setbacks ensure the proposal contributes to the desired character of the zone.
- The scale of the facility is relatively small and typical of most child care centres in South Australia.
- Noise associated with the facility is well managed as discussed further within this report.
- Operating hours are limited to 6:30am to 6:30pm Monday to Friday.
- All car parking areas and mechanical equipment are located on the opposite side of the building to residential land uses.

Interface considerations are discussed further within this report however for the purposes of the above, clause (c) is satisfied.

In respect to the undermining of nearby centres, the nearest centre is a small Local Centre Zone situated approximately 100 metres south of the subject land further along Salisbury Highway. A large Urban Core Zone that accommodates the Salisbury Railway Station, Salisbury Cinemas and a variety of shops, offices and restaurants is situated approximately 400 metres north of the subject land. The proposed land use will not jeopardise the efficient operation of these centres because:



- The nearby Local Centre Zone is small and only accommodates 2 or 3 shops. This particular Local Centre Zone does not comprise a land use that would compete with the proposed child care centre.
- The subject land is sufficiently distanced from the Urban Core Zone so as not to compete with land uses within it. In addition, the majority of existing land uses within the Urban Core Zone provide substantially different services to the childcare centre proposed.
- The applicant has identified a need for a child care centre in the locality regardless of others throughout the council area. There is a high demand for child care places within South Australia.

The proposed land use is clearly anticipated within the zone and is appropriate for the reasons explained throughout this report. On this basis, clause (d) is satisfied.

## 5.2 Siting, Design and Appearance

The design provisions of the Residential Zone primarily relate to residential development.

The Desired Character of the zone states:

A cohesive built form will be achieved through design elements such as interesting roof forms, articulated buildings, recessed vehicle garaging, and landscaped spaces between buildings and the public road. Buildings will include windows, doors and balconies that overlook the street and areas of public open space to promote surveillance and community interaction.

While some design guidance can be taken from the Residential Zone, the general section of the development plan provides more applicable policies.

General Section, Design and Appearance Principles 1, 3 and 7 state:

1 The design of a building may be of a contemporary nature and exhibit an innovative style provided the overall form is sympathetic to the scale of development in the locality and with the context of its setting with regard to shape, size, materials and colour.

3 Buildings should be designed to reduce their visual bulk and provide visual interest through design elements such as:

- (a) articulation
- (b) colour and detailing
- (c) small vertical and horizontal components
- (d) design and placing of windows
- (e) variations to facades.

7 The external walls and roofs of buildings should not incorporate highly reflective materials which will result in glare to neighbouring properties or drivers.

The proposal has been designed and sited with careful consideration to the surrounding residential character which predominantly comprises low-scale, single storey detached dwellings.



To ensure a cohesive built form in accordance with the desired character of the zone, and to satisfy the general provisions, the following has been implemented:

- Broad front, side and rear setbacks that provide substantial spacing around the building for car parking and landscaping.
- A single storey and low scale built form with eave heights ranging from 3.6 to 3.9 metres and a total height up to 4.5 metres.
- A mix of flat and low-pitched roof elements that provide interesting roof features yet keep the building low-scale and respectful of surrounding dwellings.
- Windows and doors that face Salisbury Highway, Evan Avenue and the proposed car parking area. These windows and doors are proportionate to the building and will be glazed to enhance casual surveillance during opening hours.
- Horizontal and vertical components that provide design balance and enhance building articulation and visual interest.
- Materials and finishes which are common of residential buildings including brick feature walls. Proposed materials are also low-light reflective to prevent glare.
- Proportionate and modest advertisements that do not dominate the proposed building or its locality.

### 5.3 Advertisements

The following advertisements are proposed:

- 1 x wall mounted advertisement (6m wide x 2m high).
- 2 x freestanding advertisements (1.5m wide x 4m high).

General Section, Advertisements Principle 7 states:

**7 Advertisements and/or advertising hoardings attached to buildings should not be sited on the roof or higher than the walls of a building.**

The proposed wall mounted advertisement will:

- Face Salisbury Highway with dimensions that are proportionate to the proposed building.
- Not be sited on the roof or higher than the walls of the building. The top of the sign will be approximately 2.7 metres above ground level and at least 1.2 metres below the predominate eave height.
- Sit flush on the building's façade and therefore be setback approximately 14 metres from the Salisbury Highway frontage.
- Not be illuminated, move, flash or rotate.

General Section, Advertisements Principle 15 states:

**15 Freestanding advertisements and/or advertising hoardings should be:**



**(a) limited to only one primary advertisement per site or complex**

**(b) of a scale and size in keeping with the desired character of the locality and compatible with the development on the site.**

Principle 18 further states that freestanding advertisements should not have a total height exceeding 3 metres.

Two freestanding advertisements with a total height of 4 metres each are considered acceptable for the following reasons:

- The proposal effectively amalgamates two allotments. Currently, two small scale non-residential land uses, as envisaged by the zone, could be individually developed on each property and have their own freestanding advertisement.
- The combined frontage of the two existing allotments to Salisbury Highway is approximately 55 metres. The distance between the two freestanding advertisements is approximately 32 metres. The proposed freestanding advertisements will not dominate the streetscape given the length of the frontage to Salisbury Highway and the space between each.
- Salisbury Highway is wide (i.e. approximately 30 metres) and the proposal will have an additional setback of 14 metres. In this broad and open context, the four-metre-high freestanding advertisements will not appear overly tall.
- The desired character of the zone allows buildings of 2 or more storeys to facilitate an increase in density. The desired character further states that medium density development of up to four storey's in height is anticipated in areas close to centres, public transport. In this context the proposed freestanding advertisements are not high buildings.
- The freestanding advertisements will be complemented by low level landscaping and the existing street trees to be retained in Evan Avenue.
- The advertisements will not be illuminated, move, flash or rotate nor will they cause any overshadowing upon residential land.
- The advertisements have a design and shape that provides colour and interest in the streetscape.

General Section, Advertisements Principle 2 states:

**2 The number of advertisements and/or advertising hoardings associated with a development should be minimised to avoid:**

**(a) clutter**

**(b) disorder**

**(c) untidiness of buildings and their surrounds**

**(d) driver distraction.**

The proposed advertisements are orderly, coherent and consistent in theme and design. The proposed advertisements do not cause clutter, disorder or untidiness within the locality and will not be



illuminated, flash, move, rotate or be reflective so as to cause glare. On this basis, proposed advertisements are considered to satisfy Principle 2.

## 5.4 Transportation and Access

General Section, Transportation and Access Objective 2 states:

### 2 Development that:

- (a) provides safe and efficient movement for all motorised and non-motorised transport modes
- (b) ensures access for vehicles including emergency services, public infrastructure maintenance and commercial vehicles.
- (c) provides off street parking
- (d) is appropriately located so that it supports and makes best use of existing transport facilities and networks.

When combined, the existing allotments have 2 vehicle crossovers (1 from Salisbury Highway and 1 from Evan Avenue). This arrangement will be retained however the crossovers will be widened to 6 metres to accommodate two-way vehicle movement and simultaneous access/egress.

The proposed access/egress arrangement to/from Evan Avenue is safe and convenient because:

- Evan Avenue is speed limited to 50km/h.
- Vehicle movements in and out of the driveway will be generally slow.
- The crossover will be setback approximately 19 metres from the intersection with Salisbury Highway. This is in accordance with relevant Australian Standards and ensures sufficient awareness for motorists exiting the subject land when a vehicle is turning into Evan Avenue from Salisbury Highway.
- Vehicles can currently turn left and right into Evan Avenue safely and legally.
- A footpath travels parallel with, and alongside, Evan Avenue. This means that pedestrians have a defined paved footpath with excellent sight lines.
- The vehicle crossover will have good sight lines for motorists and pedestrians despite preserving existing street trees.

The proposed access/egress arrangement to/from Salisbury Highway is safe and convenient because:

- Salisbury Highway is speed limited to 60km/h given numerous single width driveways that have direct access to and from, including those that service the allotments comprising the subject land.
- Vehicle movements in and out of the driveway will be generally slow.
- A raised central median is situated within Salisbury Highway for the full frontage length of the subject land. This means that vehicles entering and exiting the subject land from Salisbury Highway will be limited to 'left in' and 'left out' movements only.



- A wide footpath travels parallel with, and alongside, Salisbury Highway. This means that pedestrians have a defined paved footpath with excellent sight lines.
- No street trees are situated along the Salisbury Highway frontage meaning that all motorists have clear sight lines when travelling along Salisbury Highway and when using the proposed vehicle crossover.

GTA Consultants have undertaken a Transport Impact Assessment which forms **Attachment 4**. On behalf of the applicant, GTA Consultants had preliminary discussions with DPTI regarding this proposal. DPTI have indicated their “in principle” support.

GTA Consultants have concluded the following:

- Additional traffic generated by the proposed development could not be expected to compromise the safety or function of the surrounding road network.
- A high proportion of customers will likely be from the surrounding residential areas and hence already be using the roads surrounding the centre to travel to and from other destinations (e.g. commuting to work).
- Child care centres generate very few trips during the day between peak periods. Hence any traffic impact will be during the morning and evening when children are dropped off and picked up.

## 5.5 Car and Bicycle Parking

### 5.5.1 Car Parking

General Section, Transportation and Access Principle 32 states:

**32 Development should provide off-street vehicle parking and specifically marked disabled car parking places to meet anticipated demand in accordance with Table Sal/2 - Off Street Vehicle Parking Requirements.**

Table Sal/2 guides that a child care centre should have 1 car parking space per 4 children.

The proposal will have a maximum capacity of 114 children. 29 on-site car parking spaces are proposed, 1 of which is marked as a disabled car parking place. On this basis, sufficient car parking is provided with 29 car parking spaces being capable of accommodating 116 children as per the car parking guideline prescribed by the Development Plan.

General Section, Transportation and Access Principle 33 states:

**33 Development should be consistent with Australian Standard AS 2890 Parking facilities.**

As per the Transport Impact Assessment prepared by GTA Consultants (**attachment 4**), the car parking layout has been designed in accordance with relevant Australian Standards with each space having appropriate dimensions.



### 5.5.2 Bicycle Parking

General Section, Transportation and Access Principle 19 states:

**19 Developments should encourage and facilitate cycling as a mode of transport by incorporating end-of-journey facilities including:**

**(c) secure bicycle parking facilities provided at the rate set out in Table Sal/3 - Off Street Bicycle Parking Requirements.**

Table Sal/3 does not include specific bicycle parking rates for child care centres.

Notwithstanding this, the proposal will provide a bike rack. It is unlikely that parents will ride to the facility if they are chauffeuring children. It is also unlikely that children up to 6 years old will ride to the child care centre. On this basis, the bike rack is primarily intended for staff members and is considered to satisfy Development Plan guidelines given no particular rates are prescribed for the proposed use.

### 5.6 Interface between Land Uses

As demonstrated earlier in this report, the proposal is acceptable in terms of its setbacks, height, operating hours and shadow impacts.

General Section, Interface between Land Uses Principle 1 states:

**1 Development should not detrimentally affect the amenity of the locality or cause unreasonable interference through any of the following:**

- (a) the emission of effluent, odour, smoke, fumes, dust or other airborne pollutants**
- (b) noise**
- (c) vibration**
- (d) electrical interference**
- (e) light spill**
- (f) glare**
- (g) hours of operation**
- (h) traffic impacts.**

In accordance with Principle 1, the proposal will not emit light spill, odour, smoke, fumes or other airborne pollutant impacts. In this regard:

- Effluent will be managed in a typical manner and discharged to the local sewer system.
- The driveway and vehicle manoeuvring areas will be sealed with bitumen to prevent dust.
- Lights will be shaded and directed to prevent light spill and external building materials and finishes will be muted to prevent glare.



General Section, Interface between Land Uses, Principle 7 states:

**7 Development that emits noise (other than music noise) should include noise attenuation measures that achieve the relevant Environment Protection (Noise) Policy criteria when assessed at the nearest existing noise sensitive premises.**

Any noise generated from the site will be limited to:

- Children playing in the outdoor play areas.
- Car parking activity (people talking as they vacate or approach their vehicles, the opening and closing of vehicle doors, vehicles starting, vehicles idling, and vehicles moving into and accelerating away from their park position).
- Mechanical plant equipment (air conditioners).

The applicant has engaged Sonus (acoustic engineers) to undertake an Environmental Noise Assessment which forms **Attachment 5**.

The Environmental Noise Assessment notes the following:

- Outdoor play areas will not be used prior to 7am.
- Noise from children is specifically excluded from the Environment Protection (Noise) Policy 2007 which Principle 7 refers to. Predicted noise levels from children playing have therefore been compared against the recommendations of the Guidelines for Community Noise published by the World Health Organisation (WHO) with regard to annoyance during the day (7am and 10pm).

To ensure compliance with relevant noise criteria, the following acoustic treatments are recommended by Sonus and will be applied to the development:

- Acoustic absorption material be applied to the south-western veranda in the 'outdoor play area'.
- Acoustic insulation be applied to particular walls of the building.
- Boundary fencing to 'outdoor play areas' be airtight sealed at all junctions.
- Install particular barriers/boundary fences to particular heights.
- The storage shed be constructed from minimum BMT Colorbond.
- Mechanical equipment be located adjacent the south-western corner of the building.

On this basis, Sonus concludes:

*"the development will minimise adverse impact and conflict between land uses and not detrimentally affect the amenity of the locality or cause unreasonable interference through noise, thereby achieving the relevant provisions of the Salisbury Council Development Plan".*

## 5.7 Waste Storage and Collection

Waste from the proposed use will be stored within a purpose built enclosure designed and sited to be discreet on the subject land and when viewed from public areas. The storage unit will be of a size that is appropriate for anticipated waste generation.

Waste collection will occur outside of business hours when the car park will be empty. This is typical of many child care centres around the metropolitan area.

As per the Transport Impact Assessment prepared by GTA Consultants (**Attachment 4**), the waste collection vehicle will enter and exit the site from Evan Avenue. Sufficient space is available within the proposed car parking area to ensure safe and convenient vehicle movements for waste collection.

GTA Consultants have concluded in their assessment that:

- Waste collection can be undertaken onsite.
- A refuse vehicle can enter the site in a forward direction, collect refuse and exit the site in a forward direction.

## 5.8 Stormwater Management

PT Design have provided:

- A Stormwater Management Report with supporting calculations.
- A Stormwater Management Plan.

These documents form **Attachment 6**.

General Section, Infrastructure Principle 1 states:

**1 Development should not occur without the provision of adequate utilities and services, including:**

**(c) drainage and stormwater systems**

The site slopes approximately 0.5 metres from north to south. The finished floor level of the proposed building will be 300 millimetres above the highest adjacent top of kerb level to reduce the risk of inundation from surrounding roads during a major storm event.

A stormwater detention system will be installed as part of the development to manage the extent of stormwater leaving the site at once. All roof and paved catchment areas within the site will be directed to the stormwater detention system. All stormwater from the land will then be directed to Salisbury Highway or Evan Avenue so that no stormwater will be discharged from the subject land onto a neighbouring property.

A gross pollutant trap and grease arrestor will be provided to reduce the discharge of pollutants from the site, particularly car parking areas.

The proposal will be provided with adequate drainage and stormwater infrastructure.



## 5.9 Orderly and Sustainable Development

General Section, Orderly and Sustainable Development Objectives 3 and 4 state:

**3 Development that does not jeopardise the continuance of adjoining authorised land uses.**

**4 Development that does not prejudice the achievement of the provisions of the Development Plan.**

The proposal will not jeopardise adjoining authorised land uses or prejudice the achievement of the Development Plan. This is because:

- The proposed land use is of a scale that is envisaged in the zone.
- The proposed land use will not diminish the viability of the nearest centre zones.
- The design and appearance of the proposed building is low scale and appropriate to the locality.
- The proposal will not impact upon the amenity of the locality as explained throughout this report.
- The proposal will not detrimentally impact upon the free flow of vehicles using Salisbury Highway and Evan Avenue.
- Sufficient on-site car parking is provided.

## 6.0 Summary and Conclusion

The proposed land use will serve the local community and is a specifically envisaged land use in the zone. The suitability of such uses within zones turns primarily on matters of interface impacts and management of access and car parking and the proposal performs excellently when considered against such impact considerations

With an experienced design team, the proposal has been carefully sited and designed and features a built scale that is cohesive to its surroundings and that provides safe and convenient access for all transportation modes.

In summary, I note that the proposal will:

- Provide 114 child care spaces within an accessible location to local residents. Quality, easily accessible child care facilities are in high demand throughout South Australia particularly in this area where there are a number of young families.
- Enhance the appearance of the subject land with a purpose-built development that is sympathetic to the established character of the locality.
- Not give rise to unacceptable interface impacts by way of visual intrusion, overshadowing, noise (with support provided by Sonus).
- Provide on-site car parking that satisfies the Development Plan and has been designed in accordance with Australian Standards (with support provided by GTA).
- Discreetly store waste in a location that can be safely and conveniently collected on-site, without disruption to the surrounding road network or the locality generally.
- Manage stormwater without impact upon the surrounding locality.

For all of the reasons contained within this report, I am of the view the proposed development warrants Development Plan Consent.

Yours sincerely



Phil Harnett  
Associate

DEVELOPMENT REGULATIONS 2008  
Form of Declaration  
(Schedule 5 clause 2A)

To: Salisbury Council

From: Ashley Greiner

Date of Application: 27/9/18

Location of Proposed Development: .....

158-160  
House No: 162. Lot No: .... Street: Salisbury Hwy. Town/Suburb: Salisbury.

Section No (full/part): .....Hundred: .....

513 67  
Volume: 5400... Folio: 82.....

Nature of Proposed Development: child care centre

I Ashley Greiner.....being the applicant/  
a person acting on behalf of the applicant (delete the inapplicable statement) for  
the development described above declare that the proposed development will  
involve the construction of a building which would, if constructed in accordance  
with the plans submitted, not be contrary to the regulations prescribed for the  
purposes of section 86 of the *Electricity Act 1996*. I make this declaration under  
clause 2A(1) of Schedule 5 of the *Development Regulations 2008*.

Date: 25/9/18

Signed: 

Note 1

This declaration is only relevant to those development applications seeking  
authorisation for a form of development that involves the construction of a building  
(there is a definition of 'building' contained in section 4(1) of the *Development Act  
1993*), other than where the development is limited to -

- a) an internal alteration of a building; or
- b) an alteration to the walls of a building but not so as to alter the shape of the building.





Government of South Australia  
Department of Planning,  
Transport and Infrastructure

Product Register Search (CT 5113/67)  
Date/Time 26/09/2018 11:00AM  
Customer Reference  
Order ID 20180926003781  
Cost \$28.75



The Registrar-General certifies that this Title Register Search displays the records maintained in the Register Book and other notations at the time of searching.



## Certificate of Title - Volume 5113 Folio 67

Parent Title(s) CT 4369/692  
Creating Dealing(s) CONVERTED TITLE  
Title Issued 19/03/1993 Edition 4 Edition Issued 08/09/2017

### Estate Type

FEE SIMPLE

### Registered Proprietor

DINH NGOC BICH LE  
OF 209-211 WHITES ROAD PARALOWIE SA 5108

### Description of Land

ALLOTMENT 124 DEPOSITED PLAN 22485  
IN THE AREA NAMED SALISBURY  
HUNDRED OF YATALA

### Easements

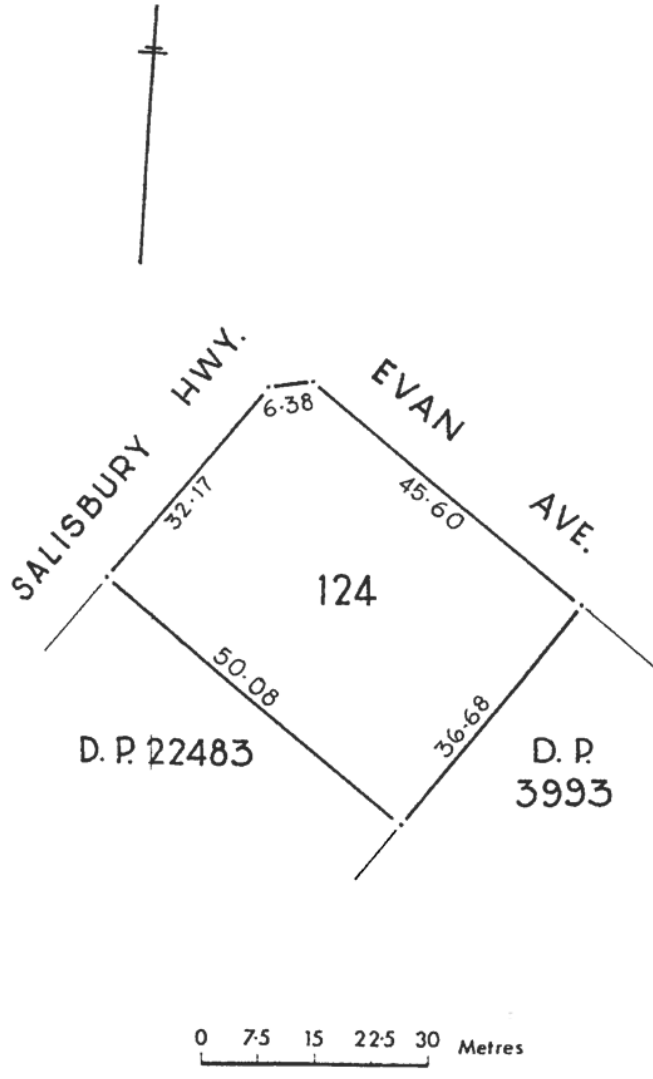
NIL

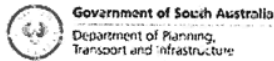
### Schedule of Dealings

Dealing Number	Description
10930302	MORTGAGE TO AUSTRALIA & NEW ZEALAND BANKING GROUP LTD.

### Notations

Dealings Affecting Title	NIL
Priority Notices	NIL
Notations on Plan	NIL
Registrar-General's Notes	NIL
Administrative Interests	NIL





Product Register Search (CT 5400/82)  
 Date/Time 26/09/2018 10:59AM  
 Customer Reference  
 Order ID 20180926003750  
 Cost \$28.75



The Registrar-General certifies that this Title Register Search displays the records maintained in the Register Book and other notations at the time of searching.



### Certificate of Title - Volume 5400 Folio 82

Parent Title(s) CT 4362/862  
 Creating Dealing(s) CONVERTED TITLE  
 Title Issued 25/02/1997 Edition 3 Edition Issued 04/12/2014

### Estate Type

FEE SIMPLE

### Registered Proprietor

THI XUAN DAO LUONG  
 OF 209-211 WHITE ROAD PARALOWIE SA 5108

### Description of Land

ALLOTMENT 122 DEPOSITED PLAN 22483  
 IN THE AREA NAMED SALISBURY  
 HUNDRED OF YATALA

### Easements

NIL

### Schedule of Dealings

Dealing Number	Description
12229646	MORTGAGE TO NATIONAL AUSTRALIA BANK LTD.

### Notations

Dealings Affecting Title	NIL
Priority Notices	NIL
Notations on Plan	NIL
Registrar-General's Notes	NIL
Administrative Interests	NIL



***ATTACHMENT 2***  
***CATEGORY 3 NOTICE AND COPY***  
***OF REPRESENTATIONS***



City of Salisbury  
ABN 82 615 416 895

12 James Street  
PO Box 8  
Salisbury SA 5108  
Australia

CATEGORY 3

Telephone 08 8406 8222  
Facsimile 08 8281 5466  
city@salisbury.sa.gov.au

TTY 08 8406 8596  
(for hearing impaired)  
www.salisbury.sa.gov.au

10 October 2018

To: The Owner/Occupier

Dear Sir/Madam

**Application No:** 361/1720/2018/3B  
**Location:** 158-160 Salisbury Highway , Salisbury SA 5108  
162 Salisbury Highway , Salisbury SA 5108  
**Nature of Development:** CHILD CARE CENTRE COMPRISING NEW BUILDING, SIGNAGE,  
BOUNDARY ACOUSTIC FENCING AND ASSOCIATED CAR  
PARKING

Enclosed is a Notice for the above proposed development, which is located near your property.

Council are interested in your views on the proposed development.

In addition to the plans enclosed, further documentation relating to the proposal is also available on the council website via the following link:

[www.salisbury.sa.gov.au/Build/Development\\_Applications/Advertised\\_Applications](http://www.salisbury.sa.gov.au/Build/Development_Applications/Advertised_Applications)

The decision whether to approve, refuse or approve with conditions, is based on an assessment of the proposal against the provisions within the City of Salisbury Development Plan. It is important to note that all comments raised, both in support of, and objecting to the proposed development, will be taken into account when assessing the application.

Please note that Australia Post has recently changed its delivery charges and timeframes meaning that correspondence may take longer to receive than anticipated. To ensure your representation is received within the statutory timeframe, you may want to consider delivering your submission electronically, direct by submission at the counter or otherwise via Express Post.

I would be happy to assist you with the consideration of the proposed development. If you have any queries or would like further information, please do not hesitate to contact me.

Yours faithfully

A handwritten signature in blue ink, appearing to read "Aaron Curtis".

**Aaron Curtis**  
Team Leader – Planning  
Development Services  
Telephone: (08) 8406 8358  
Fax: (08) 8281 5466  
Email: representations@salisbury.sa.gov.au

**DEVELOPMENT ACT 1993  
CITY OF SALISBURY**

**NOTICE OF APPLICATION FOR CATEGORY 3 DEVELOPMENT**  
Pursuant to Section 38(5) of the *Development Act 1993*

An application for development has been lodged with the Council for assessment. The details are as follows:

<b>APPLICATION NO:</b>	361/1720/2018/3B
<b>APPLICANT:</b>	Ao Salisbury Pty Ltd C/- Urps Suite 12/154 Fullarton Rd ROSE PARK SA 5067
<b>NATURE OF DEVELOPMENT:</b>	CHILD CARE CENTRE COMPRISING NEW BUILDING, SIGNAGE, BOUNDARY ACOUSTIC FENCING AND ASSOCIATED CAR PARKING
<b>LOCATION:</b>	158-160 Salisbury Highway, Salisbury SA 5108 162 Salisbury Highway, Salisbury SA 5108
<b>CERTIFICATE OF TITLE:</b>	CT-5113/67, CT-5400/82
<b>ZONE:</b>	Residential

The application may be examined at the Office of the Council located at 12 James Street, Salisbury during business hours (8.30am – 5pm Monday to Friday) and on Council's web site at [www.salisbury.sa.gov.au](http://www.salisbury.sa.gov.au). Any person or body may make representations in writing, or by email to [development@salisbury.sa.gov.au](mailto:development@salisbury.sa.gov.au), concerning this application and should address their representation to the Chief Executive Officer at PO Box 8, Salisbury or [representations@salisbury.sa.gov.au](mailto:representations@salisbury.sa.gov.au). Representations must be received **no later than Wednesday 24<sup>th</sup> October 2018**.

Each person making a submission should indicate whether they wish to appear personally, or be represented by another party, in support of their submission. Please note that should you nominate to be heard in support of your representation, you will be required to attend a Development Assessment Panel meeting held at the Council offices, scheduled on the fourth Tuesday of each month at 6.00pm (unless otherwise advised).

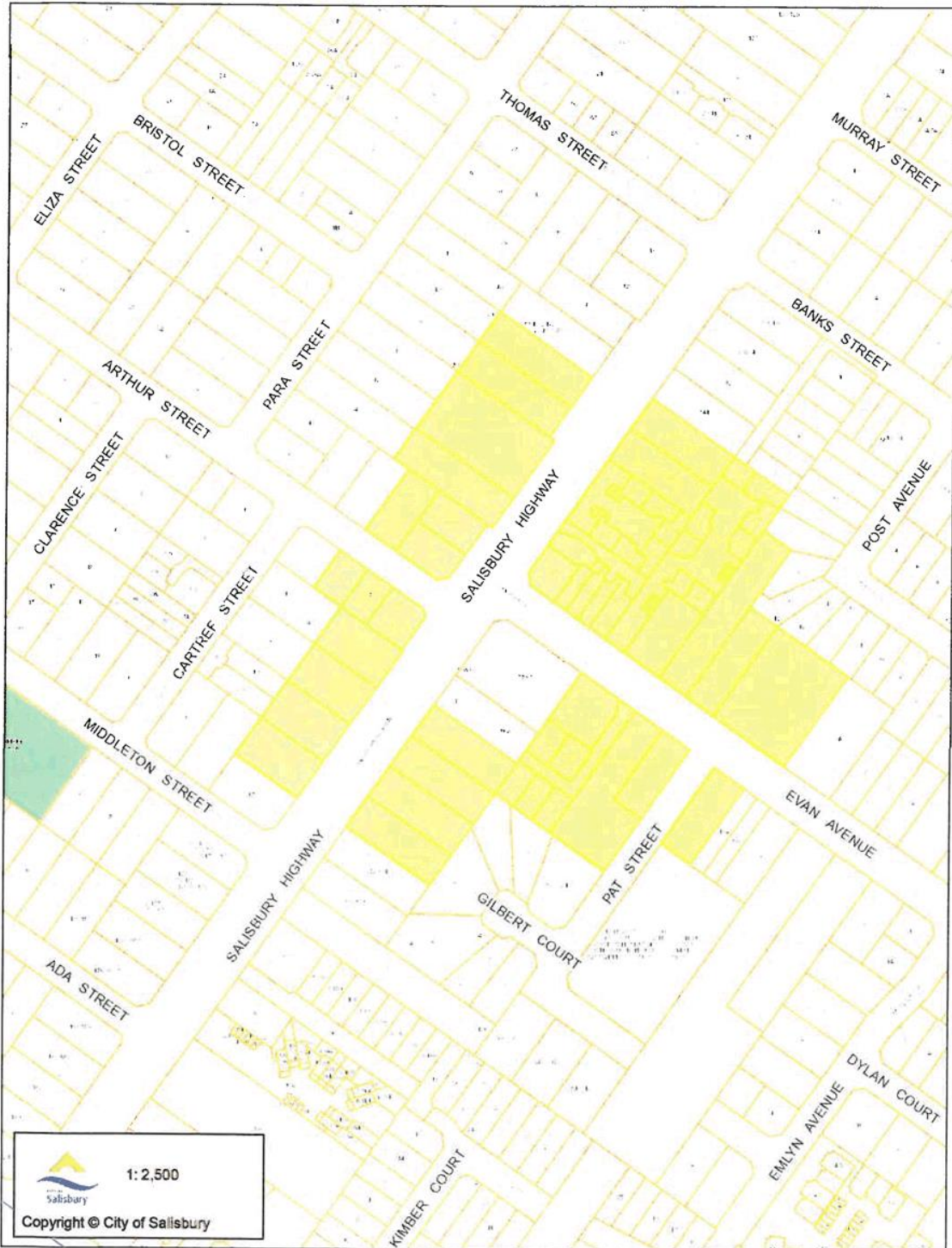
Please note that, pursuant to Section 38(8) of the *Development Act 1993*, a copy of each representation received will be forwarded to the applicant to allow them to respond to all representations received.

Signed: Aaron Curtis, Team Leader - Planning

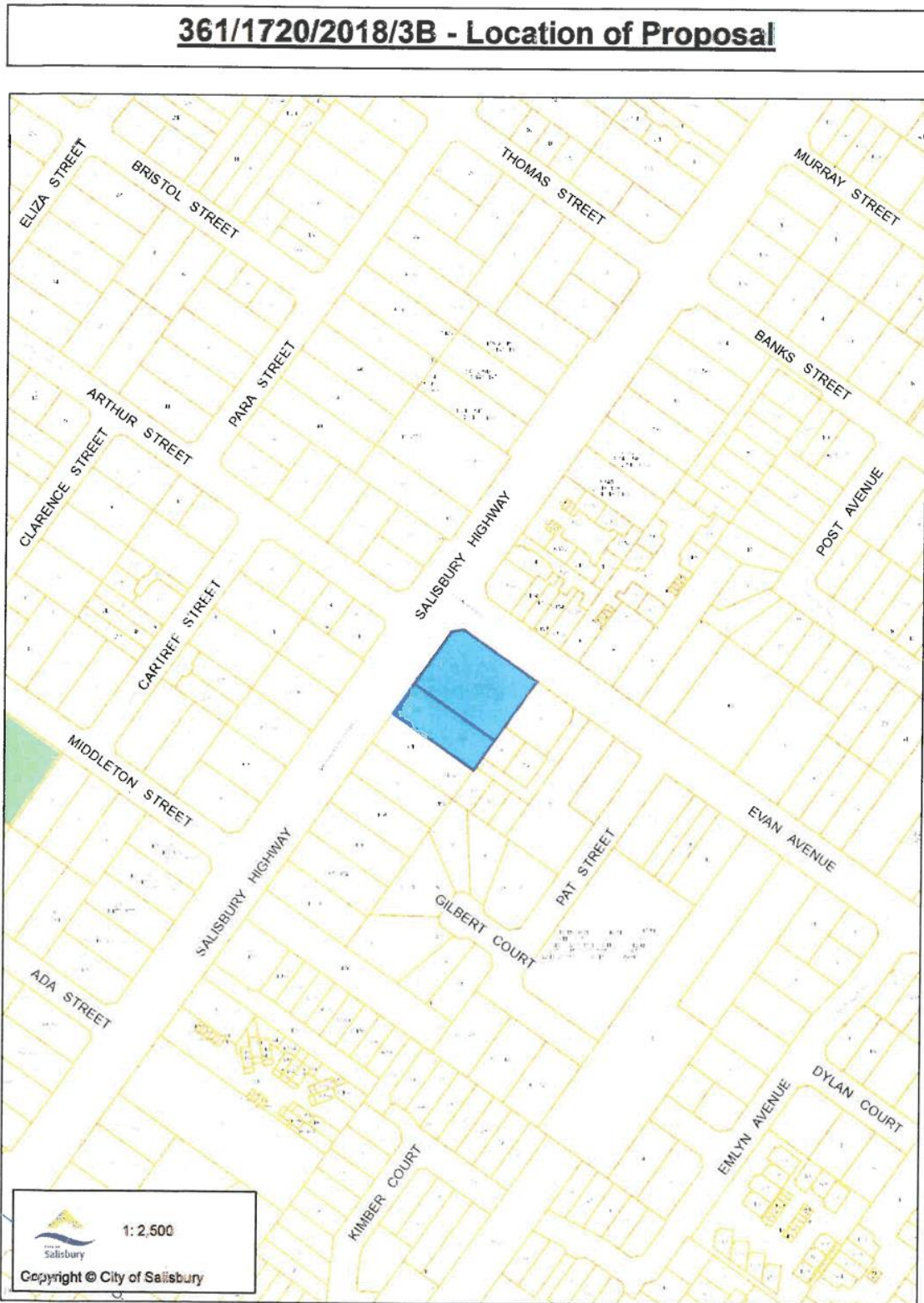
Date: 11 October 2018

***THIS IS THE FIRST AND ONLY PUBLICATION OF THIS NOTICE***

**361/1720/2018/1B - Properties Notified**







Item 5.1.1 - Attachment 2 - Category 3 Notice and Copy of Representations



City of Salisbury  
ABN: 52 615 416 895

12 James Street  
PO Box 8  
Salisbury SA 5108  
Australia

Telephone 08 8406 8222  
Facsimile 08 8281 5466  
city@salisbury.sa.gov.au

TTY 08 8406 8596  
(for hearing impaired)  
www.salisbury.sa.gov.au

29 October 2018

Ao Salisbury Pty Ltd  
C/- Urps  
Suite 12/154 Fullarton Rd  
ROSE PARK SA 5067

Dear Sir/Madam

**Applicant:** Ao Salisbury Pty Ltd  
**Application No:** 361/1720/2018/3B  
**Subject Site:** 158-160 Salisbury Highway , Salisbury SA 5108  
**Proposed Development:** CHILD CARE CENTRE COMPRISING NEW BUILDING, SIGNAGE, BOUNDARY ACOUSTIC FENCING AND ASSOCIATED CAR PARKING

The Category 3 public notification period concluded on Wednesday 24<sup>th</sup> October 2018. In total, 23 representations have been received. Please find attached copies of the submissions.

In accordance with Section 38(8) of the *Development Act 1993*, your response to the representations is requested within 10 business days from the date of this letter.

The following is a list of the names and addresses of the representations.

#### Name and Address of Representors

G M Prince	19 Evan Ave, SALISBURY SA 5108
R Tedesco	26 Evan Ave, SALISBURY SA 5108
K Woolger	164 Salisbury Hwy, SALISBURY SA 5108
N Bivone and A Bivone	159 Salisbury Hwy, SALISBURY SA 5108
L N Ghimire	1 Evan Ave, SALISBURY SA 5108
R J Tinkler	Unit 3/154 Salisbury Hwy, SALISBURY SA 5108
G Bivone	40 Immanuel Dr, GREENWITH SA 5125
N J Karandrikas	4 Arthur St, SALISBURY SA 5108
T M Williams and M Banks	6 Arthur St, SALISBURY SA 5108
C Luppino	9 Arthur St, SALISBURY SA 5108
R Caruso and K Caruso	155 Salisbury Hwy, SALISBURY SA 5108
B Robbie and V Robbie	25a Evan Ave, SALISBURY SA 5108
D F Luppino	9 Arthur St, SALISBURY SA 5108
S E Humpherson	34b Evan Ave, SALISBURY SA 5108
S Kemot	34a Evan Ave, SALISBURY SA 5108
H P Vuong	Unit 2/154 Salisbury Hwy, SALISBURY SA 5108
C J Chenoweth	7 Evan Ave, SALISBURY SA 5108
L Thomas and R Antony	16 Thorngate Dr, PARALOWIE SA 5108
G Ciccone and G Ciccone	15 Theta St, SALISBURY SA 5108
G Ciccone Nominees Pty Ltd	15 Theta St, SALISBURY SA 5108
J H Readings and S J Readings	163 Salisbury Hwy, SALISBURY SA 5108
S P Collins and A Sargent	Unit 8/6 Evan Ave, SALISBURY SA 5108
A J Lennox	Unit 7/6 Evan Ave, SALISBURY SA 5108



City of Salisbury  
 ABN 52 615 416 895  
 12 James Street  
 PO Box 8  
 Salisbury SA 5108  
 Australia

Telephone 08 8406 8222  
 Facsimile 08 8281 5466  
 city@salisbury.sa.gov.au  
 TTY 08 8406 8596  
 (for hearing impaired)  
 www.salisbury.sa.gov.au

In addition to responding to the representations, could you please respond to the following matters, requested pursuant to Section 39 of the *Development Act 1993*:

1. A Civil Siteworks and Drainage Plan, prepared by an experienced civil engineer is requested for the proposed development and should include:
  - i) Building finished floor levels;
  - ii) Design pavement levels and grades;
  - iii) Cut/fill and any retaining walls;
  - iv) Stormwater pit, pipe sizes and detention area/size;
  - v) Kerbing types;
  - vi) Internal paths.

Please submit the Civil Siteworks and Drainage Plan.

2. Thank you for submitting a Landscape Concept Plan for review/assessment. The plan is in principle acceptable, however, it is lacking details in terms of the final plant mix/schedule. A further plan should be submitted which details the species/types to be used, noting that a mix of shade trees, shrubs and groundcovers should be incorporated using predominantly native species. Shade trees should provide amenity and visual relief within the car parking areas. In addition, consideration should be given to use of low level hedge/plantings adjacent to Salisbury Highway and the Evan Avenue access in order to maximise traffic sightlines.
3. Further to item 2, Under the General Section, "Crime Prevention" module, Principle of Development Control 6 states that:

- 6** *Landscaping should be used to assist in discouraging crime by:*
- (a) *Screen planting areas susceptible to vandalism;*
  - (b) *Planting trees or ground covers, rather than shrubs, alongside footpaths;*
  - (c) *Planting vegetation other than ground covers a minimum distance of two metres from footpaths to reduce concealment opportunities.*

The Sonus Site Plan (Figure 1) shows that a 2.4m high barrier will be established to portion of the Evan Avenue boundary. This fence provides limited relief and may be susceptible to vandalism. It is acknowledged that the fence is required for acoustic attenuation, however, there is scope to accommodate the fence within the site (ie. stepping fence back 1m for example) to achieve the required acoustic attenuation while also providing landscaping in front to deter graffiti of the fence and to provide a high quality presentation to Evan Avenue. Please consider this aspect of the development layout and provide further detail/amended plans.

4. Under the General Section, "Landscaping, Fencing and Walls" module, Principle of Development Control 2 states that:

- 2** *Landscaping should:*
- (a) *Include the planting of locally indigenous species where appropriate;*
  - (b) *Be oriented towards the street frontage;*
  - (c) *Result in the appropriate clearance from powerlines and other infrastructure being maintained.*



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The proposed site plan incorporates a 1m landscaping strip adjacent to the Salisbury Highway boundary and portion of the Evan Avenue boundary, however, the landscaping strip is only 1m in depth and due to the limited depth of the car parks (4.8m), vehicles will overhang into the landscaping strip, thus compromising what is a limited depth landscaping strip thus compromising the plantings. Accordingly, it is suggested that the depth of the car parks be increased and inclusion of wheel stops to avoid encroachment into the landscaping bed. It is also suggested the landscaping strip should be increased in depth to enable more substantive plantings to be accommodated. It is noted that the 7m aisle could possibly be reduced in width in order to partly accommodate the increased car parking depth and increased depth of the landscaping bed.

Further, car parking spaces 25-29 inclusive have a depth of 4.8m, thus meaning they will encroach into the adjacent footpath. It is suggested that wheel stops be incorporated and the depth of the car parking spaces be increased in order to avoid conflict with the footpath.

This aspect of the development should be reviewed.

5. The plan incorporates an "enclosed bin storage" adjacent to car parking spaces 24 and 25. The traffic report confirms that a 10m long refuse vehicle will be able to enter the site via Evan Avenue in order to collect refuse. It is noted that garbage truck collection will take place *'outside of business operating hours when the car park will be empty (ie. early morning or weekends'*. While this is *'in-principle'* acceptable, please confirm:
  - a) Timing of collection, noting that waste collection should not occur outside the hours of 7am and 7pm on Monday-Saturdays or before 9am or after 7pm on any Sunday or public holiday;
  - b) Expected frequency of collection;
  - c) Size/type and number of receptacles and evidence that this is adequate to service the intended number of children/staff and can be accommodated within the enclosed bin storage area.

The Northern Adelaide Waste Management Authority (NAWMA) provide a basic collection service, however, should the proposal necessitate collection that exceeds the standard collection, a private service may be arranged. Note that collection within the site (whether through NAWMA or other provider) will require a collection agreement to be in place.

Please respond to the above matters within 30 days from the date of this letter. Please note the Commissioner of Highways is yet to provide a response in accordance with Schedule 8(3) of the *Development Regulations 2008*.

Should you require any assistance or further advice please do not hesitate to contact me on the details below.

Yours sincerely

**Aaron Curtis**  
Team Leader - Planning  
Phone: 08 8406 8367  
Email: development@salisbury.sa.gov.au

DW

**Cassandra White**

---

**From:** Lijesh Thomas [REDACTED]  
**Sent:** Wednesday, 24 October 2018 2:16 PM  
**To:** Development  
**Subject:** Application no: 361/1720/2018/3B  
**Attachments:** 24101800.PDF

Hi Aaron,

Please see attached our Statement of Representation re: application 361/1720/2018/3B.

Thanks and regards,

Lijesh Thomas &  
Remya Antony



**STATEMENT OF REPRESENTATION**  
Pursuant to Section 38 of the *Development Act 1993*

23 OCT 2018

**To:** City of Salisbury  
PO Box 8, SALISBURY SA 5108  
Email: [representations@salisbury.sa.gov.au](mailto:representations@salisbury.sa.gov.au)

THIS SHEET PROVIDES YOU WITH THE OPPORTUNITY TO MAKE COMMENTS IN RELATION TO A PROPOSED DEVELOPMENT. PLEASE FIND ATTACHED DETAILS OF THE PROPOSED DEVELOPMENT.

<b>Application Number:</b>	361/1720/2018/3B
<b>Applicant</b>	Ao Salisbury Pty Ltd
<b>Nature of Development:</b>	CHILD CARE CENTRE COMPRISING NEW BUILDING, SIGNAGE, BOUNDARY ACOUSTIC FENCING AND ASSOCIATED CAR PARKING
<b>Location:</b>	158-160 Salisbury Highway, Salisbury SA 5108, 162 Salisbury Highway, Salisbury SA 5108

**YOUR DETAILS:** (this information must be provided to ensure that this is a valid representation)

NAME(S): Rosa Tedesco

ADDRESS: 26 Evan Ave Salisbury SA 5108

PHONE NO: [REDACTED] EMAIL: .....

I am: (please tick one of the following boxes as appropriate)

- The owner/occupier of the property located at: 26 Evan Ave Salisbury
- Other (please state): .....

**YOUR COMMENTS:**

I/We: (please tick the most appropriate box below)

- Support the proposed development.
- Oppose the proposed development.

**Whether you support or oppose this proposal you must provide written reasons below to ensure that this is a valid representation.**

• Too much traffic

• It will make our street ugly and very busy

• lots of disruption to traffic coming in and out of our street we will lose parking in

PTO



Regulation 35(e) of the *Development Regulations 2008* requires that a representation must indicate a person's desire to be heard. Please note that if you do not indicate that you wish to be heard, it will be taken that you do not wish to be heard by the Panel.

I/We:

- Do not wish to be heard in support of my representation.
- Wish to be heard in support of my representation, and I will be:

- Appearing personally,

**OR**

- Represented by the following person: Joe Bivoni

Contact details: .....

*(Please note, matters raised in your written representation will be considered during the assessment and do not need to be repeated at the hearing).*

**Your written representation must be received by Council no later than 11.59pm on Wednesday 24<sup>th</sup> October 2018, to ensure that it is a valid representation and taken into account.**

**Representor's Declaration:**

I am aware that the representation will become a public document as prescribed in the *Freedom of Information Act 1991*, and will be made available to the applicant, agencies and other bodies pursuant to the *Development Act 1993*, and may be uploaded to the Council's website as an attachment to the hearing agenda.

Signature: R. Lebr...

Date: 23 / 10 / 18

**Please complete this checklist to ensure your representation is valid:**

- Name and address of person (or persons).
- If more than one person, details of person making the representation.
- Detail of reasons for making the representation.
- Indication whether or not the person (or persons) wishes to be heard.
- Submitted no later than 11.59pm on **Wednesday 24<sup>th</sup> October 2018**.





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23 OCT 2018

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<b>Location:</b>	158-160 Salisbury Highway, Salisbury SA 5108, 162 Salisbury Highway, Salisbury SA 5108

**YOUR DETAILS:** (this information must be provided to ensure that this is a valid representation)

NAME(S): Gina Prince

ADDRESS: 19 Euan Ave Salisbury

PHONE NO: [REDACTED] EMAIL: .....

I am: (please tick one of the following boxes as appropriate)

- The owner/occupier of the property located at: 19 Euan Ave Salisbury
- Other (please state): .....

**YOUR COMMENTS:**

I/We: (please tick the most appropriate box below)

- Support the proposed development.
- Oppose the proposed development.

**Whether you support or oppose this proposal you must provide written reasons below to ensure that this is a valid representation.**

• too much traffic

• It will make our street ugly and very busy

• lots of disruptions to traffic coming in and out of our street. We will lose parking in

PTO



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I/We:

- Do not wish to be heard in support of my representation.
- Wish to be heard in support of my representation, and I will be:
  - Appearing personally,

OR

- Represented by the following person: ..... *Joe Bivony* .....
- Contact details: ..... *[Signature]* .....

*(Please note, matters raised in your written representation will be considered during the assessment and do not need to be repeated at the hearing).*

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Signature: ..... *[Signature]* ..... Date: *23/10/18*

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**STATEMENT OF REPRESENTATION**  
Pursuant to Section 38 of the *Development Act 1993*

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PO Box 8, SALISBURY SA 5108  
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<b>Location:</b>	158-160 Salisbury Highway, Salisbury SA 5108, 162 Salisbury Highway, Salisbury SA 5108

**YOUR DETAILS:** (this information must be provided to ensure that this is a valid representation)

NAME(S): LIJESH THOMAS & REMYA ANTONY

ADDRESS: 16 THORNGATE DRIVE PARALOWIE SA-5108

PHONE NO: ... [REDACTED] ... EMAIL: ... [REDACTED]

I am: (please tick one of the following boxes as appropriate)

- The owner/occupier of the property located at: 4/3 EVAN AVE SALISBURY
- Other (please state): .....

**YOUR COMMENTS:**

I/We: (please tick the most appropriate box below)

- Support the proposed development.
- Oppose the proposed development.

**Whether you support or oppose this proposal you must provide written reasons below to ensure that this is a valid representation.**

We support the child care centre as it brings more employment and development to Salisbury. Only concern is with the dogs of the tenants at 4/3 EVAN AVE. The constant barking of the

PTO



Regulation 35(e) of the *Development Regulations 2008* requires that a representation must indicate a person's desire to be heard. Please note that if you do not indicate that you wish to be heard, it will be taken that you do not wish to be heard by the Panel.

I/We:

- Do not wish to be heard in support of my representation.
- Wish to be heard in support of my representation, and I will be:
  - Appearing personally,
  - OR**
  - Represented by the following person: .....
  - Contact details: .....

*(Please note, matters raised in your written representation will be considered during the assessment and do not need to be repeated at the hearing).*

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Signature:  Date: 22 / 10 / 2018

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**STATEMENT OF REPRESENTATION**  
Pursuant to Section 38 of the *Development Act 1993*

1857  
21 Oct

To: City of Salisbury  
PO Box 8, SALISBURY SA 5108  
Email: [representations@salisbury.sa.gov.au](mailto:representations@salisbury.sa.gov.au)

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<b>Location:</b>	158-160 Salisbury Highway, Salisbury SA 5108, 162 Salisbury Highway, Salisbury SA 5108

**YOUR DETAILS:** (this information must be provided to ensure that this is a valid representation)

NAME(S): .....

ADDRESS: .....

PHONE NO: ..... EMAIL: .....

I am: (please tick one of the following boxes as appropriate)

- The owner/occupier of the property located at: T EVAN AVE
- Other (please state): P. O. KEYSWETA

**YOUR COMMENTS:**

I/We: (please tick the most appropriate box below)

- Support the proposed development.
- Oppose the proposed development.

**Whether you support or oppose this proposal you must provide written reasons below to ensure that this is a valid representation.**

I HAVE LIVED HERE FOR MANY YEARS - ALMOST 30  
I DON'T WANT TO HAVE SUCH A BIG APPROVAL  
SO CLOSE TO MY HOME, AFTER ALL, IT'S NOT  
SUCH A GOOD IDEA TO HAVE SUCH A PROPERTY  
AMONG PRIVATE HOMES.

PTO

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I/We:

- Do not wish to be heard in support of my representation.
- Wish to be heard in support of my representation, and I will be:

- Appearing personally,

OR

- Represented by the following person: ..... *PUT UP THIS DEVELOPEMENT IN ROSE AR!*

Contact details: .....

*(Please note, matters raised in your written representation will be considered during the assessment and do not need to be repeated at the hearing).*

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Signature: ..... *[Handwritten Signature]*

Date: *20/10/18*

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<b>Location:</b>	158-160 Salisbury Highway, Salisbury SA 5108, 162 Salisbury Highway, Salisbury SA 5108

**YOUR DETAILS:** (this information must be provided to ensure that this is a valid representation)

NAME(S): JAMES & SUSAN READINGS  
ADDRESS: 163 SALISBURY HWY SALISBURY SA 5108  
PHONE NO: [REDACTED] EMAIL: [REDACTED]

I am: (please tick one of the following boxes as appropriate)

- The owner/occupier of the property located at: 163 SALISBURY HWY
- Other (please state):

**YOUR COMMENTS:**

I/We: (please tick the most appropriate box below)

- Support the proposed development.
- Oppose the proposed development.

**Whether you support or oppose this proposal you must provide written reasons below to ensure that this is a valid representation.**

WE OPPOSE THIS PROPOSED DEVELOPMENT BECAUSE OF INCREASED NOISE ACTIVITY INTO OUR LOUNGE & BED ROOMS FROM A PARKING BAY OUTSIDE OUR PROPERTY AND DIRECTLY ACROSS FROM THE PROPOSED DEVELOPMENT THIS INCLUDES CARS STOPPING/STARTING CAR DOORS CLOSING, PARENTS AND CHILDRENS VOICES WHICH ARE ELEVATED AT TIMES.

PTO

361/1720/2018/3B

ALSO VOICES ESPECIALLY PLAY TIMES AND EXTRA VEHICLE NOISES FROM THE PROPOSED DEVELOPMENT. ACCESS IN AND OUT OF OUR PROPERTY WILL BE AFFECTED WITH VEHICLES PARKED CLOSE TO OR ON THE EXIT OF OUR PROPERTY WE HAVE ALREADY GOT LIMITED VISION TO SEE TRAFFIC COMING DOWN THE SALISBURY HWY. THIS WILL DRAMATICALLY INCREASE WHEN LEAVING OUR PROPERTY

OTHER CONCERNS

DUTY OF CARE FROM THE COUNCIL KNOWING PARENTS AND CHILDREN WILL BE CROSSING ONE SIDE OF THE BUSY SALISBURY HWY TO THE OTHER WITH OUT A PROPER CROSSING ALSO THE EXTRA STRESS AND AGGRAVATION TO ALL MOTORISTS TURNING INTO OR OUT OF EVAN AVENUE WITH THE EXTRA VEHICLES ASSOCIATED WITH THE NEW DEVELOPMENT

**My concerns would be addressed by:** *(state changes/actions to the proposal sought)*

PTO

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I/We:

- Do not wish to be heard in support of my representation.
- Wish to be heard in support of my representation, and I will be:

Appearing personally,

**OR**

Represented by the following person: .....

Contact details: .....

*(Please note, matters raised in your written representation will be considered during the assessment and do not need to be repeated at the hearing).*

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Signature: *[Handwritten Signature]*.....

Date: *20/10/2018*

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**YOUR DETAILS:** (this information must be provided to ensure that this is a valid representation)

NAME(S): Gaetano & Giuseppina Ciccone / G. Ciccone Nominees

ADDRESS: 15 Theta St Salisbury / 148<sup>148</sup> Salisbury Hwy Salisbury Pty Ltd

PHONE NO: [REDACTED] EMAIL: [REDACTED]

I am: (please tick one of the following boxes as appropriate)

- The owner/occupier of the property located at: 148<sup>148</sup> / Salisbury Hwy Salisbury
- Other (please state): .....

**YOUR COMMENTS:**

I/We (please tick the most appropriate box below)

- Support the proposed development. with conditions
- Oppose the proposed development.

**Whether you support or oppose this proposal you must provide written reasons below to ensure that this is a valid representation.**

1. The increase in traffic for Evan Ave and Salisbury Hwy needs to be addressed. 2. There will be increased risk of accidents with people turning right onto Salisbury Hwy from Evan Ave this needs to be

PTO

361/1720/2018/3B

taken into consideration.  
3. A large child care facility needs ample parking for staff and parents. Our concern is that parents will perhaps use our car park to park their cars then walk their child/children to the child care facility. Ample parking for parents must be available at all times.

4. Congestion in Evan Ave with parents parking at on both sides of the street must also be addressed as cars turn left into Evan Ave quickly and could cause accidents if there is not enough road space. This must also be addressed eg with "no parking" signage or yellow lines.

**My concerns would be addressed by:** (state changes/actions to the proposal sought)

- Slip lanes and signage need to be installed.
- No parking and yellow lines need to be put on The side street.
- More parking inside the child care facility needs to be made available.

PTO

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I/We:

- Do not wish to be heard in support of my representation.
- Wish to be heard in support of my representation, and I will be:
  - Appearing personally,
  - OR**
  - Represented by the following person: .....
  - Contact details: .....

*(Please note, matters raised in your written representation will be considered during the assessment and do not need to be repeated at the hearing).*

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*\*Last time this happened my email address became public knowledge - I don't want this to happen again.*

Signature: *[Signature]* Date: 20/10/18

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<b>Application Number:</b>	361/1720/2018/3B
<b>Applicant</b>	Ao Salisbury Pty Ltd
<b>Nature of Development:</b>	CHILD CARE CENTRE COMPRISING NEW BUILDING, SIGNAGE, BOUNDARY ACOUSTIC FENCING AND ASSOCIATED CAR PARKING
<b>Location:</b>	158-160 Salisbury Highway, Salisbury SA 5108, 162 Salisbury Highway, Salisbury SA 5108

**YOUR DETAILS:** (this information must be provided to ensure that this is a valid representation)

NAME(S): MRS A SERGENT AND MR S COLLINS

ADDRESS: UNIT 8 / 6 EVAN AVENUE SALISBURY SA 5108

PHONE NO: [REDACTED] EMAIL: [REDACTED]

I am: (please tick one of the following boxes as appropriate)

- The owner/occupier of the property located at: UNIT 8 / 6 EVAN AVE SALISBURY
- Other (please state): .....

**YOUR COMMENTS:**

I/We: (please tick the most appropriate box below)

- Support the proposed development.
- Oppose the proposed development.

**Whether you support or oppose this proposal you must provide written reasons below to ensure that this is a valid representation.**

ANOTHER CHILD CARE CENTRE WILL BE GOOD FOR THIS AREA AND EXCELLENT TO SEE THAT MORE EMPLOYMENT IS COMING FOR SALISBURY AND ALSO GOOD THAT THE LAND IS USED FOR SOMETHING BECAUSE THE CURRENT HOUSE THERE ON THAT LAND

PTO









**STATEMENT OF REPRESENTATION**  
Pursuant to Section 38 of the *Development Act 1993*

To: City of Salisbury  
PO Box 8, SALISBURY SA 5108  
Email: [representations@salisbury.sa.gov.au](mailto:representations@salisbury.sa.gov.au)

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<b>Location:</b>	158-160 Salisbury Highway, Salisbury SA 5108, 162 Salisbury Highway, Salisbury SA 5108

**YOUR DETAILS:** (this information must be provided to ensure that this is a valid representation)

NAME(S): Mr. Anthony Lemson

ADDRESS: Unit 7 / 6 Egan Ave. Salisbury

PHONE NO: [REDACTED] EMAIL: [REDACTED]

I am: (please tick one of the following boxes as appropriate)

- The owner/occupier of the property located at: .....
- Other (please state): RENTING

**YOUR COMMENTS:**

I/We: (please tick the most appropriate box below)

- Support the proposed development.
- Oppose the proposed development.

**Whether you support or oppose this proposal you must provide written reasons below to ensure that this is a valid representation.**

On the assumption that the centre is in step with local educational requirements.

PTO



Regulation 35(e) of the *Development Regulations 2008* requires that a representation must indicate a person's desire to be heard. Please note that if you do not indicate that you wish to be heard, it will be taken that you do not wish to be heard by the Panel.

I/We:

- Do not wish to be heard in support of my representation.
- Wish to be heard in support of my representation, and I will be:
  - Appearing personally,
  - OR**
  - Represented by the following person: .....
  - Contact details: .....

*(Please note, matters raised in your written representation will be considered during the assessment and do not need to be repeated at the hearing).*

**Your written representation must be received by Council no later than 11.59pm on Wednesday 24<sup>th</sup> October 2018, to ensure that it is a valid representation and taken into account.**

**Representor's Declaration:**

I am aware that the representation will become a public document as prescribed in the *Freedom of Information Act 1991*, and will be made available to the applicant, agencies and other bodies pursuant to the *Development Act 1993*, and may be uploaded to the Council's website as an attachment to the hearing agenda.

Signature: A. J. Jensen Date: 12<sup>th</sup> / 10 / 2018

**Please complete this checklist to ensure your representation is valid:**

- Name and address of person (or persons).
- If more than one person, details of person making the representation.
- Detail of reasons for making the representation.
- Indication whether or not the person (or persons) wishes to be heard.
- Submitted no later than 11.59pm on **Wednesday 24<sup>th</sup> October 2018**.



**STATEMENT OF REPRESENTATION**  
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<b>Location:</b>	158-160 Salisbury Highway, Salisbury SA 5108, 162 Salisbury Highway, Salisbury SA 5108

**YOUR DETAILS:** (this information must be provided to ensure that this is a valid representation)

NAME(S): MS Kelle wadger

ADDRESS: 164 Salisbury Hwy Salisbury

PHONE NO: [REDACTED] EMAIL: [REDACTED]

I am: (please tick one of the following boxes as appropriate)

- The owner/occupier of the property located at: .....
- Other (please state): TENNANT

**YOUR COMMENTS:**

I/We: (please tick the most appropriate box below)

- Support the proposed development.
- Oppose the proposed development.

**Whether you support or oppose this proposal you must provide written reasons below to ensure that this is a valid representation.**

I OPPOSE it Due to it will cause traffic Problems, noise Problems from start of knock down & till forever, I live NOT far to said devel development, once Built the

PTO

Item 5.1.1 - Attachment 2 - Category 3 Notice and Copy of Representations

361/1720/2018/3B

Play areas for children will be right outside my bedroom windows, no fence stops the sound of children playing ect. I'm a Grandmother of 2 both under 5, they are little so I know how loud little ones are and no fence will make it quieter. There is no need for another child care centre a block up from a new one being done now. &

This is a residential area for homes and people to live. Not a commercial or business zone. That's where it should be built not where housing can go when housing is needed more than another child care centre.

**My concerns would be addressed by:** (state changes/actions to the proposal sought)

Traffic jams, problems and accidents over 100 children next door right on a very bad highway. &

I'm a tenant and have lived here for over 5 years and I was happy to stay for another but with the amount of rent I pay was fine until this proposed development. The noise of bulldozing 2 houses ect then construction it would be unbearable for all in area. It's zoned residential, that land should be used for much needed housing over PTO child care. It should be in business or commercial. Not residential.

Regulation 35(e) of the *Development Regulations 2008* requires that a representation must indicate a person's desire to be heard. Please note that if you do not indicate that you wish to be heard, it will be taken that you do not wish to be heard by the Panel.

I/We:

- Do not wish to be heard in support of my representation.
- Wish to be heard in support of my representation, and I will be:

Appearing personally,

**OR**

Represented by the following person: Joe Biyane.....

Contact details: [REDACTED].....

*(Please note, matters raised in your written representation will be considered during the assessment and do not need to be repeated at the hearing).*

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Signature: [Signature].....

Date: 23/10/18.

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<b>Location:</b>	158-160 Salisbury Highway, Salisbury SA 5108, 162 Salisbury Highway, Salisbury SA 5108

**YOUR DETAILS:** (this information must be provided to ensure that this is a valid representation)

NAME(S): N + A BIVONE

ADDRESS: 159 Salisbury Highway, Salisbury SA

PHONE NO: [REDACTED] .. EMAIL: ..

I am: (please tick one of the following boxes as appropriate)

- The owner/occupier of the property located at: as above
- Other (please state): .....

**YOUR COMMENTS:**

I/We: (please tick the most appropriate box below)

- Support the proposed development.
- Oppose the proposed development.

**Whether you support or oppose this proposal you must provide written reasons below to ensure that this is a valid representation.**

It will be a Traffic Disaster. Not enough car parks supplied at the Centre. Parking will be a problem for everybody. The building will look like a Warehouse, and we live opposite so we will be looking at it each day.

PTO The site is facing Salisbury Highway and will be dangerous should children go out the gate.



361/1720/2018/3B

This area is zoned "Residential" and must remain for houses only.

We strongly oppose the proposal and we will refuse to pay future council rates if this proposal goes ahead. The Developer should not be given consideration ahead of the wishes of most of the Residents in the Area.

**My concerns would be addressed by:** (state changes/actions to the proposal sought)

The CC Centre being built in a safer area where existing <sup>residents</sup> ~~houses~~ are not going to be subjected to parking problems and excessive noise from a large Child Care Centre. Salisbury Highway is extremely busy and we have had too many accidents already. I repaired fences and walls adjacent to the proposed site and there was another fence at No. 166 destroyed by a car a few weeks ago.

PTO

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I/We:

- Do not wish to be heard in support of my representation.
- Wish to be heard in support of my representation, and I will be:

Appearing personally,

OR

Represented by the following person: my brother G. Joe Bivone.....

Contact details: [REDACTED] 40 Immanuel Dr  
Greenwith S.A. 5125

*(Please note, matters raised in your written representation will be considered during the assessment and do not need to be repeated at the hearing).*

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Signature: [Signature].....

Date: 20 / 10 / 18

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<b>Location:</b>	158-160 Salisbury Highway, Salisbury SA 5108, 162 Salisbury Highway, Salisbury SA 5108

**YOUR DETAILS:** (this information must be provided to ensure that this is a valid representation)

NAME(S): MARK BANKS & TRINA WILLIAMS

ADDRESS: 6 ARTHUR STREET, SALISBURY, SA 5108

PHONE NO: [REDACTED] ..... EMAIL [REDACTED]

*Next* am: (please tick one of the following boxes as appropriate)

- The owner/occupier of the property located at: .....
- Other (please state): renting tenants

**YOUR COMMENTS:**

I/We: (please tick the most appropriate box below)

- Support the proposed development.
- Oppose the proposed development.

**Whether you support or oppose this proposal you must provide written reasons below to ensure that this is a valid representation.**

→ this will increase traffic flow on salisbury highway & cause congestion with cars pulling up suddenly to enter/exiting the premises  
→ with 120 places available for children plus staff - the 29 car parks provided will not accommodate the numbers thereby parents/guardians resorting to using local side streets to facilitate drop offs/pick ups  
 PTO

361/1720/2018/3B

- following from previous comment - depending on the direction coming from, parents will park in side streets on other side of highway thereby requiring themselves & their children having to negotiate the traffic in peak hours - ~~safe~~ incredibly dangerous for both them & other drivers impacting through flow of traffic on busy road.
- we have concerns for the occupiers of properties bordering the complex will be impacted significantly with the noise of children/careers at a multitude of times of the day with the hours that child care centres must run with <sup>Also there will be noise</sup> with ~~from~~ <sup>from</sup> A/C generators.
- The entry/exit gates are only single lock & not a dual set of gates to prevent accidental exiting of children (ie gates currently seen at parks/playgrounds)
- Visual impact of the building (proposed) does not "fit" with rest of the area/suburb & looks like a large warehouse.

**My concerns would be addressed by:** (state changes/actions to the proposal sought)

I don't feel that the position of the complex is the most appropriate for the type of business being undertaken here. This is going to see ~~have~~ a large number of young & ~~at~~ times unpredictable children that need to be fully protected at all times. Placing this business on what is an incredibly busy thoroughfare (including large trucks) is completely inappropriate. Based on the above reasoning & added with increased local traffic & noise there is nothing that could alter our decision at this point in time.

PTO

Regulation 35(e) of the *Development Regulations 2008* requires that a representation must indicate a person's desire to be heard. Please note that if you do not indicate that you wish to be heard, it will be taken that you do not wish to be heard by the Panel.

I/We:

- Do not wish to be heard in support of my representation.
- Wish to be heard in support of my representation, and I will be:
  - Appearing personally,
  - OR**
  - Represented by the following person: .....
  - Contact details: .....

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Signature:  ..... Date: 22 / 10 / 18

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<b>Location:</b>	158-160 Salisbury Highway, Salisbury SA 5108, 162 Salisbury Highway, Salisbury SA 5108

**YOUR DETAILS:** (this information must be provided to ensure that this is a valid representation)

NAME(S): Lok Nath Ghimire

ADDRESS: 1 Evan Avenue

PHONE NO: [REDACTED] ..... EMAIL: [REDACTED]

I am: (please tick one of the following boxes as appropriate)

- The owner/occupier of the property located at: .....
- Other (please state): .....

**YOUR COMMENTS:**

I/We: (please tick the most appropriate box below)

- Support the proposed development.
- Oppose the proposed development.

**Whether you support or oppose this proposal you must provide written reasons below to ensure that this is a valid representation.**

I just live next to the childcare center and also next to outdoor playground. It's going to very noisy and I believe my property value will drop down. The extra traffic and lack of parking space will

PTO

361/1720/2018/3B

means people parking in front of my house.  
Because of the extra traffick it will  
make more dangerous.

**My concerns would be addressed by:** *(state changes/actions to the proposal sought)*

Not having centre built next door of  
my property. Build it some were else  
more suitable.

PTO

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I/We:

- Do not wish to be heard in support of my representation.
- Wish to be heard in support of my representation, and I will be:

Appearing personally,

**OR**

Represented by the following person: Rupa Ghimire.....

Contact details: [REDACTED].....

1 Evan Avenue

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Signature: .....

Date: 22/10/2018

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**YOUR DETAILS:** (this information must be provided to ensure that this is a valid representation)

NAME(S): BRIAN + VALMAI ROBBIE

ADDRESS: 25A EVAN AVE SALISBURY 5108

PHONE NO: [REDACTED] EMAIL: [REDACTED]

I am: (please tick one of the following boxes as appropriate)

- The owner/occupier of the property located at: 25A EVAN AVE SALISBURY 5108
- Other (please state): .....

**YOUR COMMENTS:**

We: (please tick the most appropriate box below)

- Support the proposed development.
- Oppose the proposed development.

**Whether you support or oppose this proposal you must provide written reasons below to ensure that this is a valid representation.**

WITH REFERENCE TO THIS PROPOSAL WE FEEL THAT OVER PARKING IN EVAN AVE WOULD BE A PROBLEM AS VALMAI REQUIRES THE USE OF A MOBILITY SCOOTER WHICH MEANS HAVING TO CROSS THE ROAD IN ORDER TO ACCESS THE PAVEMENT

PTO

Item 5.1.1 - Attachment 2 - Category 3 Notice and Copy of Representations

361/1720/2018/3B

IT IS ALSO A BIT OF A CONCERN TO US  
ABOUT EMERGENCY VEHICLES AMBULANCES,  
FIRE, WATER ???

ALL ISSUES

**My concerns would be addressed by:** (state changes/actions to the proposal sought)

SAFETY OF CHILDREN ?? ACCESSABILITY +  
PARKING ISSUES ALSO "SAFETY OF  
PARENTS PICK UP + DROP OFF ISSUES  
ON BUSY ROAD ??  
NO PEDESTRIAN CROSSINGS  
NO LIGHTS

TOO DANGEROUS !!!

PTO

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- Wish to be heard in support of my representation, and I will be:
  - Appearing personally,
  - OR**
  - Represented by the following person: .....
  - Contact details: .....

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Signature:  ..... Date: 22 / 10 / 18

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**STATEMENT OF REPRESENTATION**  
Pursuant to Section 38 of the *Development Act 1993*

RECEIVED  
24 OCT

To: City of Salisbury  
PO Box 8, SALISBURY SA 5108  
Email: [representations@salisbury.sa.gov.au](mailto:representations@salisbury.sa.gov.au)

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<b>Location:</b>	158-160 Salisbury Highway, Salisbury SA 5108, 162 Salisbury Highway, Salisbury SA 5108

**YOUR DETAILS:** (this information must be provided to ensure that this is a valid representation)

NAME(S):..... Rocco + Kate Caruso.....  
ADDRESS:..... 155 SALISBURY HWY SALISBURY.....  
PHONE NO: [REDACTED] EMAIL: .....

I am: (please tick one of the following boxes as appropriate)

- The owner/occupier of the property located at: ..... AS ABOVE.....
- Other (please state): .....

**YOUR COMMENTS:**

I/We: (please tick the most appropriate box below)

- Support the proposed development.
- Oppose the proposed development.

**Whether you support or oppose this proposal you must provide written reasons below to ensure that this is a valid representation.**

As a resident of Salisbury for more than 25 years I have major concerns for not only my family but others due to the increase in traffic as I have

PTO

361/1720/2018/3B

witnessed + assisted in many  
 car accidents the most recent being  
 1 mth ago, by building the child care centre  
 not only will there be an increase in traffic  
 you will be running the risk of more accident  
 of which children could be hurt or God for bid  
 worse. There is also an existing child care  
 on Salisbury Hwy, that myself and my children  
 attended. It will also decrease the value of  
 my home greatly and disturb my husbands  
 sleep during the day as he works at night.  
 Most of my neighbors are also elderly and  
 would not appreciate the increase in noise.

**My concerns would be addressed by:** *(state changes/actions to the proposal sought)*

A more suitable location would be  
 recommended a way from this busy ~~home~~  
 highway as it is quite a large structure  
 and to be quite honest ~~to~~ an eye sore.  
~~then some more~~

PTO

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- Wish to be heard in support of my representation, and I will be:
  - Appearing personally,
  - OR**
  - Represented by the following person: .....
  - Contact details: .....

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Signature:  ..... Date: 21 / 10 / 18

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RECEIVED  
21 OCT 2018

THIS SHEET PROVIDES YOU WITH THE OPPORTUNITY TO MAKE COMMENTS IN RELATION TO A PROPOSED DEVELOPMENT. PLEASE FIND ATTACHED DETAILS OF THE PROPOSED DEVELOPMENT.

<b>Application Number:</b>	361/1720/2018/3B
<b>Applicant</b>	Ao Salisbury Pty Ltd
<b>Nature of Development:</b>	CHILD CARE CENTRE COMPRISING NEW BUILDING, SIGNAGE, BOUNDARY ACOUSTIC FENCING AND ASSOCIATED CAR PARKING
<b>Location:</b>	158-160 Salisbury Highway, Salisbury SA 5108, 162 Salisbury Highway, Salisbury SA 5108

**YOUR DETAILS:** (this information must be provided to ensure that this is a valid representation)

NAME(S): Sue Humpherson  
ADDRESS: 34B Evan Ave, Salisbury, 5108  
PHONE NO: [REDACTED] EMAIL:

I am: (please tick one of the following boxes as appropriate)

- The owner/occupier of the property located at: As above
- Other (please state):

**YOUR COMMENTS:**

I/We: (please tick the most appropriate box below)

- Support the proposed development.
- Oppose the proposed development.

**Whether you support or oppose this proposal you must provide written reasons below to ensure that this is a valid representation.**

Concerns re extra traffic & parking.  
There is already several unit blocks on this road where residents need to park on the street.  
U turns at the Salisbury Hwy & Evan Ave are

PTO

361/1720/2018/3B

Already concerning a I have witnessed quite a few near misses.

Not enough car parks on site to accomodate all staff & parents etc. Foot traffic on the highway eg: if crossing from one side to the other of Salisbury Hwy - will need a proper pedestrian crossing. Concerns for safety of children & Adults.

**My concerns would be addressed by:** *(state changes/actions to the proposal sought)*

PTO



Regulation 35(e) of the *Development Regulations 2008* requires that a representation must indicate a person's desire to be heard. Please note that if you do not indicate that you wish to be heard, it will be taken that you do not wish to be heard by the Panel.

I/We:

- Do not wish to be heard in support of my representation.
- Wish to be heard in support of my representation, and I will be:

Appearing personally,

**OR**

Represented by the following person: .....

Contact details: .....

*(Please note, matters raised in your written representation will be considered during the assessment and do not need to be repeated at the hearing).*

**Your written representation must be received by Council no later than 11.59pm on Wednesday 24<sup>th</sup> October 2018, to ensure that it is a valid representation and taken into account.**

**Representor's Declaration:**

I am aware that the representation will become a public document as prescribed in the *Freedom of Information Act 1991*, and will be made available to the applicant, agencies and other bodies pursuant to the *Development Act 1993*, and may be uploaded to the Council's website as an attachment to the hearing agenda.

Signature:  .....

Date: 23/10/18

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- Detail of reasons for making the representation.
- Indication whether or not the person (or persons) wishes to be heard.
- Submitted no later than 11.59pm on **Wednesday 24<sup>th</sup> October 2018.**



**STATEMENT OF REPRESENTATION**  
Pursuant to Section 38 of the *Development Act 1993*



**To:** City of Salisbury  
PO Box 8, SALISBURY SA 5108  
Email: [representations@salisbury.sa.gov.au](mailto:representations@salisbury.sa.gov.au)

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<b>Location:</b>	158-160 Salisbury Highway, Salisbury SA 5108, 162 Salisbury Highway, Salisbury SA 5108

**YOUR DETAILS:** (this information must be provided to ensure that this is a valid representation)

NAME(S): Danielle Luppino

ADDRESS: S Arthur Street Salisbury

PHONE NO: ..... EMAIL: .....

I am: (please tick one of the following boxes as appropriate)

- The owner/occupier of the property located at: .....
- Other (please state): .....

**YOUR COMMENTS:**

I/We: (please tick the most appropriate box below)

- Support the proposed development.
- Oppose the proposed development.

**Whether you support or oppose this proposal you must provide written reasons below to ensure that this is a valid representation.**

- increase in traffic on an already busy major arterial road
- pedestrian + child safety concerns
- increase in noise

PTO

361/1720/2018/3B

- concerned about traffic congestion in Arthur Street which will contribute to accessibility + clearway into + out of private residents drive ways.
- concerned with patrons of childcare centre parking on Arthur Street causing traffic hazards.

**My concerns would be addressed by:** *(state changes/actions to the proposal sought)*

Not building this centre in proposed location + moving it else where

PTO

Regulation 35(e) of the *Development Regulations 2008* requires that a representation must indicate a person's desire to be heard. Please note that if you do not indicate that you wish to be heard, it will be taken that you do not wish to be heard by the Panel.

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Date: 22/10/18

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<b>Location:</b>	158-160 Salisbury Highway, Salisbury SA 5108, 162 Salisbury Highway, Salisbury SA 5108

**YOUR DETAILS:** (this information must be provided to ensure that this is a valid representation)

NAME(S): CATERINA LUPPINO

ADDRESS: 9 ARTHUR ST. SALISBURY.

PHONE NO: ..... EMAIL: .....

I am: (please tick one of the following boxes as appropriate)

- The owner/occupier of the property located at: .....
- Other (please state): .....

**YOUR COMMENTS:**

I/We: (please tick the most appropriate box below)

- Support the proposed development.
- Oppose the proposed development.

**Whether you support or oppose this proposal you must provide written reasons below to ensure that this is a valid representation.**

BRING A LOT OF TRAFFIC TO THE AREA,  
PARKING DOWN SIDE STREETS.  
MAIN ROAD (SALISBURY H/WAY) ALREADY GETS  
CONGESTED, CHILD SAFETY. ~~is~~

PTO

361/1720/2018/3B

Too close to main inter sections therefore causing more traffic delays therefore increasing child safety. noise pollution.

**My concerns would be addressed by:** (state changes/actions to the proposal sought)

By not building on s/Highway. That there is surely other places that the child-care could be built on - preferably not on a main street.

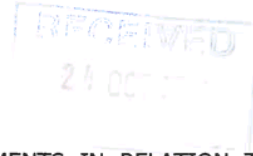
PTO





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<b>Location:</b>	158-160 Salisbury Highway, Salisbury SA 5108, 162 Salisbury Highway, Salisbury SA 5108

**YOUR DETAILS:** (this information must be provided to ensure that this is a valid representation)

NAME(S): Nikolaos Karandrikas

ADDRESS: 4 Arthur street Salisbury

PHONE NO: ..... EMAIL: [REDACTED]

I am: (please tick one of the following boxes as appropriate)

- The owner/occupier of the property located at: 4 Arthur street
- Other (please state): .....

**YOUR COMMENTS:**

I/We: (please tick the most appropriate box below)

- Support the proposed development.
- Oppose the proposed development.

**Whether you support or oppose this proposal you must provide written reasons below to ensure that this is a valid representation.**

I believe this will cause an increase in traffic on my street and find a childcare on the highway dangerous. I do not want more lights on the road such as pedestrian crossings.

PTO





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I/We:

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Signature:  .....

Date: 22 / 10 / 18

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**YOUR DETAILS:** (this information must be provided to ensure that this is a valid representation)

NAME(S): Giuseppe BIVONE  
(Joe)

ADDRESS: 40 Immanuel Dr. Greenwith S.A. 5125

PHONE NO: [REDACTED] EMAIL: [REDACTED]

I am: (please tick one of the following boxes as appropriate)

- The owner/occupier of the property located at: 164 Salisbury Highway  
Salisbury
- Other (please state): .....

**YOUR COMMENTS:**

I/We: (please tick the most appropriate box below)

- Support the proposed development.
- Oppose the proposed development.

**Whether you support or oppose this proposal you must provide written reasons below to ensure that this is a valid representation.**

• The Area is zoned Residential.  
• The proposal (with 114+ children + 20 staff) will bring traffic and parking problems next door to my property, on a very busing Salisbury Highway.

PTO

Item 5.1.1 - Attachment 2 - Category 3 Notice and Copy of Representations

361/1720/2018/3B

- There are already Child Care Centres in close proximity to this proposed large centre.
- It would not be fair to the nearby residents as this centre will create excessive noise and massive parking problems in Evan Ave and Arthur Ave due to limited parking on the Salisbury Highway and in the Centre
- Nearby residents will find their property values will drop
- We have already seen cars crashing through fences on the proposed site and adjoining properties
- It is <sup>on</sup> a most busy and congested Highway that is one of the district's most dangerous.
- I was only given 5 working days notice of this proposal

**My concerns would be addressed by:** (state changes/actions to the proposal sought)

building the C.C. Centre somewhere else where the safety issues, increased traffic and parking and noise problems will be minimised, and affect less existing residents.

PTO

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<b>Location:</b>	158-160 Salisbury Highway, Salisbury SA 5108, 162 Salisbury Highway, Salisbury SA 5108

**YOUR DETAILS:** (this information must be provided to ensure that this is a valid representation)

NAME(S): Ryan Tankler

ADDRESS: 3/154 Salisbury Highway, Salisbury SA 5108

PHONE NO: [REDACTED] EMAIL: [REDACTED]

I am: (please tick one of the following boxes as appropriate)

- The owner/occupier of the property located at: 3/154 Salisbury Highway
- Other (please state): .....

**YOUR COMMENTS:**

I/We: (please tick the most appropriate box below)

- Support the proposed development.
- Oppose the proposed development.

**Whether you support or oppose this proposal you must provide written reasons below to ensure that this is a valid representation.**

I oppose this proposal for a child care centre to be located at 158-162 Salisbury Highway, Salisbury SA 5108.

The reason I oppose this development is because of the following.

PTO

361/1720/2018/3B

- Too much traffic around the centre, making it hard to park near my property, also making it unsafe for children.

- Excessive noise from the children and their parents dropping off / picking up from the centre. I'm a shift worker, so this will make it hard for me to sleep.

Overall the development of a child care centre across the road from my property will affect my quality of life in a negative way.

**My concerns would be addressed by:** *(state changes/actions to the proposal sought)*

Not building it at the location specified.

PTO

Item 5.1.1 - Attachment 2 - Category 3 Notice and Copy of Representations

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I/We:

Do not wish to be heard in support of my representation.

Wish to be heard in support of my representation, and I will be:

Appearing personally,

OR

Represented by the following person: .....

Contact details: ~~0401024261~~ .....

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Signature: ..... *R W* .....

Date: *21 / 10 / 18*

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<b>Location:</b>	158-160 Salisbury Highway, Salisbury SA 5108, 162 Salisbury Highway, Salisbury SA 5108

**YOUR DETAILS:** (this information must be provided to ensure that this is a valid representation)

NAME(S): Sarah Kernot

ADDRESS: 34A Evan Ave Salisbury

PHONE NO: ..... EMAIL: [REDACTED]

I am: (please tick one of the following boxes as appropriate)

- The owner/occupier of the property located at: 34A Evan Ave
- Other (please state): .....

**YOUR COMMENTS:**

I/We: (please tick the most appropriate box below)

- Support the proposed development.
- Oppose the proposed development.

**Whether you support or oppose this proposal you must provide written reasons below to ensure that this is a valid representation.**

I am concerned for the proposed space not containing enough carparks and therefore parents would be forced to park on Evan Ave this would impose on those residing on the street

PTO

PTO

361/1720/2018/3B

and also pose a risk to children entering/exiting cars on the street and accessing the proposed site.

It is also of concern the size of the Centre and the look of the site from Evan Ave would not be appealing.

As a shift worker I also have issues with the increase noise level. Many residents on the street work shifts or are retired and this would cause a large impact.

I was not made aware of this proposed development by council!

**My concerns would be addressed by:** *(state changes/actions to the proposal sought)*

PTO

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Signature: 

Date: 22/10/18

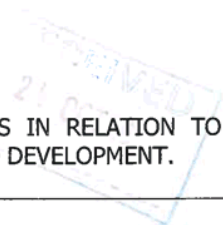
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**YOUR DETAILS:** (this information must be provided to ensure that this is a valid representation)

NAME(S): HONG PHUC VUONG

ADDRESS: UNIT 2 / 154 SALISBURY HIGHWAY

PHONE NO: [REDACTED] EMAIL: .....

I am: (please tick one of the following boxes as appropriate)

- The owner/occupier of the property located at: .....
- Other (please state): .....

**YOUR COMMENTS:**

I/We: (please tick the most appropriate box below)

- Support the proposed development.
- Oppose the proposed development.

**Whether you support or oppose this proposal you must provide written reasons below to ensure that this is a valid representation.**

I don't like the new child care to be built, because it will affect my quite with a lot of noise coming across from the building and busy traffic around the building

PTO

361/1720/2018/3B

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**My concerns would be addressed by:** *(state changes/actions to the proposal sought)*

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PTO

Item 5.1.1 - Attachment 2 - Category 3 Notice and Copy of Representations

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Signature:  ..... Date: 21 / 10 / 2018

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***ATTACHMENT 3***  
***COMMISSIONER OF HIGHWAYS***  
***REFERRAL RESPONSE***



Government of South Australia

Department of Planning,  
Transport and Infrastructure

In reply please quote 2018/01920, Process ID: 542237  
Enquiries to Annie Liu  
Telephone (08) 8343 2586  
E-mail dpti.luc@sa.gov.au

DEVELOPMENT DIVISION  
Transport Assessment  
and Policy Reform

GPO Box 1533  
Adelaide SA 5001

ABN 92 366 288 135

5 November 2018

Mr Aaron Curtis  
City of Salisbury  
PO Box 8  
SALISBURY SA 5108

Dear Mr Curtis,

#### SCHEDULE 8 - REFERRAL RESPONSE

<b>Development No.</b>	361/1720/18
<b>Applicant</b>	AO Salisbury Pty Ltd
<b>Location</b>	158-160 and 162 Salisbury Highway, Salisbury
<b>Proposal</b>	Child care centre (114 children) comprising new building, storage shed, signage, boundary acoustic fencing and associated car parking, manoeuvring areas, bin storage and landscaping

I refer to the above development application forwarded to the Commissioner of Highways (CoH) in accordance with Section 37 of the *Development Act 1993*. The proposed development involves development adjacent a main road as described above.

The following response is provided in accordance with Section 37(4)(b) of the *Development Act 1993* and Schedule 8 of the *Development Regulations 2008*.

#### CONSIDERATION

The subject site abuts Salisbury Highway, an arterial road under the care, control and management of CoH, as well as Evan Avenue, a local road under the care, control and management of the City of Salisbury. The adjacent section of Salisbury Highway is designated as a Peak Hour Route under the Department of Planning, Transport and Infrastructure's (DPTI's) '*A Functional Hierarchy for South Australia's Land Transport Network*'. At this location, Salisbury Highway carries approximately 37,900 vehicles per day (5% commercial vehicles) and has a posted speed limit of 60 km/h. Evan Avenue is subject to the urban default speed limit of 50km/h.

The application proposes to widen the existing access point on Salisbury Highway to 6 metres. This widened access should be suitably flared from the property boundary to the kerblineline to facilitate simultaneous two way vehicular movements. The existing crossover on Evan Avenue will be closed and a new 6 metres wide access will be provided approximately 19 metres from Salisbury Highway. DPTI does not object in-principle to the proposed access arrangements.

#### Carpark

Council should ensure that the proposed development provides sufficient on-site car parking, designed in accordance with *AS/NZS 2890.1:2004* and *AS/NZS 2890.6:2009*. Additionally, clear sightlines, as shown in Figure 3.3 'Minimum Sight Lines for Pedestrian Safety' in

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AS/NZS 2890.1:2004, should be provided at the property line to ensure adequate visibility between vehicles leaving the site and pedestrians on the adjacent footpath.

**Signage**

Three non-illuminated signs are proposed. As the proposed signs are non-illuminated and are located within the site, DPTI has no in-principle objections.

**Refuse Collection**

The Transport Impact Assessment report produced by GTA Consultants, Reference S159600, dated 18 September 2018, shows that a 10 metres rigid truck can enter and exit the site in a forward direction for refuse collection and that to minimise conflicts that waste collection will occur outside of business operating hours when the carpark will be empty.

**ADVICE**

DPTI does not object in-principle to the proposed development. The planning authority is advised to attach the following conditions to any approval:

1. All vehicular access points shall be in general accordance with Proposed Site Plan produced by Flightpath Architects Pty Ltd, Drawing No. 3522.SK02, Revision A, dated 26 September 2018. The access points shall be suitably flared from the property boundary to the kerbline to facilitate simultaneous two-way vehicle movements.
2. All vehicles shall enter and exit the site in a forward direction.
3. All off-street car parking areas shall be designed in accordance with AS/NZS 2890.1:2004 and AS/NZS 2890.6:2009. Additionally, clear sightlines, as shown in Figure 3.3 'Minimum Sight Lines for Pedestrian Safety' in AS/NZS 2890.1:2004, shall be provided at the property line to ensure adequate visibility between vehicles leaving the site and pedestrians on the adjacent footpath.
4. The largest vehicle permitted on-site shall be a 10 metres rigid truck, which shall only be permitted to access the site outside of business operating hours.
5. Stormwater run-off shall be collected on-site and discharged without jeopardising the integrity and safety of the adjacent roads. Any alterations to the road drainage infrastructure required to facilitate this shall be at the applicant's cost.

Yours sincerely,



*for* **MANAGER, TRANSPORT ASSESSMENT AND POLICY REFORM**  
for **COMMISSIONER OF HIGHWAYS**

A copy of the decision notification form should be forwarded to [dpti.developmentapplications@sa.gov.au](mailto:dpti.developmentapplications@sa.gov.au)

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***ATTACHMENT 4  
INTERNAL REFERRAL  
RESPONSES***

*The following internal referral responses were received:*

*Development Engineering*

*The stormwater management report (20/09/2018) prepared by PT Design satisfies Council's requirements. The Civil and Stormwater Plan can be submitted for approval as a Reserved Matter.*

*Standard conditions are recommended.*

*A non-standard condition is recommended as follows:*

*Construction of new stormwater infrastructure (Junction box on Evan Avenue) to be handed to Council is to be completed to the reasonable satisfaction of Council.*

*Landscape Design*

*What is proposed appears acceptable, other than the turf installed in the small areas within the car parking and adjacent the building.*

*The use of Syzygium austral is not recommended adjacent to Salisbury Highway and the Evan Avenue fencelines. The plant is larger than the proposed space at maturity and thus has potential for encroachment onto the footpath in Salisbury Highway. The species should be substituted for something that has a smaller mature plant size, such as hybrids of Syzygium austral 'Tiny Trev' or 'Winter Lights'.*

*The location of CA – Cupaniopsis should be considered in relation to the space in which it is proposed, as in a number of locations (around the car parking). The garden area depth that they are located in is less than 1m which will have a limiting capacity in the tree's mature size.*

*I will make comment that the boundary hedge planting needs to be maintained low so that pedestrians and footpath users are not concealed (particularly in the immediate area adjacent driveways), especially on Salisbury Highway with the opportunity now for cyclists to be on the footpath and traffic sight lines when entering Salisbury Highway.*

***ATTACHMENT 5***  
***RELEVANT DEVELOPMENT PLAN***  
***PROVISIONS***



## Salisbury Council

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Please refer to the Salisbury Council page at [www.sa.gov.au/developmentplans](http://www.sa.gov.au/developmentplans) to see any amendments not consolidated.



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Department of Planning,  
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## Advertisements

### OBJECTIVES

- 1 Urban and rural landscapes that are not disfigured by advertisements and/or advertising hoardings.
- 2 Advertisements and/or advertising hoardings that do not create a hazard.
- 3 Advertisements and/or advertising hoardings designed to enhance the appearance of the building and locality.

### PRINCIPLES OF DEVELOPMENT CONTROL

- 1 The location, siting, design, materials, size, and shape of advertisements and/or advertising hoardings should be:
  - (a) consistent with the predominant character of the urban or rural landscape
  - (b) in harmony with any buildings or sites of historic significance or heritage value in the area
  - (c) co-ordinated with and complement the architectural form and design of the building they are to be located on.
- 2 The number of advertisements and/or advertising hoardings associated with a development should be minimised to avoid:
  - (a) clutter
  - (b) disorder
  - (c) untidiness of buildings and their surrounds
  - (d) driver distraction.
- 3 Buildings occupied by a number of tenants should exhibit co-ordinated and complementary advertisements and/or advertising hoardings to identify the tenants and their type of business.
- 4 The content of advertisements should be limited to information relating to the legitimate use of the associated land.
- 5 Advertisements and/or advertising hoardings should:
  - (a) be completely contained within the boundaries of the subject allotment
  - (b) be sited to avoid damage to, or pruning or lopping of, on-site landscaping or street trees
  - (c) not obscure views to vistas or objects of high amenity value.
- 6 Advertisements and/or advertising hoardings should not be erected on:
  - (a) a public footpath or veranda post
  - (b) a road, median strip or traffic island
  - (c) a vehicle adapted and exhibited primarily as an advertisement

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- (d) residential land.
- 7 Advertisements and/or advertising hoardings attached to buildings should not be sited on the roof or higher than the walls of a building.
- 8 Advertisements and/or advertising hoardings erected on a veranda or that project from a building wall should:
- have a clearance over a footway, of at least 2.5 metres, to allow for safe and convenient pedestrian access
  - where erected on the side of a veranda, not exceed the width of the veranda or project from the veranda
  - where erected on the front of a veranda, not exceed the length of the veranda or project from the veranda
  - where projecting from a wall, have the edge of the advertisement or advertising hoarding abutting the surface of the wall.
- 9 Advertisements should be designed to conceal their supporting advertising hoarding from view.
- 10 Advertisements should convey the owner/occupier and/or generic type of business, merchandise or services using simple, clear and concise language, symbols, print style and layout and a small number of colours.
- 11 Advertisements which perform a secondary role in identifying the business, goods or services should only be readable in the immediate vicinity of the site.
- 12 Outside of townships and country settlements advertisements other than traffic signs, tourist signs or advertisements on an existing tourist information bay display board, should not be erected in road reserves.

#### Safety

- 13 Advertisements and/or advertising hoardings should not create a hazard by:
- being so highly illuminated as to cause discomfort to an approaching driver, or to create difficulty in the driver's perception of the road or persons or objects on the road
  - being liable to interpretation by drivers as an official traffic sign, or convey to drivers information that might be confused with instructions given by traffic signals or other control devices, or impair the conspicuous nature of traffic signs or signals
  - distracting drivers from the primary driving task at a location especially where the demands on driver concentration are high
  - obscuring a driver's view of other road or rail vehicles at/or approaching level crossings, or of pedestrians or of features of the road that are potentially hazardous (eg junctions, bends, changes in width, traffic control devices).
- 14 Any internally illuminated advertising signs and/or advertising hoardings which utilise LED, LCD or other similar technologies should be located a minimum of 80 metres from traffic signals, level crossings and other important traffic control devices.

#### Freestanding Advertisements

- 15 Freestanding advertisements and/or advertising hoardings should be:
- limited to only one primary advertisement per site or complex

(b) of a scale and size in keeping with the desired character of the locality and compatible with the development on the site.

- 16 Freestanding advertisements and/or advertising hoardings for multiple-business tenancy complexes should:
- incorporate the name or nature of each business or activity within the site or complex in a single advertisement
  - be integrally designed and mounted below the more predominant main complex or site identity advertisement.
- 17 Portable, easel or A-frame advertisements should be displayed only where:
- no other appropriate opportunity exists for an adequate co-ordinated and permanently erected advertisement and/or advertising hoarding
  - they do not obstruct or compromise the safety of pedestrians or vehicle movement
  - there is no unnecessary duplication or proliferation of advertising information
  - there is no damage to, or removal of, any landscaping on the site
  - they are restricted to 1 per site, or 1 per major road frontage if located upon a large corner site
  - each sign does not exceed 1 square metre in advertisement area per face, and 1.2 metres in height.

18 Freestanding advertisements should not exceed the total height (measured from natural ground level) as specified within the following table:

Location of freestanding advertisement	Total height (in metres)
Mixed Use (Bulky Goods, Entertainment and Leisure) Zone Precinct 8 Retail Core (within the Ingle Farm Policy Area 2) Core area within the Urban Core (Salisbury) Zone Precinct 17 Retail Core (within the Salisbury Downs Policy Area 4) Bulky Goods Zone Precinct 23 Greenfields Commercial (within the Commercial Zone)	8
Industry Zone Neighbourhood Centre Zone Precinct 21 Para Hills West Commercial (within the Commercial Zone) Precinct 24 Pooraka Commercial (within the Commercial Zone)	6
Commercial Zone (Except within Precinct 23 Greenfields Commercial, Precinct 21 Para Hills West Commercial or Precinct 24 Pooraka Commercial) Precinct 5 Education (within the Ingle Farm Policy Area 2) Precinct 7 Recreation (within the Ingle Farm Policy Area 2) Local Centre Zone Precinct 15 Community (within the Salisbury Downs Policy Area 4) Precinct 16 Mixed Use (within the Salisbury Downs Policy Area 4) Transition area within the Urban Core (Salisbury) Zone	4
In all other locations	3

- 19 Portable, easel or A-frame advertisements associated with a development should be displayed only during the hours the development is open for trading.



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**Flags, Bunting and Streamers**

- 20 Advertisements and/or advertising hoardings incorporating any flags, bunting, streamers, or suspended objects should:
- (a) be placed or arranged to complement and accord with the scale of the associated development
  - (b) other than flags, not be positioned higher than the building they are attached or related to
  - (c) not be displayed in residential areas.

**Advertising along Arterial Roads**

- 21 Advertising and/or advertising hoardings should not be placed along arterial roads that have a speed limit of 80 km/h or more.

## Crime Prevention

### OBJECTIVES

- 1 A safe, secure, crime resistant environment where land uses are integrated and designed to facilitate community surveillance.

### PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development should be designed to maximise surveillance of public spaces through the incorporation of clear lines of sight, appropriate lighting and the use of visible permeable barriers wherever practicable.
- 2 Buildings should be designed to overlook public and communal streets and public open space to allow casual surveillance.
- 3 Development should provide a robust environment that is resistant to vandalism and graffiti.
- 4 Development should provide lighting in frequently used public spaces including those:
  - (a) along dedicated cyclist and pedestrian pathways, laneways and access routes
  - (b) around public facilities such as toilets, telephones, bus stops, seating, litter bins, automatic teller machines, taxi ranks and car parks.
- 5 Development, including car park facilities should incorporate signage and lighting that indicate the entrances and pathways to, from and within sites.
- 6 Landscaping should be used to assist in discouraging crime by:
  - (a) screen planting areas susceptible to vandalism
  - (b) planting trees or ground covers, rather than shrubs, alongside footpaths
  - (c) planting vegetation other than ground covers a minimum distance of two metres from footpaths to reduce concealment opportunities.
- 7 Site planning, buildings, fences, landscaping and other features should clearly differentiate public, communal and private areas.
- 8 Buildings should be designed to minimise and discourage access between roofs, balconies and windows of adjoining dwellings.
- 9 Public toilets should be located, sited and designed:
  - (a) to promote the visibility of people entering and exiting the facility (eg by avoiding recessed entrances and dense shrubbery that obstructs passive surveillance)
  - (b) near public and community transport links and pedestrian and cyclist networks to maximise visibility.
- 10 Development should avoid pedestrian entrapment spots and movement predictors (eg routes or paths that are predictable or unchangeable and offer no choice to pedestrians).

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*Crime Prevention*

- 11 Development should be designed to maximise surveillance of open space, pedestrian routes, centres and residential areas by:
- (a) orienting the frontages and entrances of buildings towards the public street
  - (b) avoiding screens, high walls, carports and landscaping that obscure direct views to public areas
  - (c) placing the entrances of buildings opposite each other across a street, or group entrances of multiple dwelling developments onto a commonly visible area to provide maximum mutual surveillance
  - (d) arranging living areas, windows, access ways and balconies to overlook open space and recreation areas and provide observation points to all areas of a site, particularly entrances and car parks.

## Design and Appearance

### OBJECTIVES

- 1 Development of a high architectural standard that responds to and reinforces positive aspects of the local environment and built form.
- 2 Roads, open spaces, buildings and land uses laid out and linked so that they are easy to understand and navigate.

### PRINCIPLES OF DEVELOPMENT CONTROL

- 1 The design of a building may be of a contemporary nature and exhibit an innovative style provided the overall form is sympathetic to the scale of development in the locality and with the context of its setting with regard to shape, size, materials and colour.
- 2 Buildings should be designed and sited to avoid creating extensive areas of uninterrupted walling facing areas exposed to public view.
- 3 Buildings should be designed to reduce their visual bulk and provide visual interest through design elements such as:
  - (a) articulation
  - (b) colour and detailing
  - (c) small vertical and horizontal components
  - (d) design and placing of windows
  - (e) variations to facades.
- 4 Where a building is sited on or close to a side boundary, the side boundary wall should be sited and limited in length and height to minimise:
  - (a) the visual impact of the building as viewed from adjoining properties
  - (b) overshadowing of adjoining properties and allow adequate sun light to neighbouring buildings.
- 5 Building form should not unreasonably restrict existing views available from neighbouring properties and public spaces.
- 6 Transportable buildings and buildings which are elevated on stumps, posts, piers, columns or the like, should have their suspended footings enclosed around the perimeter of the building with brickwork or timber, and the use of verandas, pergolas and other suitable architectural detailing to give the appearance of a permanent structure.
- 7 The external walls and roofs of buildings should not incorporate highly reflective materials which will result in glare to neighbouring properties or drivers.
- 8 Structures located on the roofs of buildings to house plant and equipment should form an integral part of the building design in relation to external finishes, shaping and colours.
- 9 Building design should emphasise pedestrian entry points to provide perceptible and direct access from public street frontages and vehicle parking areas.

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- 10 Development should provide clearly recognisable links to adjoining areas and facilities.
- 11 Buildings, landscaping, paving and signage should have a co-ordinated appearance that maintains and enhances the visual attractiveness of the locality.
- 12 Buildings (other than ancillary buildings or group dwellings) should be designed so that their main façade faces the primary street frontage of the land on which they are situated.
- 13 Where applicable, development should incorporate verandas over footpaths to enhance the quality of the pedestrian environment.
- 14 Development should be designed and sited so that outdoor storage, loading and service areas are screened from public view by an appropriate combination of built form, solid fencing and/or landscaping.
- 15 Outdoor lighting should not result in light spillage on adjacent land.
- 16 Balconies should:
  - (a) be integrated with the overall architectural form and detail of the building
  - (b) be sited to face predominantly north, east or west to provide solar access
  - (c) have a minimum area of 2 square metres.

#### **Development Adjacent Heritage Places**

- 17 The design of multi-storey buildings should not detract from the form and materials of adjacent State and local heritage places listed in [Table Sal/4 - State Heritage Places](#).
- 18 Development on land adjacent to a State or local heritage place, as listed in [Table Sal/4 - State Heritage Places](#) should be sited and designed to reinforce the historic character of the place and maintain its visual prominence.

#### **Overshadowing**

- 19 The design and location of buildings should enable direct winter sunlight into adjacent dwellings and private open space and minimise the overshadowing of:
  - (a) windows of habitable rooms
  - (b) upper-level private balconies that provide the primary open space area for a dwelling
  - (c) solar collectors (such as solar hot water systems and photovoltaic cells).

#### **Visual Privacy**

- 20 Development should minimise direct overlooking of habitable rooms and private open spaces of dwellings through measures such as:
  - (a) off-setting the location of balconies and windows of habitable rooms with those of other buildings so that views are oblique rather than direct
  - (b) building setbacks from boundaries (including building boundary to boundary where appropriate) that interrupt views or that provide a spatial separation between balconies or windows of habitable rooms
  - (c) screening devices (including fencing, obscure glazing, screens, external ventilation blinds, window hoods and shutters) that are integrated into the building design and have minimal negative effect on residents' or neighbours' amenity.
- 21 Permanently fixed external screening devices should be designed and coloured to complement the associated building's external materials and finishes

**Building Setbacks from Road Boundaries**

- 22 The setback of buildings from public roads should:
- (a) be similar to, or compatible with, setbacks of buildings on adjoining land and other buildings in the locality
  - (b) contribute positively to the streetscape character of the locality
  - (c) not result in or contribute to a detrimental impact upon the function, appearance or character of the locality.

23 Except where specified in a particular zone, policy area or precinct, the main face of a building should be set back from the primary road frontage in accordance with the following table:

Setback difference between buildings on adjacent allotments	Setback of new building
Up to 2 metres	The same setback as one of the adjacent buildings, as illustrated below:
<p style="text-align: center;">When <math>b - a \leq 2</math>, setback of new dwelling = a or b</p>	
Greater than 2 metres	At least the average setback of the adjacent buildings.

24 Except where specified in a particular zone, policy area, or precinct, buildings and structures should be set back from road boundaries having regard to the requirements set out in Table Sald1 - Building Setbacks from Road Boundaries.

25 Except where specified in a zone, policy area or precinct, the setback of development from a secondary street frontage should reflect the setbacks of the adjoining buildings and other buildings in the locality.

26 Development likely to encroach within a road widening setback under the *Metropolitan Adelaide Road Widening Plan Act 1972* should be set back sufficiently from the boundary required for road widening.

Salisbury Council  
General Section  
Energy Efficiency

## Energy Efficiency

### OBJECTIVES

- 1 Development designed and sited to conserve energy.
- 2 Development that provides for on-site power generation including photovoltaic cells and wind power.

### PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development should provide for efficient solar access to buildings and open space all year around.
- 2 Buildings should be sited and designed:
  - (a) to ensure adequate natural light and winter sunlight is available to the main activity areas of adjacent buildings
  - (b) so that open spaces associated with the main activity areas face north for exposure to winter sun.

### On-site Energy Generation

- 3 Development should facilitate the efficient use of photovoltaic cells and solar hot water systems by:
  - (a) taking into account overshadowing from neighbouring buildings
  - (b) designing roof orientation and pitches to maximise exposure to direct sunlight.
- 4 Public infrastructure and lighting, should be designed to generate and use renewable energy.

## Hazards

### OBJECTIVES

- 1 Maintenance of the natural environment and systems by limiting development in areas susceptible to natural hazard risk.
- 2 Development located away from areas that are vulnerable to, and cannot be adequately and effectively protected from the risk of natural hazards.
- 3 Critical community facilities such as hospitals, emergency control centres, major service infrastructure facilities, and emergency service facilities located where they are not exposed to natural hazard risks.
- 4 Development located and designed to minimise the risks to safety and property from flooding.
- 5 Development located to minimise the threat and impact of bushfires on life and property.
- 6 Expansion of existing non-rural uses directed away from areas of high bushfire risk.
- 7 The environmental values and ecological health of receiving waterways and marine environments protected from the release of acid water resulting from the disturbance of acid sulphate soils.
- 8 Protection of human health and the environment wherever site contamination has been identified or is suspected to have occurred.
- 9 Appropriate assessment and remediation of site contamination to ensure land is suitable for the proposed use and provides a safe and healthy living and working environment.
- 10 Minimisation of harm to life, property and the environment through appropriate location of development and appropriate storage, containment and handling of hazardous materials.

### PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development should be excluded from areas that are vulnerable to, and cannot be adequately and effectively protected from, the risk of hazards.
- 2 Development located on land subject to hazards as shown on the *Overlay Maps - Development Constraints* should not occur unless it is sited, designed and undertaken with appropriate precautions being taken against the relevant hazards.
- 3 There should not be any significant interference with natural processes in order to reduce the exposure of development to the risk of natural hazards.

### Flooding

- 4 Development should not occur on land where the risk of flooding is likely to be harmful to safety or damage property.
- 5 Development should not be undertaken in areas liable to inundation by tidal, drainage or flood waters unless the development can achieve all of the following:
  - (a) it is developed with a public stormwater system capable of catering for a 1-in-100 year average return interval flood event
  - (b) buildings are designed and constructed to prevent the entry of floodwaters in a 1-in-100 year average return interval flood event.



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Hazards

- 6 Development, including earthworks associated with development, should not do any of the following:
- (a) impede the flow of floodwaters through the land or other surrounding land
  - (b) increase the potential hazard risk to public safety of persons during a flood event
  - (c) aggravate the potential for erosion or siltation or lead to the destruction of vegetation during a flood
  - (d) cause any adverse effect on the floodway function
  - (e) increase the risk of flooding of other land
  - (f) obstruct a watercourse.

**Bushfire**

- 7 The following bushfire protection principles of development control apply to development of land identified as General, Medium and High bushfire risk areas as shown on the *Bushfire Protection Area BPA Maps - Bushfire Risk*.
- 8 Development in a Bushfire Protection Area should be in accordance with those provisions of the *Minister's Code: Undertaking development in Bushfire Protection Areas* that are designated as mandatory for Development Plan Consent purposes.
- 9 Buildings and structures should be located away from areas that pose an unacceptable bushfire risk as a result of one or more of the following:
- (a) vegetation cover comprising trees and/or shrubs
  - (b) poor access
  - (c) rugged terrain
  - (d) inability to provide an adequate building protection zone
  - (e) inability to provide an adequate supply of water for fire-fighting purposes.
- 10 Residential, tourist accommodation and other habitable buildings should:
- (a) be sited on the flatter portion of allotments and avoid steep slopes, especially upper slopes, narrow ridge crests and the tops of narrow gullies, and slopes with a northerly or westerly aspect
  - (b) be sited in areas with low bushfire hazard vegetation and set back at least 20 metres from existing hazardous vegetation
  - (c) have a dedicated and accessible water supply available at all times for fire fighting.
- 11 Extensions to existing buildings, outbuildings and other ancillary structures should be sited and constructed using materials to minimise the threat of fire spread to residential, tourist accommodation and other habitable buildings in the event of bushfire.
- 12 Buildings and structures should be designed and configured to reduce the impact of bushfire through using simple designs that reduce the potential for trapping burning debris against the building or structure, or between the ground and building floor level in the case of transportable buildings.
- 13 Land division for residential or tourist accommodation purposes within areas of high bushfire risk should be limited to those areas specifically set aside for these uses.

- 14 Where land division does occur it should be designed to:
- minimise the danger to residents, other occupants of buildings and fire fighting personnel
  - minimise the extent of damage to buildings and other property during a bushfire
  - ensure each allotment contains a suitable building site that is located away from vegetation that would pose an unacceptable risk in the event of bushfire
  - ensure provision of a fire hazard separation zone isolating residential allotments from areas that pose an unacceptable bushfire risk by containing the allotments within a perimeter road or through other means that achieve an adequate separation.
- 15 Vehicle access and driveways to properties and public roads created by land division should be designed and constructed to:
- facilitate safe and effective operational use for fire-fighting and other emergency vehicles and residents
  - provide for two-way vehicular access between areas of fire risk and the nearest public road.
- 16 Olive orchards should be located and developed in a manner that minimises their potential to fuel bushfires.

#### Salinity

- 17 Development should not increase the potential for, or result in an increase in, soil and water salinity.
- 18 Preservation, maintenance and restoration of locally indigenous plant species should be encouraged in areas affected by dry land salinity.
- 19 Irrigated horticulture and pasture should not increase groundwater-induced salinity.

#### Acid Sulfate Soils

- 20 Development and activities, including excavation and filling of land, that may lead to disturbance of potential or actual acid sulfate soils (including land identified on the *Overlay Maps – Development Constraints*) should be avoided unless such disturbances are managed in a way that effectively avoids the potential for harm or damage to any of the following:
- the marine and estuarine environment
  - natural water bodies and wetlands
  - agricultural or aquaculture activities
  - buildings, structures and infrastructure
  - public health.
- 21 Development, including primary production, aquaculture activities and infrastructure, should not proceed unless it can be demonstrated that the risk of releasing acid water resulting from the disturbance of acid sulfate soils is minimal.

#### Site Contamination

- 22 Development, including land division, should not occur where site contamination has occurred unless the site has been assessed and remediated as necessary to ensure that it is suitable and safe for the proposed use.

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General Section  
Hazards

**Containment of Chemical and Hazardous Materials**

- 23 Hazardous materials should be stored and contained in a manner that minimises the risk to public health and safety and the potential for water, land or air contamination.
- 24 Development that involves the storage and handling of hazardous materials should ensure that these are contained in designated areas that are secure, readily accessible to emergency vehicles, impervious, protected from rain and stormwater intrusion and other measures necessary to prevent:
  - (a) discharge of polluted water from the site
  - (b) contamination of land
  - (c) airborne migration of pollutants
  - (d) potential interface impacts with sensitive land uses.

**Landslip**

- 25 Development, including associated cut and fill activities, should not lead to an increased danger from land surface instability or to the potential of landslip occurring on the site or on surrounding land.
- 26 Development on steep slopes should promote the retention and replanting of vegetation as a means of stabilising and reducing the possibility of surface movement or disturbance.
- 27 Development in areas susceptible to landslip should:
  - (a) incorporate split level designs to minimise cutting into the slope
  - (b) ensure that cut and fill and heights of faces are minimised
  - (c) ensure cut and fill is supported with engineered retaining walls or are battered to appropriate grades
  - (d) control any erosion that will increase the gradient of the slope and decrease stability
  - (e) ensure the siting and operation of an effluent drainage field does not contribute to landslip
  - (f) provide drainage measures to ensure surface stability is not compromised
  - (g) ensure natural drainage lines are not obstructed.

## Infrastructure

### OBJECTIVES

- 1 Infrastructure provided in an economical and environmentally sensitive manner.
- 2 Infrastructure, including social infrastructure, provided in advance of need.
- 3 Suitable land for infrastructure identified and set aside in advance of need.
- 4 The visual impact of infrastructure facilities minimised.
- 5 The efficient and cost-effective use of existing infrastructure.

### PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development should not occur without the provision of adequate utilities and services, including:
  - (a) electricity supply
  - (b) water supply
  - (c) drainage and stormwater systems
  - (d) waste disposal
  - (e) effluent disposal systems
  - (f) formed all-weather public roads
  - (g) telecommunications services
  - (h) social infrastructure, community services and facilities
  - (i) gas services.
- 2 Development should only occur where it provides, or has access to, relevant easements for the supply of infrastructure.
- 3 Development should incorporate provision for the supply of infrastructure services to be located within common service trenches where practicable.
- 4 Development should not take place until adequate and co-ordinated drainage of the land is assured.
- 5 Development in urban areas should not occur without provision of an adequate reticulated domestic quality mains water supply and an appropriate waste treatment system.
- 6 In areas where no reticulated water supply is available, buildings whose usage is reliant on a water supply should be equipped with an adequate and reliable on-site water storage system.
- 7 Electricity infrastructure should be designed and located to minimise its visual and environmental impacts.
- 8 Development and landscaping within 25 metres of the 275 kV overhead electricity lines should ensure that all clearances and safety restrictions are met.

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*Infrastructure*

- 9 In urban areas, electricity supply serving new development should be installed underground.
- 10 Utilities and services, including access roads and tracks, should be sited on areas already cleared of native vegetation. If this is not possible, their siting should cause minimal interference or disturbance to existing native vegetation and biodiversity.
- 11 Utility buildings and structures should be grouped with non-residential development where possible.
- 12 Development in proximity to infrastructure facilities should be sited and be of a scale to ensure adequate separation to protect people and property.

## Interface between Land Uses

### OBJECTIVES

- 1 Development located and designed to minimise adverse impact and conflict between land uses.
- 2 Protect community health and amenity from adverse impacts of development.
- 3 Protect desired land uses from the encroachment of incompatible development.

### PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development should not detrimentally affect the amenity of the locality or cause unreasonable interference through any of the following:
  - (a) the emission of effluent, odour, smoke, fumes, dust or other airborne pollutants
  - (b) noise
  - (c) vibration
  - (d) electrical interference
  - (e) light spill
  - (f) glare
  - (g) hours of operation
  - (h) traffic impacts.
- 2 Development should be sited and designed to minimise negative impacts on existing and potential future land uses desired in the locality.
- 3 Development adjacent to a Residential Zone should be designed to minimise overlooking and overshadowing of adjacent dwellings and private open space.
- 4 Residential development adjacent to non-residential zones and land uses should be located, designed and/or sited to protect residents from potential adverse impacts from non-residential activities.
- 5 Sensitive uses likely to conflict with the continuation of lawfully existing developments and land uses desired for the zone should be designed to minimise negative impacts.
- 6 Non-residential development on land abutting a residential zone should be designed to minimise noise impacts to achieve adequate levels of compatibility between existing and proposed uses.

### Noise Generating Activities

- 7 Development that emits noise (other than music noise) should include noise attenuation measures that achieve the relevant *Environment Protection (Noise) Policy* criteria when assessed at the nearest existing noise sensitive premises.
- 8 Development with the potential to emit significant noise (e.g. industry) should incorporate noise attenuation measures that prevent noise from causing unreasonable interference with the amenity of noise sensitive premises.

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*Interface between Land Uses*

- 9 Outdoor areas (such as beer gardens or dining areas) associated with licensed premises should be designed or sited to minimise adverse noise impacts on adjacent existing or future noise sensitive development.
- 10 Development proposing music should include noise attenuation measures that achieve the following desired noise levels:

Noise level assessment location	Desired noise level
Adjacent existing <i>noise sensitive development</i> property boundary	Less than 8 dB above the level of background noise ( $L_{90,15min}$ ) in any octave band of the sound spectrum and Less than 5 dB(A) above the level of background noise ( $LA_{90,15min}$ ) for the overall (sum of all octave bands) A-weighted level
Adjacent <i>land</i> property boundary	Less than 65dB(Lin) at 63Hz and 70dB(Lin) in all other octave bands of the sound spectrum or Less than 8 dB above the level of background noise ( $L_{90,15min}$ ) in any octave band of the sound spectrum and 5 dB(A) overall (sum of all octave bands) A-weighted level

#### Air Quality

- 11 Development with the potential to emit harmful or nuisance-generating air pollution should incorporate air pollution control measures to prevent harm to human health or unreasonable interference with the amenity of sensitive uses within the locality.
- 12 Chimneys or exhaust flues associated with commercial development (including cafes, restaurants and fast food outlets) should be designed to ensure they do not cause a nuisance or health concerns to nearby sensitive receivers by:
- incorporating appropriate treatment technology before exhaust emissions are released to the atmosphere
  - ensuring that the location and design of chimneys or exhaust flues maximises dispersion and takes into account the location of nearby sensitive uses.

#### Rural Interface

- 13 The potential for adverse impacts resulting from rural development should be minimised by:
- not locating horticulture or intensive animal keeping on land adjacent to townships
  - maintaining an adequate separation between horticulture or intensive animal keeping and townships, other sensitive uses and, where desirable, other forms of primary production.
- 14 Traffic movement, spray drift, dust, noise, odour and the use of frost fans and gas guns associated with primary production should not lead to unreasonable impact on adjacent land uses.
- 15 Existing primary production and mineral extraction should not be prejudiced by the inappropriate encroachment of sensitive uses such as urban development.
- 16 Development that is adjacent to land used for primary production (within either the zone or adjacent zones) should include appropriate setbacks and vegetative plantings designed to minimise the potential impacts of chemical spray drift and other impacts associated with primary production.

- 17 New urban development should provide a buffer of at least 40 metres wide (inclusive of any fuel break, emergency vehicle access or road) separating urban and rural activities.
- 18 Development located within 300 metres of facilities for the handling, transportation and storage of bulk commodities should:
  - (a) not prejudice the continued operation of those facilities
  - (b) be located, designed and developed having regard to the potential environmental impact arising from the operation of such facilities and the potential extended hours of operation.



## Landscaping, Fences and Walls

### OBJECTIVES

- 1 The amenity of land and development enhanced with appropriate planting and other landscaping works, using locally indigenous plant species where possible.
- 2 Functional fences and walls that enhance the attractiveness of development.

### PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development should incorporate open space and landscaping and minimise hard paved surfaces in order to:
  - (a) complement built form and reduce the visual impact of larger buildings (eg taller and broader plantings against taller and bulkier building components)
  - (b) enhance the appearance of road frontages
  - (c) screen service yards, loading areas and outdoor storage areas
  - (d) minimise maintenance and watering requirements
  - (e) enhance and define outdoor spaces, including car parking areas
  - (f) maximise shade and shelter
  - (g) assist in climate control within and around buildings
  - (h) minimise heat absorption and reflection
  - (i) maintain privacy
  - (j) maximise stormwater re-use
  - (k) complement existing vegetation, including native vegetation
  - (l) contribute to the viability of ecosystems and species
  - (m) promote water and biodiversity conservation.
- 2 Landscaping should:
  - (a) include the planting of locally indigenous species where appropriate
  - (b) be oriented towards the street frontage
  - (c) result in the appropriate clearance from powerlines and other infrastructure being maintained.
- 3 Landscaping should not:
  - (a) unreasonably restrict solar access to adjoining development
  - (b) cause damage to buildings, paths and other landscaping from root invasion, soil disturbance or plant overcrowding

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- (c) introduce pest plants
- (d) increase the risk of bushfire
- (e) remove opportunities for passive surveillance
- (f) increase leaf fall in watercourses
- (g) increase the risk of weed invasion
- (h) obscure driver sight lines
- (i) create a hazard for train or tram drivers by obscuring sight lines at crossovers.

4 Fences and walls, including retaining walls, should:

- (a) not result in damage to neighbouring trees
- (b) be compatible with the associated development and with existing predominant, attractive fences and walls in the locality
- (c) enable some visibility of buildings from and to the street to enhance safety and allow casual surveillance
- (d) incorporate articulation or other detailing where there is a large expanse of wall facing the street
- (e) assist in highlighting building entrances
- (f) be sited and limited in height, to ensure adequate sight lines for motorists and pedestrians especially on corner sites
- (g) in the case of side and rear boundaries, be of sufficient height to maintain privacy and/or security without adversely affecting the visual amenity or access to sunlight of adjoining land
- (h) be constructed of non-flammable materials.

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Natural Resources

## Natural Resources

### OBJECTIVES

- 1 Retention, protection and restoration of the natural resources and environment.
- 2 Protection of the quality and quantity of South Australia's surface waters, including inland, *marine and estuarine* and underground waters.
- 3 The ecologically sustainable use of natural resources including water resources, including *marine waters*, ground water, surface water and watercourses.
- 4 Natural hydrological systems and environmental flows reinstated, and maintained and enhanced.
- 5 Development consistent with the principles of water sensitive design.
- 6 Development sited and designed to:
  - (a) protect natural ecological systems
  - (b) achieve the sustainable use of water
  - (c) protect water quality, including receiving waters
  - (d) reduce runoff and peak flows and prevent the risk of downstream flooding
  - (e) minimise demand on reticulated water supplies
  - (f) maximise the harvest and use of stormwater
  - (g) protect stormwater from pollution sources.
- 7 Storage and use of stormwater which avoids adverse impact on public health and safety.
- 8 Native flora, fauna and ecosystems protected, retained, conserved and restored.
- 9 Restoration, expansion and linking of existing native vegetation to facilitate habitat corridors for ease of movement of fauna.
- 10 Minimal disturbance and modification of the natural landform.
- 11 Protection of the physical, chemical and biological quality of soil resources.
- 12 Protection of areas prone to erosion or other land degradation processes from inappropriate development.
- 13 Protection of the scenic qualities of natural and rural landscapes.

### PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development should be undertaken with minimum impact on the natural environment, including air and water quality, land, soil, biodiversity, and scenically attractive areas.
- 2 Development should ensure that South Australia's natural assets, such as biodiversity, water and soil, are protected and enhanced.

3 Development should not significantly obstruct or adversely affect sensitive ecological areas such as creeks, wetlands, estuaries and significant seagrass and mangrove communities.

4 Development should be appropriate to land capability and the protection and conservation of water resources and biodiversity.

#### **Water Sensitive Design**

5 Development should be designed to maximise conservation, minimise consumption and encourage re-use of water resources.

6 Development should not take place if it results in unsustainable use of surface or underground water resources.

7 Development should be sited and designed to:

(a) capture and re-use stormwater, where practical

(b) minimise surface water runoff

(c) prevent soil erosion and water pollution

(d) protect and enhance natural water flows

(e) protect water quality by providing adequate separation distances from watercourses and other water bodies

(f) not contribute to an increase in salinity levels

(g) avoid the water logging of soil or the release of toxic elements

(h) maintain natural hydrological systems and not adversely affect:

(i) the quantity and quality of groundwater

(ii) the depth and directional flow of groundwater

(iii) the quality and function of natural springs.

8 Water discharged from a development site should:

(a) be of a physical, chemical and biological condition equivalent to or better than its pre-developed state

(b) not exceed the rate of discharge from the site as it existed in pre-development conditions.

9 Development should include stormwater management systems to protect it from damage during a minimum of a 1-in-100 year average return interval flood.

10 Development should have adequate provision to control any stormwater over-flow runoff from the site and should be sited and designed to improve the quality of stormwater and minimise pollutant transfer to receiving waters.

11 Development should include stormwater management systems to mitigate peak flows and manage the rate and duration of stormwater discharges from the site to ensure the carrying capacities of downstream systems are not overloaded.

12 Development should include stormwater management systems to minimise the discharge of sediment, suspended solids, organic matter, nutrients, bacteria, litter and other contaminants to the stormwater system.

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13 Stormwater management systems should preserve natural drainage systems, including the associated environmental flows.

14 Stormwater management systems should:

(a) maximise the potential for stormwater harvesting and re-use, either on-site or as close as practicable to the source

(b) utilise, but not be limited to, one or more of the following harvesting methods:

(i) the collection of roof water in tanks

(ii) the discharge to open space, landscaping or garden areas, including strips adjacent to car parks

(iii) the incorporation of detention and retention facilities

(iv) aquifer recharge.

15 Where it is not practicable to detain or dispose of stormwater on site, only clean stormwater runoff should enter the public stormwater drainage system.

16 Artificial wetland systems, including detention and retention basins, should be sited and designed to:

(a) ensure public health and safety is protected

(b) minimise potential public health risks arising from the breeding of mosquitoes.

#### **Water Catchment Areas**

17 Development should ensure watercourses and their beds, banks, wetlands and floodplains are not damaged or modified and are retained in their natural state, except where modification is required for essential access or maintenance purposes.

18 No development should occur where its proximity to a swamp or wetland will damage or interfere with the hydrology or water regime of the swamp or wetland.

19 A wetland or low-lying area providing habitat for native flora and fauna should not be drained, except temporarily for essential management purposes to enhance environmental values.

20 Along watercourses, areas of remnant native vegetation, or areas prone to erosion, that are capable of natural regeneration should be fenced off to limit stock access.

21 Development such as cropping, intensive animal keeping, residential, tourism, industry and horticulture, that increases the amount of surface run-off should include a strip of land at least 20 metres wide measured from the top of existing banks on each side of a watercourse that is:

(a) fenced to exclude livestock

(b) kept free of development, including structures, formal roadways or access ways for machinery or any other activity causing soil compaction or significant modification of the natural surface of the land

(c) revegetated with locally indigenous vegetation comprising trees, shrubs and other groundcover plants to filter run-off so as to reduce the impacts on native aquatic ecosystems and to minimise soil loss eroding into the watercourse.

- 22 Development resulting in the depositing of an object or solid material in a watercourse or floodplain or the removal of bank and bed material should not:
- (a) adversely affect the migration of aquatic biota
  - (b) adversely affect the natural flow regime
  - (c) cause or contribute to water pollution
  - (d) result in watercourse or bank erosion
  - (e) adversely affect native vegetation upstream or downstream that is growing in or adjacent to a watercourse.
- 23 The location and construction of dams, water tanks and diversion drains should:
- (a) occur off watercourse
  - (b) not take place in ecologically sensitive areas or on erosion-prone sites
  - (c) provide for low flow by-pass mechanisms to allow for migration of aquatic biota
  - (d) not negatively affect downstream users
  - (e) minimise in-stream or riparian vegetation loss
  - (f) incorporate features to improve water quality (eg wetlands and floodplain ecological communities)
  - (g) protect ecosystems dependent on water resources.
- 24 Irrigated horticulture and pasture should not increase groundwater-induced salinity.
- 25 Development should comply with the current *Environment Protection (Water Quality) Policy*.

#### **Biodiversity and Native Vegetation**

- 26 Development should retain existing areas of native vegetation and where possible contribute to revegetation using locally indigenous plant species.
- 27 Development should be designed and sited to minimise the loss and disturbance of native flora and fauna, including marine animals and plants, and their breeding grounds and habitats.
- 28 Native vegetation should be conserved and its conservation value and function not compromised by development if the native vegetation does any of the following:
- (a) provides an important habitat for wildlife or shade and shelter for livestock
  - (b) has a high plant species diversity or includes rare, vulnerable or endangered plant species or plant associations and communities
  - (c) provides an important seed bank for locally indigenous vegetation
  - (d) has high amenity value and/or significantly contributes to the landscape quality of an area, including the screening of buildings and unsightly views
  - (e) has high value as a remnant of vegetation associations characteristic of a district or region prior to extensive clearance for agriculture
  - (f) is growing in, or is characteristically associated with a wetland environment.

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- 29 Native vegetation should not be cleared if such clearing is likely to lead to, cause or exacerbate any of the following:
- (a) erosion or sediment within water catchments
  - (b) decreased soil stability
  - (c) soil or land slip
  - (d) deterioration in the quality of water in a watercourse or surface water runoff
  - (e) a local or regional salinity problem
  - (f) the occurrence or intensity of local or regional flooding.
- 30 Development that proposes the clearance of native vegetation should address or consider the implications that removing the native vegetation will have on the following:
- (a) provision for linkages and wildlife corridors between significant areas of native vegetation
  - (b) erosion along watercourses and the filtering of suspended solids and nutrients from run-off
  - (c) the amenity of the locality
  - (d) bushfire safety
  - (e) the net loss of native vegetation and other biodiversity.
- 31 Where native vegetation is to be removed, it should be replaced in a suitable location on the site with locally indigenous vegetation to ensure that there is not a net loss of native vegetation and biodiversity.
- 32 Development should be located and occur in a manner which:
- (a) does not increase the potential for, or result in, the spread of pest plants, or the spread of any non-indigenous plants into areas of native vegetation or a conservation zone
  - (b) avoids the degradation of remnant native vegetation by any other means including as a result of spray drift, compaction of soil, modification of surface water flows, pollution to groundwater or surface water or change to groundwater levels
  - (c) incorporates a separation distance and/or buffer area to protect wildlife habitats and other features of nature conservation significance.
- 33 Development should promote the long-term conservation of vegetation by:
- (a) avoiding substantial structures, excavations, and filling of land in close proximity to the trunk of trees and beneath their canopies
  - (b) minimising impervious surfaces beneath the canopies of trees
  - (c) taking other effective and reasonable precautions to protect both vegetation and the integrity of structures and essential services.
- 34 Horticulture involving the growing of olives should be located at least:
- (a) 500 metres from:
    - (i) a national park
    - (ii) a conservation park

- (iii) a wilderness protection area
  - (iv) the edge of a substantially intact stratum of native vegetation greater than 5 hectares in area
  - (b) 50 metres from the edge of stands of native vegetation 5 hectares or less in area.
- 35 Horticulture involving the growing of olives should have at least one locally indigenous tree that will grow to a height of at least 7 metres sited at least every 100 metres around the perimeter of the orchard.

**Soil Conservation**

- 36 Development should not have an adverse impact on the natural, physical, chemical or biological quality and characteristics of soil resources.
- 37 Development should be designed and sited to prevent erosion.
- 38 Development should take place in a manner that will minimise alteration to the existing landform.
- 39 Development should minimise the loss of soil from a site through soil erosion or siltation during the construction phase of any development and following the commencement of an activity.



## Orderly and Sustainable Development

### OBJECTIVES

- 1 Orderly and economical development that creates a safe, convenient and pleasant environment in which to live.
- 2 Development occurring in an orderly sequence and in a compact form to enable the efficient provision of public services and facilities.
- 3 Development that does not jeopardise the continuance of adjoining authorised land uses.
- 4 Development that does not prejudice the achievement of the provisions of the Development Plan.
- 5 Development abutting adjoining Council areas having regard to the policies of that Council's Development Plan.
- 6 Urban development contained within existing townships and settlements and located only in zones designated for such development.

### PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development should not prejudice the development of a zone for its intended purpose.
- 2 Land outside of townships and settlements should primarily be used for primary production and conservation purposes.
- 3 The economic base of the region should be expanded in a sustainable manner.
- 4 Urban development should form a compact extension to an existing built-up area.
- 5 Ribbon development should not occur along the coast, water frontages or arterial roads shown in *Overlay Maps - Transport*.
- 6 Development should be located and staged to achieve the economical provision of public services and infrastructure, and to maximise the use of existing services and infrastructure.
- 7 Where development is expected to impact upon the existing infrastructure network (including the transport network), development should demonstrate how the undue effect will be addressed.
- 8 Vacant or underutilised land should be developed in an efficient and co-ordinated manner to not prejudice the orderly development of adjacent land.

## Siting and Visibility

### OBJECTIVES

- 1 Protection of scenically attractive areas, particularly natural, rural and coastal landscapes.

### PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development should be sited and designed to minimise its visual impact on:
  - (a) the natural, rural or heritage character of the area
  - (b) areas of high visual or scenic value, particularly rural and coastal areas
  - (c) views from the coast, near-shore waters, public reserves, tourist routes and walking trails
  - (d) the amenity of public beaches
- 2 Buildings should be sited in unobtrusive locations and, in particular, should:
  - (a) be grouped together
  - (b) where possible be located in such a way as to be screened by existing vegetation when viewed from public roads.
- 3 Buildings and structures on land outside of urban areas should be designed to minimise their visual impact in the landscape, in particular:
  - (a) the profile of buildings should be low and the rooflines should complement the natural form of the land
  - (b) the mass of buildings should be minimised by variations in wall and roof lines and by floor plans which complement the contours of the land
  - (c) large eaves, verandas and pergolas should be incorporated into designs so as to create shadowed areas that reduce the bulky appearance of buildings.
- 4 The nature of external surface materials of buildings should not detract from the visual character and amenity of the landscape.
- 5 The number of buildings and structures on land outside of urban areas should be limited to that necessary for the efficient management of the land.
- 6 Driveways and access tracks should be designed and surfaced to blend sympathetically with the landscape and to minimise interference with natural vegetation and landforms.
- 7 Development should be screened through the establishment of landscaping using locally indigenous plant species:
  - (a) around buildings and earthworks to provide a visual screen as well as shade in summer, and protection from prevailing winds
  - (b) along allotment boundaries to provide permanent screening of buildings and structures when viewed from adjoining properties and public roads
  - (c) along the verges of new roads and access tracks to provide screening and minimise erosion.

## Transportation and Access

### OBJECTIVES

- 1 A comprehensive, integrated, affordable and efficient air, rail, sea, road, cycle and pedestrian transport system that will:
  - (a) provide equitable access to a range of public, community and private transport services for all people
  - (b) ensure a high level of safety
  - (c) effectively support the economic development of the State
  - (d) have minimal negative environmental and social impacts
  - (e) maintain options for the introduction of suitable new transport technologies.
- 2 Development that:
  - (a) provides safe and efficient movement for all motorised and non-motorised transport modes
  - (b) ensures access for vehicles including emergency services, public infrastructure maintenance and commercial vehicles
  - (c) provides off street parking
  - (d) is appropriately located so that it supports and makes best use of existing transport facilities and networks.
- 3 A road hierarchy that promotes safe and efficient transportation in an integrated manner throughout the State.
- 4 Provision of safe, pleasant, accessible, integrated and permeable pedestrian and cycling networks.
- 5 Safe and convenient freight movement throughout the State.

### PRINCIPLES OF DEVELOPMENT CONTROL

#### Land Use

- 1 Land uses arranged to support the efficient provision of sustainable transport networks and encourage their use.

#### Movement Systems

- 2 Development should be integrated with existing transport networks, particularly major rail and road corridors as shown on *Location Maps* and *Overlay Maps - Transport*, and designed to minimise its potential impact on the functional performance of the transport networks.
- 3 Transport corridors should be sited and designed so as to not unreasonably interfere with the health and amenity of adjacent sensitive land uses.
- 4 Roads should be sited and designed to blend with the landscape and be in sympathy with the terrain.

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- 5 Land uses that generate large numbers of visitors such as shopping centres and areas, places of employment, schools, hospitals and medium to high density residential uses should be located so that they can be serviced by existing transport networks and encourage walking and cycling.
- 6 Development generating high levels of traffic, such as schools, shopping centres and other retail areas, entertainment and sporting facilities, should incorporate passenger pick-up and set down areas. The design of such areas should ensure interference to existing traffic is minimised and give priority to pedestrians, cyclists and public and community transport users.
- 7 The location and design of public and community transport set-down and pick-up points should maximise safety and minimise the isolation and vulnerability of users.
- 8 Development should provide safe and convenient access for all anticipated modes of transport including cycling, walking, public and community transport, and motor vehicles.
- 9 Development at intersections, pedestrian and cycle crossings, and crossovers to allotments should maintain or enhance sightlines for motorists, cyclists and pedestrians to ensure safety for all road users and pedestrians.
- 10 Driveway crossovers affecting pedestrian footpaths should maintain the level of the footpath.
- 11 Development should discourage commercial and industrial vehicle movements through residential streets and adjacent other sensitive land uses such as schools.
- 12 Industrial/commercial vehicle movements should be separated from passenger vehicle car-parking areas.
- 13 Development should make sufficient provision on site for the loading, unloading and turning of all traffic likely to be generated.

**Cycling and Walking**

- 14 Development should ensure that a permeable street and path network is established that encourages walking and cycling through the provision of safe, convenient and attractive routes with connections to adjoining streets, paths, open spaces, schools, public and community transport stops and activity centres.
- 15 Development should provide access, and accommodate multiple route options, for cyclists by enhancing and integrating with:
  - (a) open space networks, recreational trails, parks, reserves and recreation areas
  - (b) Adelaide's Metropolitan Open Space System.
- 16 Cycling and pedestrian networks should be designed to be permeable and facilitate direct and efficient passage to neighbouring networks and facilities.
- 17 New developments should give priority to and not compromise existing designated bicycle routes.
- 18 Where development coincides with, intersects or divides a proposed bicycle route or corridor, development should incorporate through-access for cyclists.
- 19 Developments should encourage and facilitate cycling as a mode of transport by incorporating end-of-journey facilities including:
  - (a) showers, changing facilities, and secure lockers
  - (b) signage indicating the location of bicycle facilities

(c) secure bicycle parking facilities provided at the rate set out in *Table Sal/3 - Off Street Bicycle Parking Requirements*.

- 20 Pedestrian facilities and networks should be designed and provided in accordance with relevant provisions of the *Australian Standards and Austroads Guide to Traffic Engineering Practice Part 13*.
- 21 Cycling facilities and networks should be designed and provided in accordance with the relevant provisions of the *Australian Standards and Austroads Guide to Traffic Engineering Practice Part 14*.

#### Access

- 22 Development should have direct access from an all weather public road.
- 23 Development should be provided with safe and convenient access which:
- (a) avoids unreasonable interference with the flow of traffic on adjoining roads
  - (b) provides appropriate separation distances from existing roads or level crossings
  - (c) accommodates the type and volume of traffic likely to be generated by the development or land use and minimises induced traffic through over-provision
  - (d) is sited and designed to minimise any adverse impacts on the occupants of and visitors to neighbouring properties.
- 24 Development should not restrict access to publicly owned land such as recreation areas.
- 25 The number of vehicle access points onto arterial roads shown on *Overlay Maps - Transport* should be minimised, and where possible access points should be:
- (a) limited to local roads
  - (b) shared between developments.
- 26 The number of access points for cyclists and pedestrians onto all adjoining roads should be maximised.
- 27 Development with access from roads with existing or projected traffic volumes exceeding 6000 vehicles per day should be sited to avoid the need for vehicles to reverse on to or from the road.
- 28 Development with access from arterial roads or roads as shown on *Overlay Maps - Transport* should be sited to avoid the need for vehicles to reverse on to or from the road.
- 29 Driveways, access tracks and parking areas should be designed and constructed to:
- (a) follow the natural contours of the land
  - (b) minimise excavation and/or fill
  - (c) minimise the potential for erosion from run-off
  - (d) avoid the removal of existing vegetation
  - (e) be consistent with *Australian Standard AS 2890 Parking facilities*.

#### Access for People with Disabilities

- 30 Development should be sited and designed to provide convenient access for people with a disability.
- 31 Where appropriate and practical, development should provide for safe and convenient access to the coast and beaches for disabled persons.

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### Vehicle Parking

- 32 Development should provide off-street vehicle parking and specifically marked disabled car parking places to meet anticipated demand in accordance with *Table Sal/2 - Off Street Vehicle Parking Requirements* or *Table Sal/2A - Off Street Vehicle Parking Requirements for Designated Areas* (whichever applies) unless an agreement is reached between the Council and the applicant for a reduced number of parking spaces where one of the following applies:
- a financial contribution is paid into the Council Car Parking Funds specified by the Council, in accordance with the gazetted rate per car park associated with the 'Car Park Fund Areas' identified on *Concept Plan Map Sal/27 - Salisbury District Centre Car Park Fund Area*, *Concept Plan Map Sal/29 - Ingle Farm District Centre Car Park Fund Area* and *Concept Plan Map Sal/32 - Mawson Lakes Town Centre Car Parking Fund Area*
  - it can be demonstrated that fewer car parks would be required to meet the car parking needs associated with the development.
- 33 Development should be consistent with *Australian Standard AS 2890 Parking facilities*.
- 34 Vehicle parking areas should be sited and designed in a manner that will:
- facilitate safe and convenient pedestrian linkages to the development and areas of significant activity or interest in the vicinity of the development
  - include safe pedestrian and bicycle linkages that complement the overall pedestrian and cycling network
  - not inhibit safe and convenient traffic circulation
  - result in minimal conflict between customer and service vehicles
  - avoid the necessity to use public roads when moving from one part of a parking area to another
  - minimise the number of vehicle access points to public roads
  - avoid the necessity for backing onto public roads
  - where reasonably possible, provide the opportunity for shared use of car parking and integration of car parking areas with adjoining development to reduce the total extent of vehicle parking areas and the requirement for access points
  - not dominate the character and appearance of a site when viewed from public roads and spaces
  - provide landscaping that will shade and enhance the appearance of the vehicle parking areas.
- 35 Vehicle parking areas should be designed to reduce opportunities for crime by:
- maximising the potential for passive surveillance by ensuring they can be overlooked from nearby buildings and roads
  - incorporating walls and landscaping that do not obscure vehicles or provide potential hiding places
  - being appropriately lit
  - having clearly visible walkways.
- 36 Where parking areas are not obviously visible or navigated, signs indicating the location and availability of vehicle parking spaces associated with businesses should be displayed at locations readily visible to customers.

- 37 Parking areas that are likely to be used during non daylight hours should provide floodlit entrance and exit points and site lighting directed and shaded in a manner that will not cause nuisance to adjacent properties or users of the car park.
- 38 Parking areas should be sealed or paved in order to minimise dust and mud nuisance.
- 39 To assist with stormwater detention and reduce heat loads in summer, vehicle parking areas should include soft (living) landscaping.
- 40 Parking areas should be line-marked to indicate parking bays, movement aisles and direction of traffic flow.

#### Vehicle Parking for Residential Development

- 41 On-site vehicle parking should be provided having regard to:
- the number, nature and size of proposed dwellings
  - proximity to centre facilities, public and community transport within walking distance of the dwellings
  - the anticipated mobility and transport requirements of the likely occupants, particularly groups such as aged persons.
- 42 Vehicle parking areas servicing more than one dwelling should be of a size and location to:
- serve users, including pedestrians, cyclists and motorists, efficiently, conveniently and safely
  - provide adequate space for vehicles, including emergency service vehicles, to manoeuvre between the street and the parking area
  - reinforce or contribute to attractive streetscapes.

#### Vehicle Parking for Mixed Use and Corridor Zones

- 43 Loading areas and designated parking spaces for service vehicles should:
- be provided within the boundary of the site
  - not be located in areas where there is parking provided for any other purpose.
- 44 Vehicle parking spaces and multi-level vehicle parking structures within buildings should:
- enhance active street frontages by providing land uses such as commercial, retail or other non-car park uses along ground floor street frontages
  - complement the surrounding built form in terms of height, massing and scale
  - incorporate facade treatments along major street frontages that are sufficiently enclosed and detailed to complement neighbouring buildings consistent with the desired character of the locality.
- 45 In mixed use buildings, the provision of vehicle parking may be reduced in number and shared where the operating hours of commercial activities complement the residential use of the site.

#### Undercroft and Below Ground Garaging and Parking of Vehicles

- 46 Undercroft and below ground garaging of vehicles should only occur where envisaged in the relevant zone or policy area or precinct and ensure:
- the overall height and bulk of the undercroft structure does not adversely impact on streetscape character of the locality or the amenity of adjacent properties

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- (b) vehicles can safely enter and exit from the site without compromising pedestrian or cyclist safety or causing conflict with other vehicles
  - (c) driveway gradients provide for safe and functional entry and exit
  - (d) driveways and adjacent walls, fencing and landscaping are designed to provide adequate sightlines from vehicles to pedestrians using the adjacent footpath
  - (e) openings to undercroft areas are integrated with the main building so as to minimise visual impact
  - (f) landscaping, mounding and/or fencing is incorporated to improve its presentation to the street and to adjacent properties
  - (g) the overall streetscape character of the locality is not adversely impaired (e.g. visual impact, building bulk, front setbacks relative to adjacent development).
- 47 In the case of undercroft and below ground car parks where cars are visible from public areas, adequate screening and landscaping should be provided.



## Waste

### OBJECTIVES

- 1 Development that, in order of priority, avoids the production of waste, minimises the production of waste, reuses waste, recycles waste for reuse, treats waste and disposes of waste in an environmentally sound manner.
- 2 Development that includes the treatment and management of solid and liquid waste to prevent undesired impacts on the environment including, soil, plant and animal biodiversity, human health and the amenity of the locality.

### PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development should be sited and designed to prevent or minimise the generation of waste (including wastewater) by applying the following waste management hierarchy in the order of priority as shown below:
  - (a) avoiding the production of waste
  - (b) minimising waste production
  - (c) reusing waste
  - (d) recycling waste
  - (e) recovering part of the waste for re-use
  - (f) treating waste to reduce the potentially degrading impacts
  - (g) disposing of waste in an environmentally sound manner.
- 2 The storage, treatment and disposal of waste materials from any development should be achieved without risk to health or impairment of the environment.
- 3 Development should avoid as far as practical, the discharge or deposit of waste (including wastewater) onto land or into any waters (including processes such as seepage, infiltration or carriage by wind, rain, sea spray, stormwater or by the rising of the water table).
- 4 Untreated waste should not be discharged to the environment, and in particular to any water body.
- 5 Development should include appropriately sized area to facilitate the storage of receptacles that will enable the efficient recycling of waste.
- 6 Development that involves the production and/or collection of waste and/or recyclable material should include designated collection and storage area(s) that are:
  - (a) screened and separated from adjoining areas
  - (b) located to avoid impacting on adjoining sensitive environments or land uses
  - (c) designed to ensure that wastes do not contaminate stormwater or enter the stormwater collection system
  - (d) located on an impervious sealed area graded to a collection point in order to minimise the movement of any solids or contamination of water

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(e) protected from wind and stormwater and sealed to prevent leakage and minimise the emission of odours

(f) stored in such a manner that ensures that all waste is contained within the boundaries of the site until disposed of in an appropriate manner.

#### Wastewater

- 7 The disposal of wastewater to land should only occur where methods of wastewater reduction and reuse are unable to remove the need for its disposal, and where its application to the land is environmentally sustainable.
- 8 Wastewater lagoons should not be sited in any of the following areas:
  - (a) within land subject to a 1-in-100 year average return interval flood event
  - (b) within 50 metres of the top of the bank of a watercourse
  - (c) within 500 metres of the coastal high water mark
  - (d) where the base of the lagoon would be below any seasonal water table.
- 9 Artificial wetland systems for the storage of treated wastewater, such as wastewater lagoons, should be:
  - (a) sufficiently separated from adjoining sensitive uses to minimise potential adverse odour impacts
  - (b) sited and designed to minimise potential public health risks arising from the breeding of mosquitoes.

#### Waste Treatment Systems

- 10 Development that produces any sewage or effluent should be connected to a waste treatment system that complies with (or can comply with) the relevant public and environmental health legislation applying to that type of system.
- 11 The methods for, and siting of, effluent and waste storage, treatment and disposal systems should minimise the potential for environmental harm and adverse impacts on:
  - (a) the quality of surface and groundwater resources
  - (b) public health
  - (c) the amenity of a locality
  - (d) sensitive land uses.
- 12 Waste treatment should only occur where the capacity of the treatment facility is sufficient to accommodate likely maximum daily demands including a contingency for unexpected high flows and breakdowns.
- 13 Any on-site wastewater treatment system/ re-use system or effluent drainage field should be located within the allotment of the development that it will service.
- 14 A dedicated on-site effluent disposal area should not include any areas to be used for, or could be reasonably foreseen to be used for, private outdoor open space, driveways, car parking or outbuildings.
- 15 The spreading or discharging of treated liquid or solid waste onto the ground should only occur where the disposal area consists of soil and vegetation that has the capacity to store and use the waste without contaminating soil or surface or ground water resources or damaging crops.

- 16 Stock slaughter works, poultry processors, saleyards, piggeries, cattle feedlots, milking sheds, milk processing works, fish processing works, wineries, distilleries, tanneries and fellmongeries, composting works, waste or recycling depots and concrete batching works should have a wastewater management system that is designed so as not to discharge wastes generated by the premises:
- (a) into any waters
  - (b) onto land in a place where it is reasonably likely to enter any waters by processes such as:
    - (i) seepage
    - (ii) infiltration
    - (iii) carriage by wind, rain, sea spray, or stormwater
    - (iv) the rising of the watertable.
- 17 Winery waste management systems should be designed to ensure:
- (a) surface runoff does not occur from the wastewater irrigation area at any time
  - (b) wastewater is not irrigated onto waterlogged areas, land within 50 metres of a creek, or swamp or domestic or stock water bore, or land subject to flooding, steeply sloping land, or rocky or highly permeable soil overlaying an unconfined aquifer
  - (c) wastewater is not irrigated over an area which is within 50 metres of any residence on neighbouring land or 10 metres of any type of publicly owned land
  - (d) wastewater is released using low trajectory low pressure sprinklers, drip irrigators or agricultural pipe, and is not sprayed more than 1.5 metres into the air or in fine droplets if there is a potential for the spread of diseases from the wastewater
  - (e) stormwater run-off from areas which are contaminated with grape or grape products is drained to winery waste management systems during vintage periods
  - (f) stormwater from roofs and clean hard paved surfaces is diverted away from winery waste management systems and disposed of in an environmentally sound manner or used for productive purposes.

## Residential Zone

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this zone.

### OBJECTIVES

- 1 A residential zone comprising a range of dwelling types, including a minimum of 15 per cent affordable housing.
- 2 Increased dwelling densities in close proximity to centres, public transport routes and public open spaces.
- 3 A zone primarily accommodating a range of dwellings to meet the diverse needs and preferences of the community.
- 4 Revitalisation of under-utilised suburbs, particularly public housing areas, to facilitate the provision of a greater mix of housing types and improved residential amenity.
- 5 Residential development designed and insulated to minimise effects of predicted aircraft noise and industrial noise.
- 6 Development that contributes to the desired character of the zone.

### DESIRED CHARACTER

The zone covers a substantial portion of the Council area and is home to a large and diverse population. The zone is suitable for a range of low to medium density housing, with higher density in appropriate locations. In addition to housing and open space, the zone will also contain a variety of community related, non-residential land uses that contribute to the creation of pleasant, connected, well serviced and socially inclusive neighbourhoods.

A full range of dwelling types on a variety of allotment sizes will be provided, including affordable housing, special needs housing and housing for the aged in appropriate locations. Housing form will be diverse and cater for different household sizes, life cycle stages and housing preferences and will be cost-effective, energy efficient and will make efficient use of available sites. Older neighbourhoods will experience change through turnover in population and ageing of existing residents which will generate a need for more housing diversity as well as changes in density and different services and facilities to complement these changes.

Residential development will reflect a variety of building styles, yet where a consistent character exists, new buildings will be designed to harmonise with that which is existing in terms of form, mass, scale, colours and textures of materials and setback distances. A cohesive built form will be achieved through design elements such as interesting roof forms, articulated buildings, recessed vehicle garaging, and landscaped spaces between buildings and the public road. Buildings will include windows, doors and balconies that overlook the street and areas of public open space to promote surveillance and community interaction.

Setbacks to local streets will be used to enable landscaping to be provided to soften the built form and improve amenity for residents and pedestrians. The visual and physical impact of vehicle access points will be minimised by reducing both the number of access points and the width of associated crossovers and driveways. Sealed surfaces will be minimised to reduce stormwater run-off and to also provide opportunities for landscaping to improve visual amenity. Development will seek to protect and retain regulated trees that contribute to the character of the area.

In order to achieve a compact urban form an overall increase in the residential density is desirable in select locations, in keeping with the objectives for development in the Zone, although it is anticipated that the predominant form of housing will remain detached dwellings at low to medium density throughout much of the zone. Co-ordinated infill housing, redevelopment and refurbishment of existing housing are encouraged

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to maximise use of the community investment in facilities and services in existing residential areas that are located close to centres and community services. Opportunities exist to gradually develop under-utilised and vacant land within established areas, particularly when a specific housing need arises within the population, but also as an opportunity for improving access to existing services and facilities. Co-ordinated and efficient development of larger sites containing multiple allotments is encouraged in order to achieve greater housing density and diversity.

Medium density forms of housing including semi-detached dwellings, row dwellings, residential flat buildings and group dwellings are encouraged in areas with good access to services and facilities. Concentrated nodes of medium density development of up to 4 storeys in height are anticipated in areas close to centres, public transport and significant public open space, and are to be developed in a co-ordinated and orderly manner. Typically this will involve the amalgamation of sites and will result in development that compliments the prevailing character. A transition in building height will be necessary to ensure issues of overlooking is minimised and solar access maintained.

Development will be of a form and scale compatible with adjoining residential development. As part of the increase in residential density there will be an increase in the number of dwellings of two or more storeys. Such development will recognise the proximity of adjoining development and will be designed and sited to maintain the privacy of adjoining dwellings and their private open space areas, and will be designed to integrate with the existing built form.

Development of land with a known history of a potentially contaminating activity will occur once the site has been assessed and remediated to the standard necessary to ensure it is safe and suitable for the proposed use.

Development in areas which adjoin industrial land uses and areas affected by aircraft noise as identified in [Concept Plan Map Sal/2 - Edinburgh Defence Airfield Aircraft Noise Exposure](#) should be designed, sited and constructed to minimise the effects of noise.

## PRINCIPLES OF DEVELOPMENT CONTROL

### Land Use

- 1 The following forms of development are envisaged in the zone:
  - affordable housing
  - domestic outbuilding in association with a dwelling
  - domestic structure
  - dwelling
  - dwelling addition
  - small scale non-residential use that serves the local community, for example:
    - child care facility
    - health and welfare service
    - open space
    - primary or secondary school
  - recreation area
  - supported accommodation.
- 2 Development listed as non-complying is generally inappropriate.
- 3 Vacant or underutilised land should be developed in an efficient and co-ordinated manner to increase housing choice by providing dwellings at densities higher than, but compatible in form, scale and design with adjoining residential development.
- 4 Non-residential development such as shops, schools and consulting rooms should be of a nature and scale that:
  - (a) serves the local community
  - (b) is consistent with the character of the locality

(c) does not detrimentally impact on the amenity of nearby residents

(d) will not undermine the efficient operation of nearby centres.

- 5 The use and placement of outbuildings should be ancillary to and in association with a dwelling or dwellings.

#### Form and Character

- 6 Development should be undertaken in accordance with the following Concept Plan Maps:
- (a) [Concept Plan Map Sal/21 - Parlowie Residential Area 3](#)
  - (b) [Concept Plan Map Sal/22 - Burton Residential Area 1](#)
  - (c) [Concept Plan Map Sal/23 - Direk Residential Area](#)
  - (d) [Concept Plan Map Sal/24 - Frost Road/Brown Terrace Salisbury](#)
  - (e) [Concept Plan Map Sal/25 - Parlowie Residential Area 1](#)
  - (f) [Concept Plan Map Sal/26 - Parlowie Residential Area 2](#)
  - (g) [Concept Plan Map Sal/27 - Salisbury Downs Residential Area 1.](#)
- 7 Residential development should only occur within the area marked 'Lot 23' or 'Lot 24' on [Concept Plan Map Sal/24 - Frost Road/Brown Terrace Salisbury](#) where it is developed in accordance with all of the following:
- (a) the construction of a 2.4 metre high acoustical fence, which is comprised of double-sided fibre cement sheeting that has a density of not less than 12.5 kilograms per square metre, along the north-west and south-west boundaries of the land
  - (b) service areas are located between the noise source and the living areas of the dwelling
  - (c) dwellings utilise suitable sound insulation methods such as double-glazing to windows, roof and wall insulation
  - (d) the number of wall openings facing the noise source are minimised.
- 8 Direct vehicular access should not be provided onto Smith Road, Barker Road or Bridge Road.
- 9 Where remnant vegetation is located within road reserves it should be preserved and protected against loss, damage and disfigurement.
- 10 Garages and carports facing the street (other than an access lane way) should be designed with a maximum width of 6 metres or 50 per cent of the allotment or building site frontage width, whichever is the lesser distance.
- 11 Sheds, garages and similar outbuildings should be designed within the following parameters:

Parameter	Value
Maximum floor area for allotments less than 450 square metres	60 square metres
Maximum floor area for allotment greater than 450 square metres	72 square metres
Maximum building height (from natural ground level)	5 metres
Maximum wall height (from natural ground level)	3 metres

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**Affordable Housing**

- 12 Development should include a minimum 15 per cent of residential dwellings for affordable housing.
- 13 Affordable housing should be distributed throughout the zone to avoid over-concentration of similar types of housing in a particular area.

**Land Division**

- 14 Land division should create a 5 metre landscaped reserve to limit direct vehicle access where allotments are created fronting the following roads:
  - (a) Martins Road, Parafield Gardens and Paralowie
  - (b) Burton Road, Burton and Paralowie
  - (c) Bolivar Road, Burton and Paralowie
  - (d) Willochra Road, Salisbury.

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## PROCEDURAL MATTERS

### Complying Development

Complying developments are prescribed in Schedule 4 of the *Development Regulations 2008*.

### Non-complying Development

Development (including building work, a change in the use of land, or division of an allotment) for the following is non-complying:

Form of Development	Exceptions
Advertisement and/or advertising hoarding	Except where it complies with all of the following: <ul style="list-style-type: none"> <li>(a) it does not move, rotate or incorporate flashing lights</li> <li>(b) it does not wholly or partly consist of bunting, streamers, flags, wind vanes and the like</li> <li>(c) if attached to a building, it does not wholly or partly extend above the top of the wall or the fascia.</li> </ul>
Amusement machine centre	
Consulting room	Except where: <ul style="list-style-type: none"> <li>(a) the total floor area is less than 100 square metres</li> <li>(b) the site does not front an arterial road.</li> </ul>
Crematorium	
Dairy	
Dwelling where it is contained within the 'Concept Area Boundary' identified on <a href="#">Concept Plan Map Sal/22 - Burton Residential Area 1</a>	Except where the dwelling has a maximum height of no more than one-storey above natural ground level.
Farming	
Fuel depot	
Horse keeping	
Horticulture	
Industry	
Intensive animal keeping	
Motor repair station	
Office	Except where: <ul style="list-style-type: none"> <li>(a) the total floor area is less than 100 square metres</li> <li>(b) the site does not front an arterial road.</li> </ul>
Petrol filling station	Except where it comprises alterations or additions to a petrol filling station existing at 20 January 1994.
Prescribed mining operations	
Public service depot	
Residential flat building within the <b>Salisbury Residential Policy Area 18</b>	



Form of Development	Exceptions
Restaurant	
Road transport terminal	
Service trade premises	
Shop or group of shops	Except where: (a) the gross leasable area is less than 250 square metres (b) the site does not front an arterial road.
Stock sales yard	
Stock slaughter works	
Store	
Warehouse	
Waste reception, storage, treatment or disposal	
Wrecking yard	

**Public Notification**

Categories of public notification are prescribed in Schedule 9 of the *Development Regulations 2008*.

**Table Sal/1 - Building Setbacks from Road Boundaries**

Road Name	Portion of Road Affected	Setback Distance
Burton Road	The whole length on both sides	8 metres
Carey Street, Salisbury	The north-western side from Commercial Road to Ponton Road	11 metres
Clayson Road, Salisbury East	Both sides from Main North Road to Bridge Road	11 metres
Commercial Road, Salisbury	The whole length on both sides	11 metres
Cross Keys Road, Salisbury South, Salisbury	The whole length on both sides	14 metres
Deuter Road, Burton, Paralowie	The whole length on both sides	14 metres
Fendon Road, Salisbury	The whole length on both sides	14 metres
Frederick Street, Cavan	The whole length on both sides	9.5 metres
Frost Road, Salisbury South	The whole length on both sides	14 metres
Helps Road, Direk, Burton	The whole length on both sides	14 metres
James Street, Salisbury	The northern side between Gawler Street and Parabanks Shopping Centre boundary	12.9 metres
Kelly Terrace, Salisbury	The north-eastern side from Carey Street, for a distance of 100.58 metres in a south easterly direction	13 metres
Kesters Road, Para Hills West	Both sides from Main North Road to Bridge Road	14 metres
Martins Road	The whole length on both sides	8 metres
Maxwell Road	The whole length on both sides	8 metres
Milne Road	The whole length on both sides	8 metres
Nelson Road	The whole length on both sides	8 metres
Ponton Street, Salisbury	The north-eastern side from Commercial Road to Carey Street	11 metres
Pratt Avenue, Pooraka	The whole length of both sides	10.5 metres
Saints Road	The whole length of both sides	8 metres
Shepherdson Road, Parafield Gardens	The south-western side from the north-western boundary of Section 2249, Hundred of Yatala to Port Wakefield Road	12.5 metres
Shepherdson Road, Parafield Gardens	The whole length of the north-eastern side	12.5 metres

Salisbury Council  
Table Section  
Table Sal/1 - Building Setbacks from Road Boundaries

Road Name	Portion of Road Affected	Setback Distance
South Terrace, Salisbury	The north-eastern side commencing at a point 18.28 metres north-west of the north-western alignment of Hunt Street and continuing north-westward for a distance of 17.72 metres	14 metres
Spains Road, Salisbury, Salisbury Downs	The whole length on both sides between Salisbury Highway and the railway line	8 metres
Stanford Road, Salisbury Heights	The eastern side from Target Hill Road to a point 79.24 metres south-west of Taylor Avenue	10.5 metres
Stanford Road, Salisbury Heights	Both sides from Golden Grove Road to a point 79.24 metres south-west of Taylor Avenue	16.5 metres
Stanford Road, Salisbury Heights	The western side from Target Hill Road to a point 79.24 metres south-west of Taylor Avenue	18 metres
Thomas Street, Cavan	The whole length on both sides	9.5 metres
Whites Road, Salisbury North, between Waterloo Corner Road and Diment Road	The whole length on both sides	11 metres
Whites Road, Salisbury North, Paralowie, Parafield Gardens	Both sides from Kings Road to Waterloo Corner Road	14 metres
Whites Road, Parafield Gardens	Both sides from Cook Street to Shepherdson Road	8 metres
Winzor Street	The whole length on both sides	8 metres
Wright Street	The whole length on both sides	8 metres
Wynn Vale Drive	The whole length on both sides	8 metres

### Table Sal/2 - Off Street Vehicle Parking Requirements

The following vehicle parking requirements do not apply:

- (a) to the Mixed Use (Bulky Goods, Entertainment, Leisure) Zone except where the form of development is light industry whereby the rates for Industry, warehouse, stores are applicable
- (b) to development that is subject to the requirements in [Table Sal/2A - Off Street Vehicle Parking Requirements for Designated Areas](#).

Form of Development	Number of Required Car Parking Spaces
Accommodation	
Aged Care / retirement home	1 space per unit
Serviced apartment	1 space per unit plus 1 space per employee
Motel	1 space per unit
Commercial	
Bulky goods outlet	3 spaces per 100 square metres of gross leasable floor area
Cinema	1 space per 4 cinema seats
Hotel Public bar	1 space per 2 square meters of floor area available to the public
Lounge or beer garden	1 space per 6 square metres of floor area available to the public
Gaming room	1 space per 2 machines
Office	1 space per 25 square metres, with a minimum of 4 spaces per office
Restaurant	Greater of 1 space for every 3 seats or 1 space for every 15 square metres of dining area
Service trade premises	3 spaces per 100 square metres
Shop	7 spaces per 100 square metres of gross leasable area for shops outside of centre zones 5 spaces per 100 square metres of gross leasable area for shops within centre zones
Community/civic	
Child care centre	1 space per 4 children
Community centre	10 spaces per 100 square metres of total floor area
Library	4 spaces per 100 square metres
Place of worship	Greater of 1 space for every 3 seats or every 3 attendees
Dwellings	

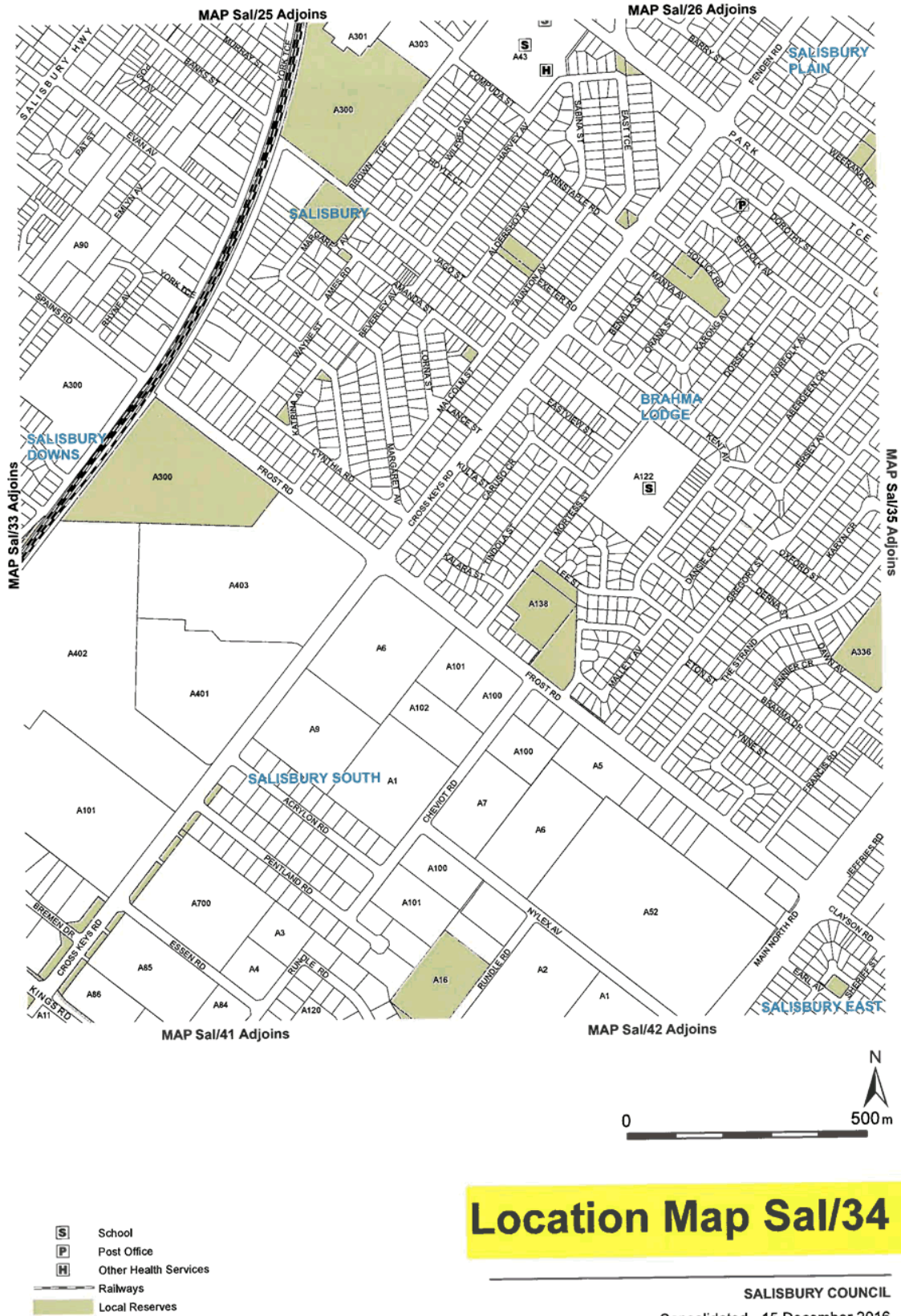
Salisbury Council  
Table Section  
Table Sal/2 - Off Street Vehicle Parking Requirements

Form of Development	Number of Required Car Parking Spaces
Detached dwelling Semi Detached Dwelling Row Dwelling	2 spaces per dwelling, one of which is to be covered
Residential flat building Multiple dwelling Group dwelling	1 space per dwelling, plus 0.5 on-site visitor car parking spaces per dwelling
Industry, warehouses, stores	
Office component	1 space per 30 square metres
Plus	Plus
Non-office component	
Up to 200 square metres	1 space per 50 square metres
Plus 200-2000 square metres	1 additional space for every 75 square metres
Plus greater than 2000 square metres	1 additional space for every 150 square metres
Or	Or
For labour intensive industries, inclusive of office component (whichever ever is greater)	0.75 car parking spaces per employee
Medical	
Consulting room	10 per 100 square metres of total floor area, with a minimum of 3 spaces per tenancy
Hospital	2.5 spaces per bed
Nursing home	1 space for every 4 beds
The following vehicle parking requirements apply to development specifically within the <b>Mixed Use (Bulky Goods, Entertainment and Leisure) Zone</b> :	
Form of Development	Minimum number of required vehicle parking spaces
All forms of development (except Light Industry)	3 spaces per 100 square metres of gross leasable floor area

Salisbury Council  
Table Section  
*Table Sal/3 - Off Street Bicycle Parking Requirements*

**Table Sal/3 - Off Street Bicycle Parking Requirements**

Form of development	Employee/resident (bicycle parking spaces)	Visitor/shopper (bicycle parking spaces)
Residential component of multi-storey building/residential flat building	1 for every 4 dwellings	1 for every 10 dwellings
Office	1 for every 200 square metres of gross leasable floor area	2 plus 1 per 1000 square metres of gross leasable floor area
Shop	1 for every 300 square metres of gross leasable floor area	1 for every 600 square metres of gross leasable floor area
Tourist accommodation	1 for every 20 employees	2 for the first 40 rooms plus 1 for every additional 40 rooms



Item 5.1.1 - Attachment 5 - Relevant Development Plan Provisions



**Overlay Map Sal/34**  
**TRANSPORT**

SALISBURY COUNCIL  
Consolidated - 15 December 2016





NOTE: Airport Building Heights should be read in conjunction with concept plan map showing heights for Edinburgh Defence Airfield

**Airport Building Heights**  
 Referral to Commonwealth Secretary  
 for Dept. of Transport and Regional Services  
 Zone A All Structures  
 Zone C All Structures Exceeding 15 metres above existing ground level



# Overlay Map Sal/34

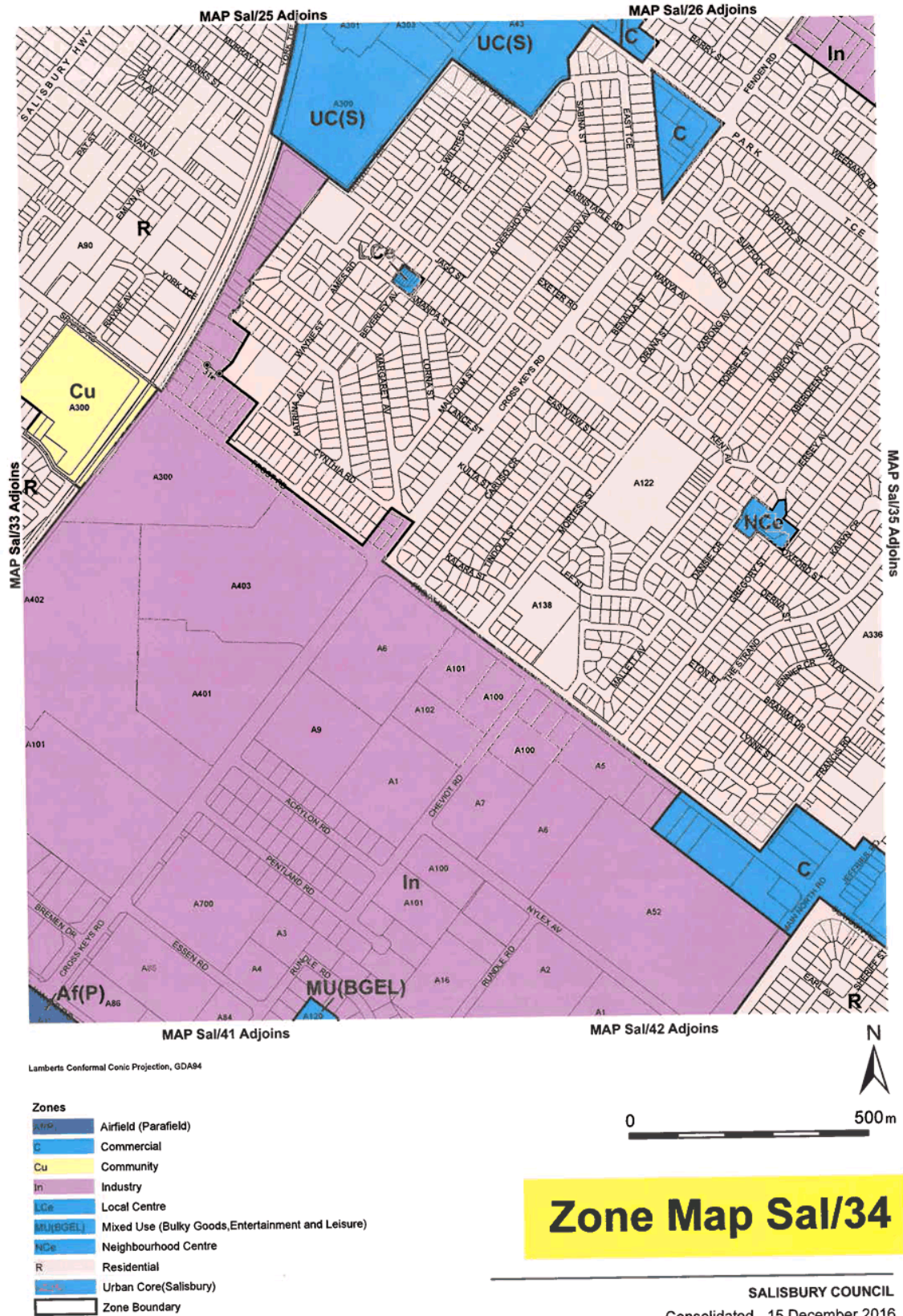
## DEVELOPMENT CONSTRAINTS

SALISBURY COUNCIL  
 Consolidated - 15 December 2016

Airport Building Heights

Item 5.1.1 - Attachment 5 - Relevant Development Plan Provisions





Item 5.1.1 - Attachment 5 - Relevant Development Plan Provisions



<b>ITEM</b>	5.1.2
	<b>COUNCIL ASSESSMENT PANEL</b>
<b>DATE</b>	4 December 2018
<b>APPLICATION NO.</b>	361/334/2018/3B
<b>APPLICANT</b>	Kilpatrick Architecture
<b>PROPOSAL</b>	Amendment to Development Approval 361/2876/2005/1B (Increase Student Number Cap From 335 to 440 Students) and New Classroom
<b>LOCATION</b>	346-354 Wright Road, Para Vista SA 5093 and 130-144 Nelson Road, Para Vista SA 5093
<b>CERTIFICATE OF TITLE</b>	Volume: 5224, Folio: 902; Volume: 5467, Folio: 729; Volume: 5256, Folio: 738; Volume: 5359, Folio: 835; Volume: 5157, Folio: 891; Volume: 5296, Folio: 388; Volume: 5191, Folio: 415; Volume: 5263, Folio: 300; Volume: 4282, Folio: 632.
<b>AUTHOR</b>	Matt Atkinson, Planning Consultant

## 1. DEVELOPMENT APPLICATION DETAILS

<b>Zone/Policy Area</b>	Residential Zone
<b>Application Type</b>	On Merit
<b>Public Notification</b>	Representations received: 6 Representations to be heard: 2
<b>Referrals - Statutory</b>	Nil
<b>Referrals – Internal</b>	Traffic Development Engineering
<b>Development Plan Version</b>	Salisbury (City) Development Plan Consolidated 15 December 2016
<b>Assessing Officer</b>	Matt Atkinson
<b>Recommendation</b>	Approval with Conditions
<b>Meeting Date</b>	4 December 2018

## 2. REPORT CONTENTS

### Assessment Report

Attachment 1:	Proposal Plans and Supporting Documentation
Attachment 2:	Notice of Category 3 Development, Copies of Representations, Applicant's Response, Notification of Amended Plan and Copy of Response from Representer
Attachment 3:	Council On-Street Car Parking Control Plan

- Attachment 4: Copies of Applicant's CIRQA Traffic and Parking Report and Supplementary Advice
- Attachment 5: Relevant Development Plan Provisions (Consolidated 15 December 2016)

### **3. EXECUTIVE SUMMARY**

This report provides a detailed assessment of the application against the relevant provisions of the Salisbury Development Plan. The assessment found that:

- a) The proposed land use (primary school) remains unchanged and the proposed development represents a reasonable expansion of an existing anticipated and approved land use.
- b) The proposed classroom building will complement other buildings within the school and will not have any unreasonable impact on adjacent property occupiers in terms of bulk, scale or appearance.
- c) Any additional on-site car parking demands associated with the proposed increase in student numbers from 335 to 440 persons, will be adequately provided for by the provision of 36 additional on-site car parking spaces (from 58 to 94 spaces).
- d) The previously approved (but yet to be constructed) set down/pick-up loop (DA 361/1433/2016/3B), when combined with the proposed amended design, will ensure that traffic will be better managed within the site, such that on-street traffic issues will be improved.
- e) The additional on-street car parking restrictions implemented in May 2017, in the form of a 'no-parking' zone and two 'bus zones' have provided further improvements to traffic management within the local area.

Accordingly, it is recommended that Development Plan Consent be granted, subject to conditions.

### **4. SUBJECT SITE**

The subject site is located at 350-354 Wright Road, Para Vista, which is comprised of nine (9) allotments. The site is currently occupied by the Prescott (Northern) Primary School.

The site has a primary frontage to Wright Road of approximately 160 metres, with a secondary frontage to Nelson Road of approximately 125 metres, and an overall site area of approximately 20,000 square metres.

The site is occupied by a range of school buildings and ancillary buildings. Existing buildings located within the school have a total floor area of 3,902 square metres.

The site is currently accessed via a two-way driveway crossover to/from Nelson Street. There are 58 car parking spaces on the land. Site photos are provided on the following page.

**Photo 1.**  
*Looking north-east on  
Wright Road towards  
front of the school  
and car parking bays*



**Photo 2.**  
*Looking east from  
Nelson Road into the  
site showing the  
church, access and  
car parking*



**Photo 3.**  
*Sealed driveway and  
car park accessed  
from Nelson Road*



**Photo 4.**  
*Looking east from the  
car park into the  
school grounds where  
the class room is to be  
constructed*





**Photo 5.**  
*Para Vista Uniting Church car park located to the west of the site providing overflow car parking for the school (if needed)*



## 5. LOCALITY

The subject land is located in the Residential Zone, as identified within the Council's Development Plan.

The locality is generally characterised by residential and community land uses. Residential development is predominantly in the form of single-storey detached dwellings at relatively low densities.

Community uses include two (2) schools (including the Prescott (Northern) Primary School on the subject land and Valley View Secondary School); and three (3) places of worship (including the Para Vista Seventh Day Adventist Church, the Para Vista Uniting Church and the Northside Baptist Church). Para Vista Primary School is located just outside the locality, approximately 350 metres north of the subject land.



Regular bus services frequent both Wright Road and Nelson Road. Nelson Road is identified within the Development Plan as secondary arterial road. Wright Road has a service road located adjacent to the subject land, which provides a higher than usual amount of on-street parking and a pick-up/drop-off area for the school.

Locality and contextual plans are provided below.

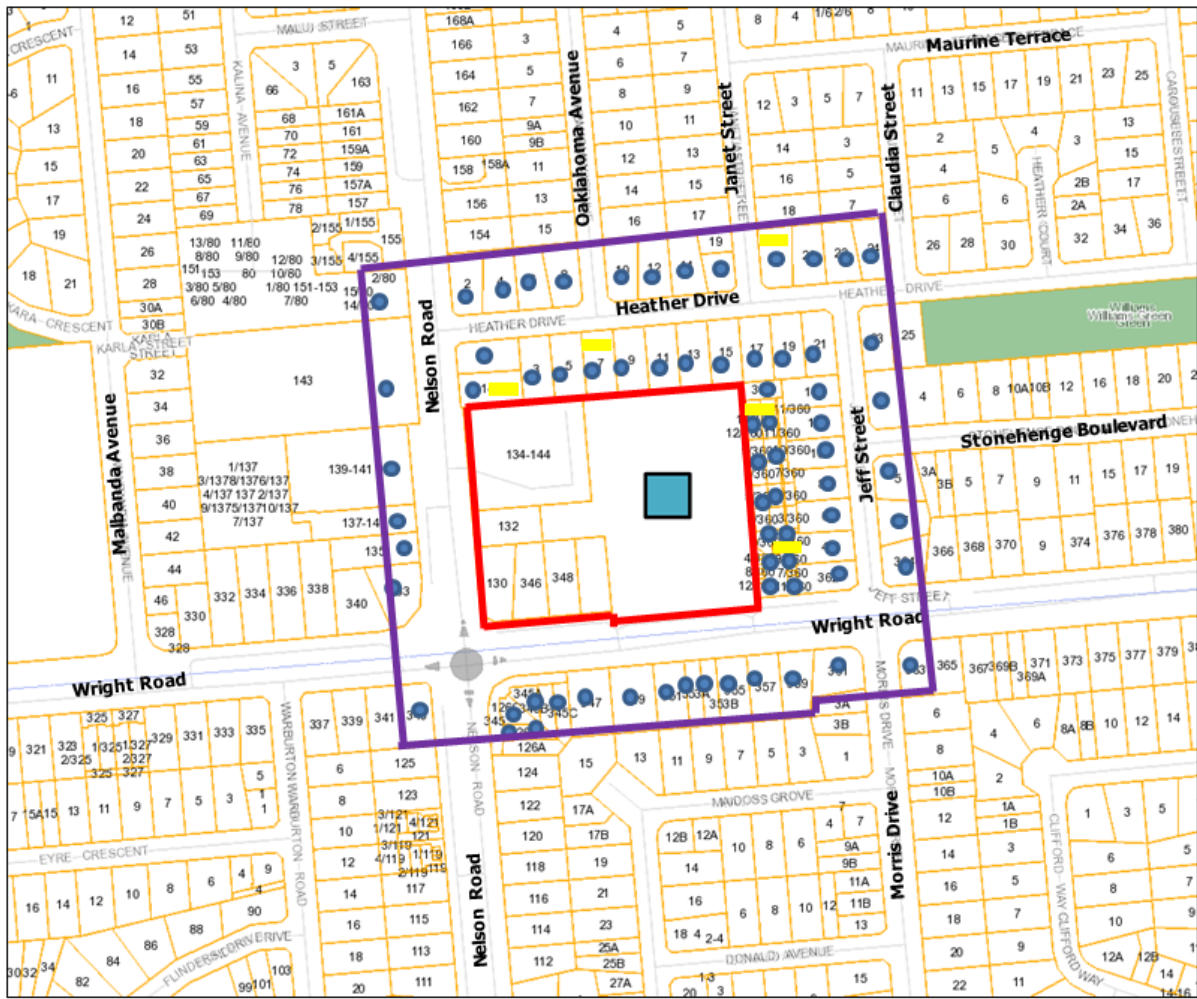
Locality Plan - Aerial






Item 5.1.2



Legend (Source: Nearmap)	
	Subject site
	Site boundary

Locality Plan – Cadastre





Legend (Source: Geocortex)	
	Subject site
	Site boundary
	Locality boundary
	Properties notified (occupiers within the subject site were also notified)
	Representations received

Contextual Plan

Item 5.1.2



Legend (Source: Nearmap)

	Subject site
	Zone

## **6. DESCRIPTION OF THE PROPOSED DEVELOPMENT**

The proposed development includes an increase in student numbers from 335 (which was a previous condition on DA 361/2876/2005/1B) to 440 and the construction of an additional single storey classroom building.

The proposed additional student numbers will be supported by the provision of a new car parking and set-down area linking to the existing Council service roadway off Wright Road, with modifications to existing access arrangements off Nelson Road, which was approved by Council on 25 September 2017 on a previous application (DA 361/1433/2016/3B).

This application seeks some minor alterations to the previously approved design to provide twenty-two (22) additional angled car parking spaces (in addition to fourteen (14) additional spaces proposed in the previously approved application). The proposal will therefore increase the provision of off-street car parking from 58 spaces to 94 spaces.

The proposed classroom building has a simple contemporary design with a skillion roof and cantilevered verandah canopy. The classroom building is proposed to be clad with cream and a contrasting dark red brick and lightweight profiled metal cladding, to complement existing buildings within the school.

The proposed classroom building will include provision for three (3) classrooms and a collaboration space. Some associated minor site works consisting of paving and retaining is also proposed.

A copy of the proposal plans and supporting documentation are contained in Attachment 1.

## **7. CLASSIFICATION**

The subject site is located in the Residential Zone, as identified within the Council's Development Plan (consolidated 15 December 2016). In this Zone, an Educational Establishment (which includes a primary school) is not listed as a complying or non-complying form of development and, as such, the application must be assessed as a merit form of development.

**8. PUBLIC NOTIFICATION**

The proposed development is neither listed as a Category 1 or 2 form of development within the Residential Zone or under Schedule 9 of the *Development Regulations 2008*.

Accordingly, the application must be advertised as a Category 3 form of development under Section 38 of the *Development Act 1993*.

Public notification took place between 4 May and 18 May 2018. The Council received six (6) representations during the notification period. Four (4) representations were in opposition and two (2) were in support of the proposal. The representors are listed in the table below.

<b>Representations received</b>		
<b>Representations received</b>		<b>Wish to be Heard</b>
1	Ms F Bedford MP JP State Member for Florey PO Box 143 MODBURY NORTH 5092	
2	I J & L Stradling 18 Heather Drive PARA VISTA 5093	
3	L Q Kho 3/360 Wright Road PARA VISTA 5093	
4	D & S T Bennett 4 Pitana Avenue PARA VISTA 5093	
5	S & S Mamo 146 Nelson Road PARA VISTA 5093	✓
6	T F & G A Fielder 7 Heather Drive PARA VISTA 5093	✓

A copy of the Category 3 public notice, submissions received, and the applicant's response are contained in Attachment 2. The content of the representation and the applicant's response are summarised in the table below:

<b>Summary of Representations</b>	
<b>Representation</b>	<b>Applicant's Response</b>
The proposed increase in student numbers to 550 will result in significant traffic impacts.	The proposed student numbers have been amended to 440, to reduce the associated traffic impacts.
Additional traffic measures should be put in place.	A drive-through set-down lane has been approved on a separate Application, which will significantly reduce on-street traffic and parking congestion.
Parents park within bus-zones on Nelson and Wright Roads.	* <i>See staff comment below.</i>
A formal agreement should be entered into between the Uniting Church and Prescott Northern School for the overflow car parking.	A written agreement has been provided by the Para Vista Uniting Church. A copy has been provided to Council.
Cars should not queue in Heather Drive and Lorraine Avenue. At present cars from Para Vista Primary School also use these roads.	** <i>See staff comment below.</i>
Do you believe that the analysis of the Traffic Engineers is correct that there would be a significant reduction in traffic congestion at the Nelson Road/Wright Road intersection? A Traffic Management Plan should be undertaken by the Applicant.	The impact of traffic on surrounding roads resulting from the proposed development has been thoroughly and professionally investigated by experts in the area of traffic management and engineering. The expert advice has concluded that the increased student numbers, while resulting in an increase in vehicle movements, will not significantly impact on surrounding roads when viewed in conjunction with the approved scheme.
Many cars have been noticed queuing on Montague Road before they collect students from Good Shepherd Lutheran School Para Vista. What analysis and traffic reports have they used?	** <i>See staff comment below.</i>
The development, if not researched correctly, could result in major traffic problems. Local residents would be affected by the increase in the impact of the proposed development of the school. Residents quality of life would be impacted by vehicles trying to park on the street and parents rushing to collect their children from school.	See comment above. In addition, the soon to be established (previously approved) drive-through set-down lane will bring benefits of improved traffic flow for the broader community and will not severely impact the operation of Nelson and Wright Roads.

*\* the Council implemented additional car parking controls and 'bus-zones' in May 2017, which should reduce the incidence of illegal parking.*

*\*\* the existing traffic conditions associated with other schools inside and outside the locality is not considered to be directly relevant to this proposal (there is an indirect link insofar as they contribute to the existing traffic conditions, however this proposal must be assessed on its merit within the context of the traffic impacts that are generated by the proposed additional student numbers only).*

As a result of the concerns raised during the public notification period, the applicant made some significant amendments to the proposal, including:

- The original proposal sought to increase the student number cap from 335 to 550 students. This was reduced to a cap of 440 students;
- The site plan was amended to show the on-site set-down/pick-up loop and car parking corridor granted planning consent on Development Application 361/1433/2016/3B. This corridor will link the Nelson Road car park entry with the Wright Road service road and provide 14 car parking spaces, in addition to the existing 53 spaces on the site and a further 22 spaces on the service road;
- An addition traffic analysis has been prepared by CIRQA. Their analysis concludes the on-site set-down/pick-up loop in addition to the additional car parking spaces will improve accommodation of parking demands and assist with easing congestion currently experienced on the adjacent road network; and
- An agreement has been entered into with the Para Vista Uniting Church to allow for overflow car parking for the school.

Given the nature of the changes and additional information provided, the amended plans and information was sent to the representors for their review on 16 October 2018, providing the representors until 31 October 2018 to provide comment on the amended details.

During this period, only one (1) representor elected to express some additional concerns, all of which related to the initial concerns summarised above.

## **9. REFERRALS – STATUTORY**

No statutory referrals were required pursuant to Schedule 8 of the *Development Regulations 2008*.



**10. REFERRALS – INTERNAL**

<b>Division</b>	<b>Summarised Comment</b>
<b>Traffic</b>	<p><i>Council's Traffic Engineer has reviewed the proposal and has concluded that an increase in student numbers to 440 would have negligible impact on existing traffic generation, given the previously approved 'kiss and drop' facility, the proposed additional car parking spaces and the agreement with the adjacent Church to utilise their car park as an overflow area.</i></p> <p><i>Whilst the proposed angled car parking spaces may result in some conflict with vehicle movements along the internal road, provided that they are constructed in accordance with the relevant Australian Standards (as proposed), they would be supported.</i></p> <p><i>In relation to on-street parking and traffic concerns raised by representors; and within correspondence received from Ms F Bedford MP JP on behalf of the residents; it is noted that additional parking controls were implemented along Nelson Street during May 2017, which included additional car parking controls and bus zones.</i></p> <p><i>A plan illustrating the 'no parking' and 'bus zones' that were implemented during May 2017 is provided in Attachment 3.</i></p>
<b>Development Engineering</b>	<p><i>Council's Development Engineers have reviewed the proposed Civil Plan and calculations and have no concerns with proposal to connect stormwater to the existing system. No conditions are recommended.</i></p>

**11. ASSESSMENT**

Pursuant to Section 35(2) of the *Development Act 1993*, it is recommended that the Panel determine that the proposed development is not seriously at variance with the Salisbury Development Plan, consolidated 15<sup>th</sup> December 2016, as the proposal constitutes a reasonable expansion of an existing use and the additional car parking and traffic demands associated with the proposal have been adequately addressed.

Assessment

Detailed assessment of the application has taken place against the relevant provisions of the Salisbury Development Plan and is described below under headings.

An extract of the relevant Development Plan, consolidated 15 December 2016, is contained in Attachment 5. The relevant provisions are also highlighted in the Attachment.

Zoning and Land Use

Principle of Development Control (PDC) 1 of the Residential Zone envisages educational establishments, including primary and secondary schools. PDC 1 states (in part):

- 1 *The following forms of development are envisaged in the zone:*
- *small scale non-residential use that serves the local community, for example:*
    - *child care facility*
    - *health and welfare service*
    - *open space*
    - *primary or secondary school*

The proposed development includes an expansion of an existing primary school, which is clearly a form of development that is anticipated by Residential Zone PDC 1. Whilst the proposed development includes an increase in student numbers from 335 to 440, the primary school is still considered to be relatively small scale, in accordance with Residential Zone PDC 1.

#### Design and Appearance

Principle of Development Control (PDC) 1 of the Design and Appearance module states:

- 1 *The design of a building may be of a contemporary nature and exhibit an innovative style provided the overall form is sympathetic to the scale of development in the locality and with the context of its setting with regard to shape, size, materials and colour.*

The proposed classroom building will complement other buildings within the school in terms of architectural design, colours and materials, in accordance with Design and Appearance module PDC 1.

The classroom building is proposed to be sited near the middle of the school, such that it will not have any adverse impact on adjacent properties in terms of its bulk and scale, visual outlook or overshadowing. The proposed building materials will not result in any unreasonable glare to neighbouring properties.

The proposed building, landscaping and paving will have a coordinated appearance with the existing school, which will maintain and enhance the visual attractiveness of the locality, consistent with Design and Appearance module PDC 11.

The proposed building design and appearance is therefore considered to be acceptable.

#### Land Use Conflict

Several of the representors have cited noise, lack of on-street car parking and traffic impacts as reasons that they do not support the proposal. Car parking and traffic impacts will be addressed under its own heading below. In relation to noise and other potential interface issues, the following provision of the Council's Development Plan is most relevant.

General Section: Interface Between Land Uses module PDC 1 states:

- 1 *Development should not detrimentally affect the amenity of the locality or cause unreasonable interference through any of the following:*
- (a) *the emission of effluent, odour, smoke, fumes, dust or other airborne pollutants;*
  - (b) *noise;*
  - (c) *vibration;*
  - (d) *electrical interference;*
  - (e) *light spill;*
  - (f) *glare;*
  - (g) *hours of operation;*
  - (h) *traffic impacts.*

The proposed hours of operation will remain unchanged and the additional student numbers are not likely to result in a perceivable increase in noise. No odours, smoke, fumes or dust will be created by the proposed development. The development will not result in any unreasonable light spill or glare. Car parking and traffic is addressed separately below.

#### Car Parking and Traffic

Several of the representors expressed concern with the lack of on-site car parking proposed and the resulting impact on the availability of on-street car parking within the locality. Representors have also expressed concern with the impact of additional student numbers on traffic within the local area.

The Salisbury Council Development Plan does not contain any car parking rates for educational establishments. As such, the Applicant was requested to provide a car parking and traffic report in support of the proposal. The Applicant engaged CIRQA Pty Ltd, an experienced Traffic Engineering firm, to prepare a car parking and traffic report for the proposal.

CIRQA provided an initial report on 7 June 2018 and supplementary information on 10 October 2018, following the amendments undertaken by the Applicant post the public notification period.

A summary of CIRQA's advice is set out below:

- the previously approved set-down/pick-up loop has been factored in to the calculations for car parking and traffic generation;
- the analysis indicates that the additional traffic generated by the student population increase to 440 students will be very low. The modelling indicates an additional 21 morning and 12 afternoon peak hour movements. These movements will be easily accommodated on the adjacent road network, particularly given the approved access and parking upgrades;
- the peak periods associated with the school occur for relatively short periods during both the morning and afternoon peak hours. Furthermore, the approved upgrades will improve traffic safety conditions associated with access to and from the school and will ameliorate the traffic impact of additional student numbers;

- the additional on-site car parking proposed accords with the Australian/New Zealand Standard for “Parking Facilities – Part 1: Off-street car parking (AS/NZS 2890.1:2004); and
- the additional car parking spaces proposed (post notification) will further improve on-site accommodation of parking demands associated with the school and will assist with easing congestion currently experienced on the adjacent road network (in addition to the improvements which will result from the provision of the set-down/pick-up loop).

Copies of CIRQA’s original report and the amended advice are contained in Attachment 4.

The Application, including the CIRQA advice, was referred to Councils Traffic Engineer for review. A summary of the Council’s Traffic Engineering advice is provided below:

- The proposed increase in student numbers to 440 would have negligible impact on existing traffic generation, factoring in the previously approved set-down/pick-up loop, the proposed additional on-site car parking spaces and the agreement with the adjacent Church to utilise their car park as an overflow area;
- Whilst the proposed angled car parking spaces may result in some conflict with vehicle movements along the internal road, provided that they are constructed in accordance with the relevant Australian Standards (as proposed), they would be supported;
- In relation to on-street parking and traffic concerns raised by representors; and within correspondence received from Ms F Bedford MP JP on behalf of the residents; it is noted that additional parking controls were implemented along Nelson Street during May 2017, which included ‘no-parking’ and ‘bus zones’;
- Since implementation, the additional on-street car parking controls have alleviated illegal parking concerns raised by the representors.

A plan illustrating the ‘no parking’ and ‘bus zones’ implemented during May 2017 is provided in Attachment 3.

The proposed additional student numbers will be supported by the provision of a new car parking and set-down area linking to the existing Council service roadway off Wright Road, with modifications to existing access arrangements off Nelson Road, which was approved by Council on 25 September 2017 on a previous application (DA 361/1433/2016/3B).

This application seeks some minor alterations to the previously approved design to provide twenty-two (22) additional angled car parking spaces (in addition to fourteen (14) additional spaces proposed in the previously approved application). The proposal will therefore result in an increase in the existing provision of off-street car parking from 58 spaces to 94 spaces.

The provision of on-site car parking is considered to be appropriate to the demands generated by the proposed increase in student numbers and will not cause any appreciable increase in demand for on-street car parking within the locality.

The on-street car parking controls implemented in May 2017, including a ‘no-parking’ zone and two (2) ‘bus zones’, have further improved traffic management within the local street network.

As such, the proposed development (when combined with the previous approved development for a set-down/pick-up loop, which has been reiterated with this proposal) and the recent on-street traffic management will result in significant improvements in traffic management within the local street network, despite the proposed increase in student numbers.

Given the context described above, the proposed development is considered to be acceptable from a traffic and car parking perspective.

## 12. CONCLUSION

The Applicant seeks Development Plan Consent for an increase in student numbers from 335 to 440 (which was a previous condition on DA 361/2876/2005/1B) and the construction of an additional single storey classroom building.

This report has provided a detailed assessment of the application against the relevant provisions of the Salisbury Development Plan. The assessment found that:

- a) The proposed land use (primary school) remains unchanged and the proposed development represents a reasonable expansion of an existing anticipated and approved land use.
- b) The proposed classroom building will complement other buildings within the school and will not have any unreasonable impact on adjacent property occupiers in terms of bulk, scale or appearance.
- c) Any additional on-site car parking demands associated with the proposed increase in student numbers from 335 to 440 persons, will be adequately provided for by the provision of 36 additional on-site car parking spaces (from 58 to 94 spaces).
- d) The previously approved (but yet to be constructed) set down/pick-up loop (DA 361/1433/2016/3B), when combined with the proposed amended design, will ensure that traffic will be better managed within the site, such that on-street traffic issues will be improved.
- e) The additional on-street car parking restrictions implemented in May 2017, in the form of a 'no-parking' zone and two 'bus zones' have provided further improvements to traffic management within the local area.

Accordingly, it is recommended that Development Plan Consent be granted, subject to conditions.

## 13. STAFF RECOMMENDATION

That the Development Assessment Panel resolve that:

- A. The proposed development is not considered to be seriously at variance with the Salisbury Development Plan – consolidated 15 December 2016.
- B. Pursuant to Section 33 of the *Development Act 1993*, Development Plan Consent is **GRANTED** to application number 361/334/2018/3B for an aAmendment to Development Approval 361/2876/2005/1B (Increase Student Number Cap From 335 to 440 Students) and New Classroom in accordance with the plans and details submitted with the application and subject to the following conditions:

Development Plan Consent Conditions

1. The development shall be carried out in accordance with the details submitted with the application and the following stamped approved plans and documents, except where otherwise varied by the conditions herein:

Drawing No.	Plan Type	Date	Prepared By
17101-SD01B	Site Plan	Received by Council dated 4 October 2018	Kilpatrick Architecture
17101-SD02	Demolition Plan	Received by Council dated 4 October 2018	Kilpatrick Architecture
17101-SD03	Floor Plan	Received by Council dated 4 October 2018	Kilpatrick Architecture
17101-SD04	Roof Plan	Received by Council dated 4 October 2018	Kilpatrick Architecture
17101-SD05	Sections	Received by Council dated 4 October 2018	Kilpatrick Architecture
17101-SD06	Elevations	Received by Council dated 4 October 2018	Kilpatrick Architecture
17101-SD08	Canopy Plan and Framing	Received by Council dated 4 October 2018	Kilpatrick Architecture
17101-SD20	Perspectives	Received by Council dated 4 October 2018	Kilpatrick Architecture

*Reason: To ensure the proposal is established in accordance with the submitted plans.*

2. The maximum number of enrolled students shall not exceed 440.

*Reason: To ensure that sufficient car parking is provided commensurate with the existing demand for the site.*

3. All waste and other rubbish shall be contained and stored pending removal in covered containers and waste containers must not be located within designated car parks or manoeuvring areas.

*Reason: To maintain the amenity of the locality.*

4. No materials, goods or containers shall be stored outside the building at any time.

*Reason: To ensure the car parking and emergency access areas are always available for the purpose they are designed. Further that the site be maintained in a clean and tidy state.*

Advice Notes

1. This is not a building consent, and a satisfactory application for Building Rules Consent must be submitted and approved before the Council can issue a Development Approval.

2. Advertisements and advertising displays are not included in the consent granted. It will be necessary to make a fresh and separate application for any future proposed advertising signage.
3. Except where otherwise varied by this Consent, the conditions imposed herein shall be in addition to conditions that apply to the subject property from previous approvals that remain active.

### **CO-ORDINATION**

Officer:	GMCiD	MDS
Date:	19.11.18	14.11.18

### **ATTACHMENTS**

This document should be read in conjunction with the following attachments:

1. Proposal Plans and Supporting Documentation
2. Notice of Category 3 Development, Copies of Representations, Applicant's Response, Notification of Amended Plan and Copy of Response from Representor
3. Council On-Street Car Parking Control Plan
4. Copies of Applicant's CIRQA Traffic and Parking Report and Supplementary Advice
5. Relevant Development Plan Provisions (Consolidated 15 December 2016)





# **Attachment 1**

## **Proposal Plans and Supporting Documentation**



**DEVELOPMENT APPLICATION FORM**

361/ 334 /2018/3 B

Please use BLOCK LETTERS and Black or Blue Ink

I wish to apply for (tick only one): <input type="checkbox"/> Development Plan Consent <input type="checkbox"/> Building Rules Consent	
<input type="checkbox"/> Full Development Approval (consists of both consents, which is required prior to any work commencing)	
APPLICANT: COMPANY / FIRST NAME Kilpatrick Architecture	SURNAME
POSTAL ADDRESS: 24 Divett Place, Adelaide	
OWNER NAME: (This must be completed) Australasian Conference Association Ltd.	EMAIL: <input type="checkbox"/> as above
OWNER POSTAL ADDRESS: PO Box 120 Prospect SA 5082	<input type="checkbox"/> as above
OWNER PHONE NO: 8269 2177	OWNER EMAIL: kaye.zydeveld@adventist.org.au
CONTACT PERSON FOR FURTHER INFORMATION <input type="checkbox"/> as above	
NAME: David Kilpatrick	TELEPHONE (W): (M): 0407 520 640
EMAIL: i@kilpatrick.net.au	Information from Council will be given by electronic communication to the nominated email address.
BUILDER NAME: TBA	BUILDERS EMAIL:
BUILDER POSTAL ADDRESS:	CONTACT NO.: LICENCE NO.:
CURRENT USE OF PROPERTY: Primary School	
DESCRIPTION OF PROPOSAL: New classroom building	DEVELOPMENT COST \$ 828,000+GST
LOCATION OF PROPOSAL	
Street No: 350-354 Street: Wright Road Suburb: Para Vista	
Lot No: 26 10 Section: Plan: D17760 F5 Volume: 5454 5157 Folio: 316 881	
OFFICE USE ONLY	
Registration Date: 28/2 /2018 Zone: R Ward: South	
BUILDING RULES CLASSIFICATION	
Classification sought:	
If Class 5, 6, 7, or 9 classification is sought, state the proposed number of employees	Male: Female:
If Class 9a classification is sought, state the number of persons for whom accommodation is provided:	
If Class 9b classification is sought, state the proposed number of occupants of the various spaces at the premises:	

I acknowledge that copies of this application and supporting documentation may be provided to interested persons in accordance with the Development Regulations, 1993. Developments requiring public notification will be made available to the public for comment via Council's web site at [www.salisbury.sa.gov.au](http://www.salisbury.sa.gov.au)

SIGNATURE:

DATE: 27/2/18

Item 5.1.2 - Attachment 1 - Proposal Plans and Supporting Documentation

**FORM OF DECLARATION  
BUILDING SAFELY NEAR POWERLINES**  
(Pursuant to Schedule 5 Clauses 2A of the Development Act 2008)

**TO** City Of Salisbury  
Development Services – Lodgement Officer  
PO Box 8  
SALISBURY SA 5108

**FROM** Name Kilpatrick Architecture  
Address 24 Divett Place  
Adelaide SA 5000  
Phone No 8396 2577 Mobile \_\_\_\_\_

**DATE OF APPLICATION** 27/2/18

**LOCATION OF PROPOSED DEVELOPMENT**

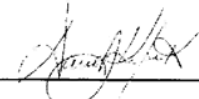
House No \_\_\_\_\_ Lot No \_\_\_\_\_ Street 350-354 Wright Rd,  
Town/Suburb Para Vista  
Section No (full/part) \_\_\_\_\_ Hundred \_\_\_\_\_  
Volume \_\_\_\_\_ Folio \_\_\_\_\_

**NATURE OF PROPOSED DEVELOPMENT**

New classroom building  
\_\_\_\_\_  
\_\_\_\_\_

**I** *(insert full name)* David Kilpatrick

being the applicant / a person acting on behalf of the applicant (delete the inapplicable statement) for the development described above declare that the proposed development will involve the construction of a building which would, if constructed in accordance with the plans submitted, not be contrary to the regulations prescribed for the purposes of section 86 of the Electricity Act 1996. I make this declaration under Clause 2A (1) of Schedule 5 of the Development Regulations 2008.

**SIGNED**  **DATE** 27/2/18

**PLEASE NOTE THAT THE FORM CONTINUES OVER**

Item 5.1.2 - Attachment 1 - Proposal Plans and Supporting Documentation

**FORM OF DECLARATION  
BUILDING SAFELY NEAR POWERLINES**  
(Pursuant to Schedule 5 Clauses 2A of the Development Act 2008)

**Note 1**

This declaration is only relevant to those development applications seeking authorisation for a form of development that involves the construction of a building (there is a definition of 'building' contained in Section 4 (1) of *the Development Act 1993*), other than where the development is limited to -

- a) an internal alteration of a building; or
- b) an alteration to the walls of a building but not so as to alter the shape of the building.

**Note 2**

The requirements of section 86 of the *Electricity Act 1996* do not apply in relation to:

- a) an aerial line and a fence, sign or notice that is less than 2.0m in height and is not designed for a person to stand on; or
- b) a service line installed specifically to supply electricity to the building or structure by the operator of the transmission or distribution network from which the electricity is being supplied.

**Note 3**

Section 86 of the *Electricity Act 1996* refers to the erection of buildings in proximity to powerlines. The regulations under this Act prescribe minimum safe clearance distances that must be complied with.

**Note 4**

The majority of applications will not have any powerline issues, as normal residential setbacks often cause the building to comply with the prescribed powerline clearance distances.

Buildings/renovations located far away from powerlines, for example towards the back of properties, will usually also comply.

Particular care needs to be taken where high voltage powerlines exist or where the development:

- is on a major road;
- commercial / industrial in nature; or
- built to the property boundary.

**Note 5**

An information brochure 'Building Safely Near Powerlines' has been prepared by the Technical Regulator to assist applicants and other interested persons.

This brochure is available from council and the Office of the Technical Regulator. The brochure and other relevant information can also be found at [sa.gov.au/energy/powerlinesafety](http://sa.gov.au/energy/powerlinesafety)

**Note 6**

In cases where applicants have obtained a written approval from the Technical Regulator to build the development specified above in its current form within the prescribed clearance distances, the applicant is able to sign the form.

WARNING: IN DEALING WITH THIS LAND, SEARCH THE CURRI CERTIFICATE

**CERTIFICATE OF TITLE**  
REAL PROPERTY ACT, 1986




VOLUME 5224 FOLIO 902

Edition 4  
Date Of Issue 04/11/1994  
Authority CONVERTED TITLE

**South Australia**

I certify that the registered proprietor is the proprietor of an estate in fee simple (or such other estate or interest as is set forth) in the land within described subject to such encumbrances, liens or other interests set forth in the schedule of endorsements.

  
  
**REGISTRAR-GENERAL**

**REGISTERED PROPRIETOR IN FEE SIMPLE**

AUSTRALASIAN CONFERENCE ASSOCIATION LTD. OF PO BOX 120 PROSPECT SA 5082

**DESCRIPTION OF LAND**

ALLOTMENT 7 FILED PLAN 5  
IN THE AREA NAMED PARA VISTA  
HUNDRED OF YATALA

**EASEMENTS**

NIL

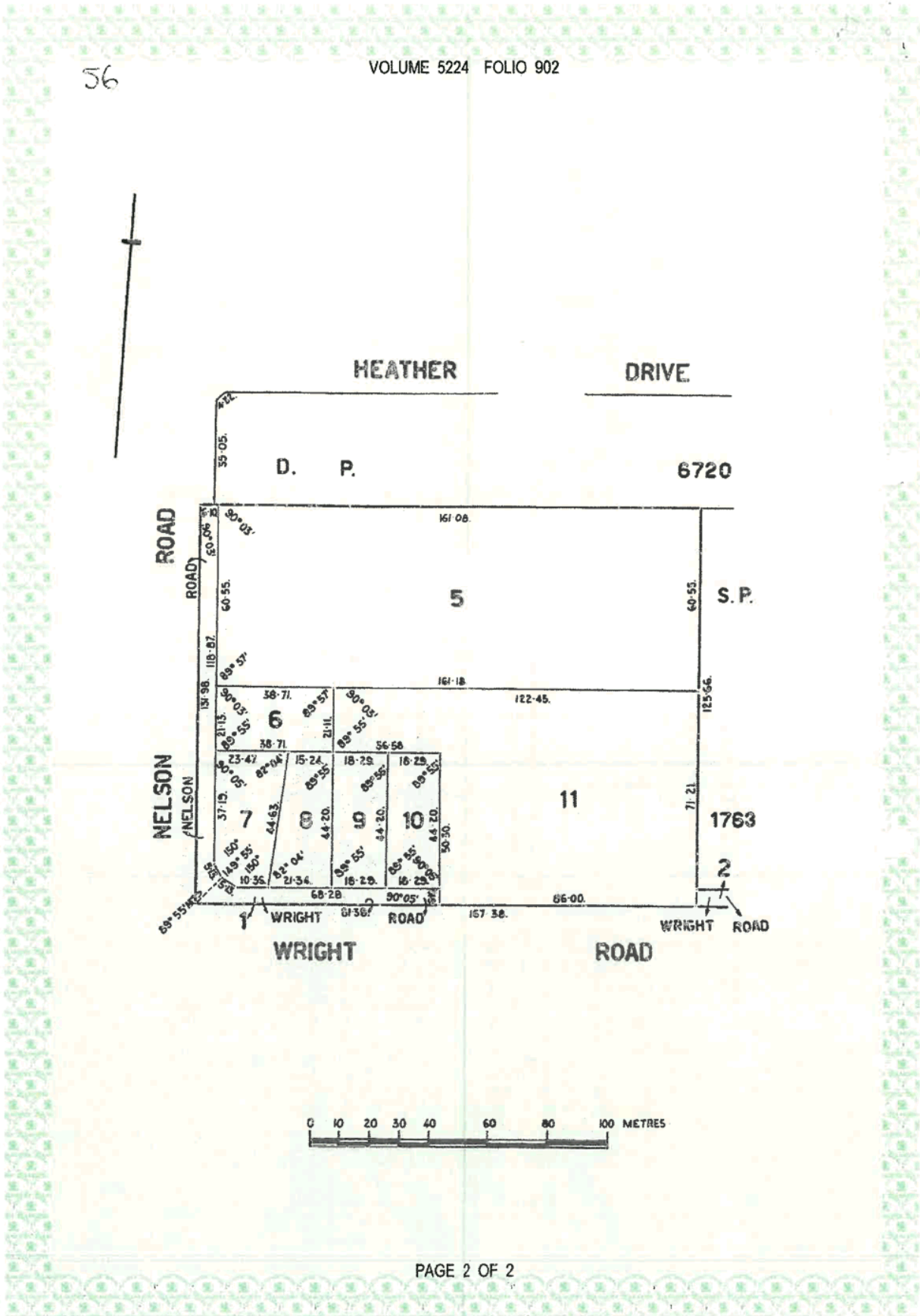
**SCHEDULE OF ENDORSEMENTS**

NIL


PAGE 1 OF 2

End of Text.

Item 5.1.2 - Attachment 1 - Proposal Plans and Supporting Documentation



**CERTIFICATE OF TITLE**  
REAL PROPERTY ACT, 1885

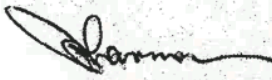



**South Australia**

VOLUME 5467 FOLIO 729  
Edition 2  
Date Of Issue 06/11/1997  
Authority CONVERTED TITLE

COPY

I certify that the registered proprietor is the proprietor of an estate in fee simple (or such other estate or interest as is set forth) in the land within described subject to such encumbrances, liens or other interests set forth in the schedule of endorsements.

  
**REGISTRAR-GENERAL** 

**REGISTERED PROPRIETOR IN FEE SIMPLE**

AUSTRALASIAN CONFERENCE ASSOCIATION LTD. OF 148 FOX VALLEY ROAD WAHROONGA NSW 2076

**DESCRIPTION OF LAND**

ALLOTMENT 6 FILED PLAN 5  
IN THE AREA NAMED PARA VISTA  
HUNDRED OF YATALA

**EASEMENTS**

NIL


**SCHEDULE OF ENDORSEMENTS**

NIL

PAGE 1 OF 2 End of Text

Item 5.1.2 - Attachment 1 - Proposal Plans and Supporting Documentation

**DUPLICATE**      **South Australia**      Register Book,  
**CERTIFICATE OF TITLE**      Volume 4282 Folio 631




New Certificate for portion of the Land in Vol.4069 Folio 463

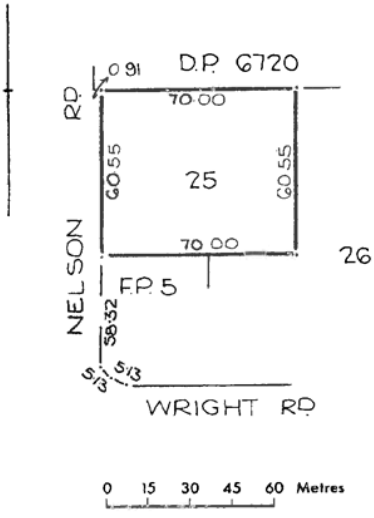
AUSTRALASIAN CONFERENCE ASSOCIATION LIMITED of 148 Fox Valley Road Wahroonga in the State of New South Wales 2076 is the proprietor of an estate in fee simple subject nevertheless to such encumbrances liens and interests as are notified by memorial underwritten or endorsed hereon in ALLOTMENT 25 of portion of Section 1570 HUNDRED OF YATALA in the area named PARA VISTA (L.T.R.O. DEPOSITED PLAN No.17760) and delineated on the plan hereon by bold black lines

In witness whereof I have signed my name and affixed my seal this *9<sup>th</sup>* day of October 1986

Signed the *9<sup>th</sup>* day of October 1986, in the presence of *K. Clemente* }

*[Signature]*  
Deputy Registrar-General







# CERTIFICATE OF TITLE

REAL PROPERTY ACT, 1936



South Australia

VOLUME 5256 FOLIO 738

Edition 4  
Date Of Issue 23/03/1995  
Authority CONVERTED TITLE

I certify that the registered proprietor is the proprietor of an estate in fee simple (or such other estate or interest as is set forth) in the land within described subject to such encumbrances, liens or other interests set forth in the schedule of endorsements.

*B PI*



ACTING REGISTRAR-GENERAL

WARNING: ONE DEALING WITH THIS LAND, SEARCH THE CURRENT CERTIFICATE

**REGISTERED PROPRIETOR IN FEE SIMPLE**

AUSTRALASIAN CONFERENCE ASSOCIATION LTD. OF 148 FOX VALLEY ROAD WAHROONGA  
NSW 2076

**DESCRIPTION OF LAND**

ALLOTMENT 8 FILED PLAN 5  
IN THE AREA NAMED PARA VISTA  
HUNDRED OF YATALA

**EASEMENTS**

NIL

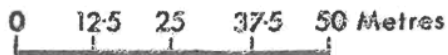
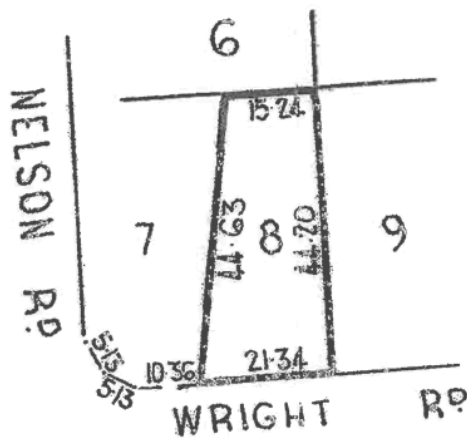
**SCHEDULE OF ENDORSEMENTS**

NIL

346

VOLUME 5256 FOLIO 738

RLM 1P



# CERTIFICATE OF TITLE

REAL PROPERTY ACT, 1896



VOLUME 5359 FOLIO 835

Edition 4  
Date Of Issue 05/09/1996  
Authority CONVERTED TITLE

South Australia

I certify that the registered proprietor is the proprietor of an estate in fee simple (or such other estate or interest as is set forth) in the land within described subject to such encumbrances, liens or other interests set forth in the schedule of endorsements.

*B A*  
ACTING REGISTRAR-GENERAL



WARNING: ONE DEALING WITH THIS LAND, SEARCH THE CURTAIN CERTIFICATE

REGISTERED PROPRIETOR IN FEE SIMPLE

AUSTRALASIAN CONFERENCE ASSOCIATION LTD. OF 148 FOX VALLEY ROAD WAHROONGA  
NSW 2076

DESCRIPTION OF LAND

ALLOTMENT 9 FILED PLAN 5  
IN THE AREA NAMED PARA VISTA  
HUNDRED OF YATALA

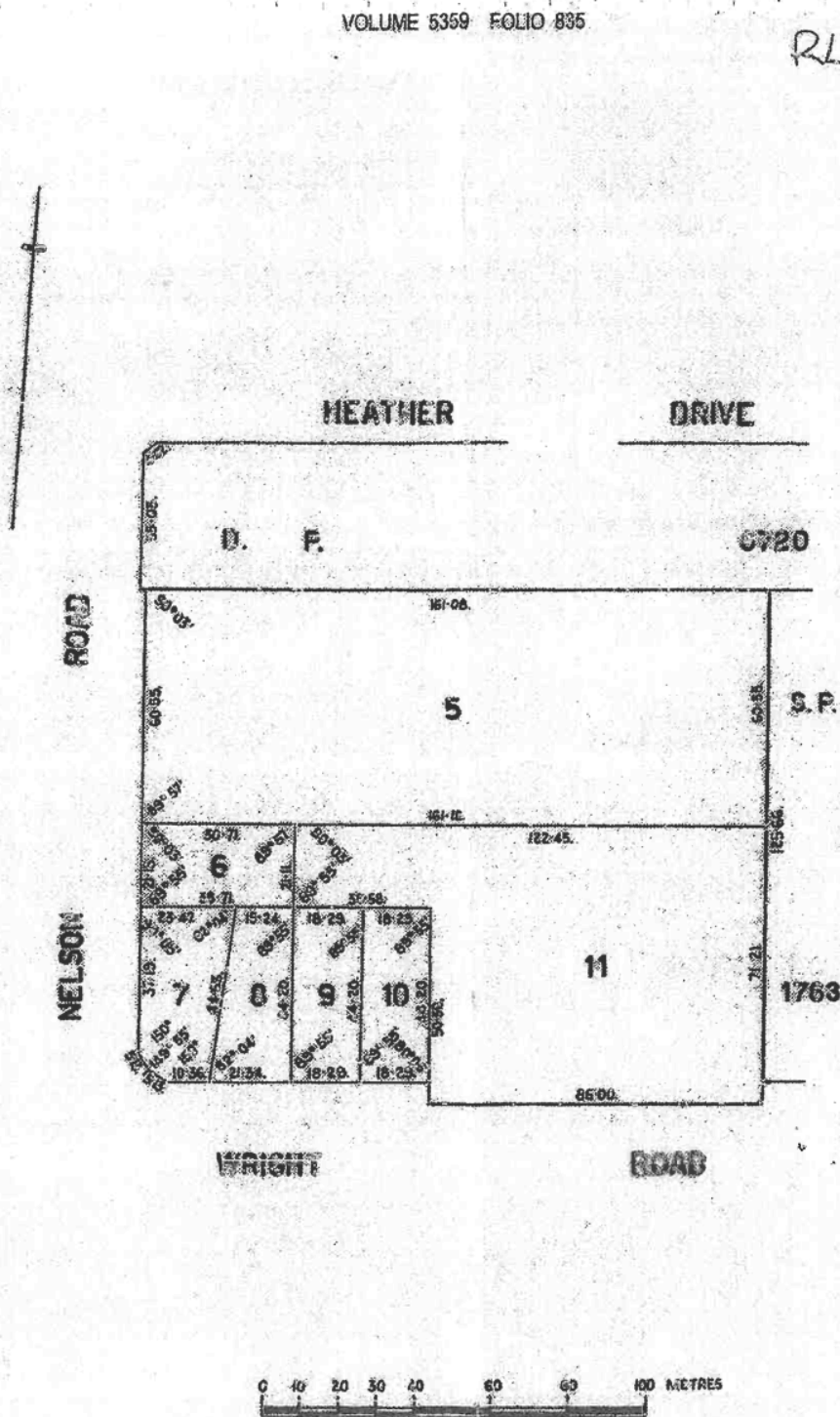
EASEMENTS

NIL

SCHEDULE OF ENDORSEMENTS

NIL

348



**CERTIFICATE OF TITLE**  
REAL PROPERTY ACT, 1886

**COPY**



VOLUME 5157 FOLIO 881

Edition 6  
Date Of Issue 30/11/1993  
Authority CONVERTED TITLE

**South Australia**

I certify that the registered proprietor is the proprietor of an estate in fee simple (or such other estate or interest as is set forth) in the land within described subject to such encumbrances, liens or other interests set forth in the schedule of endorsements.

*B. P.*



ACTING REGISTRAR-GENERAL

WARNING: BEFORE DEALING WITH THIS LAND, SEARCH THE CURRENT CERTIFICATE

REGISTERED PROPRIETOR IN FEE SIMPLE

AUSTRALASIAN CONFERENCE ASSOCIATION LTD. OF 148 FOX VALLEY ROAD WAHROONGA  
NSW 2076

DESCRIPTION OF LAND

ALLOTMENT 10 FILED PLAN 5  
IN THE AREA NAMED PARA VISTA  
HUNDRED OF YATALA

EASEMENTS

NIL

SCHEDULE OF ENDORSEMENTS

NIL

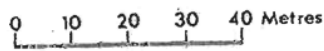
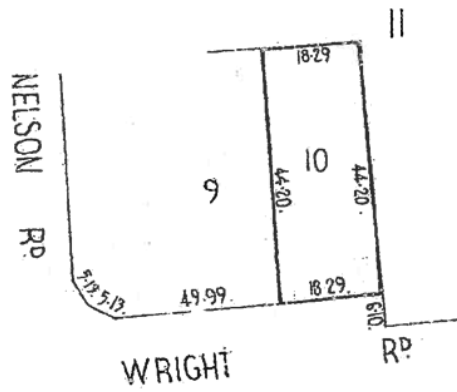
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PAGE 1 OF 2

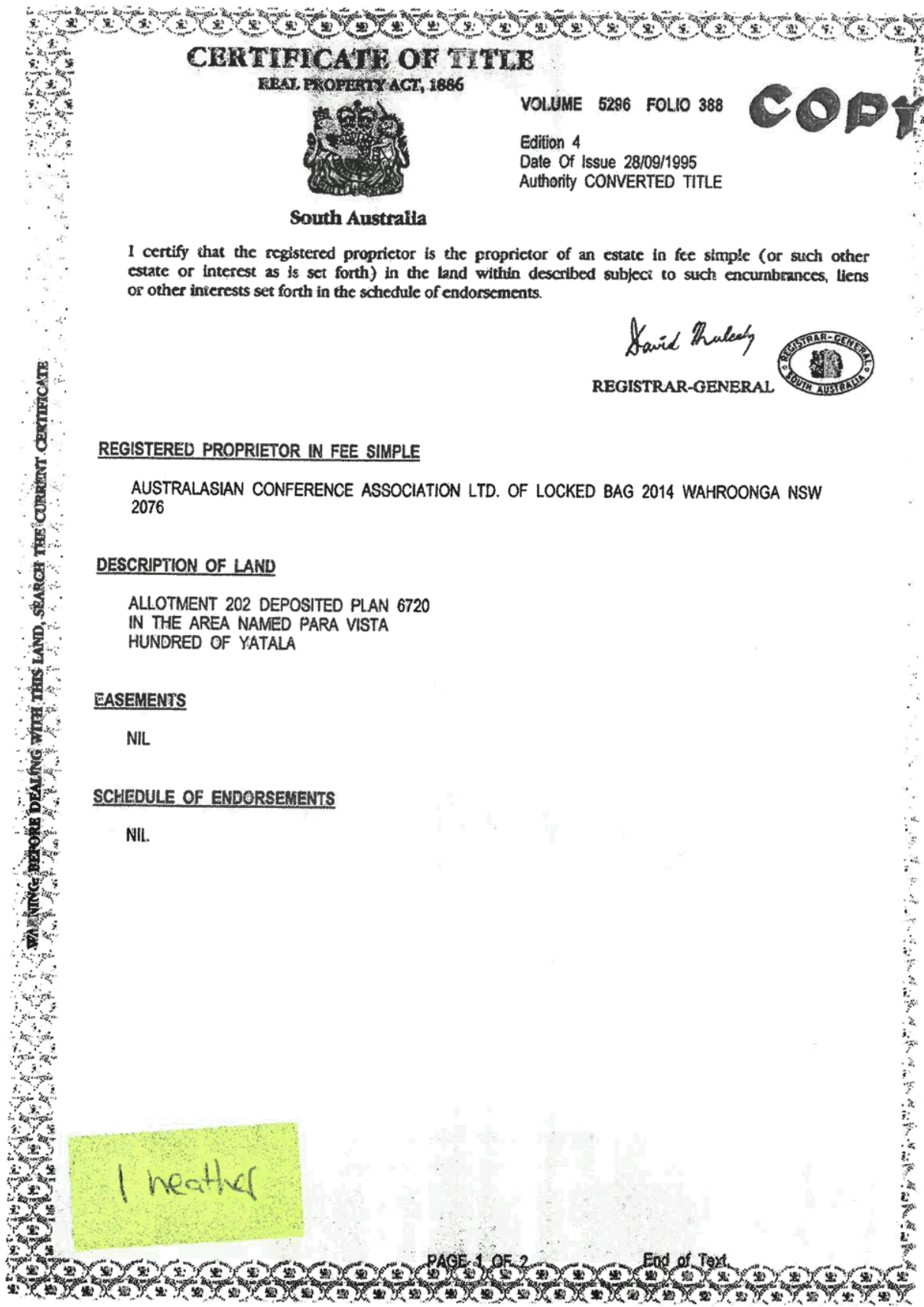
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VOLUME 5157 FOLIO 881

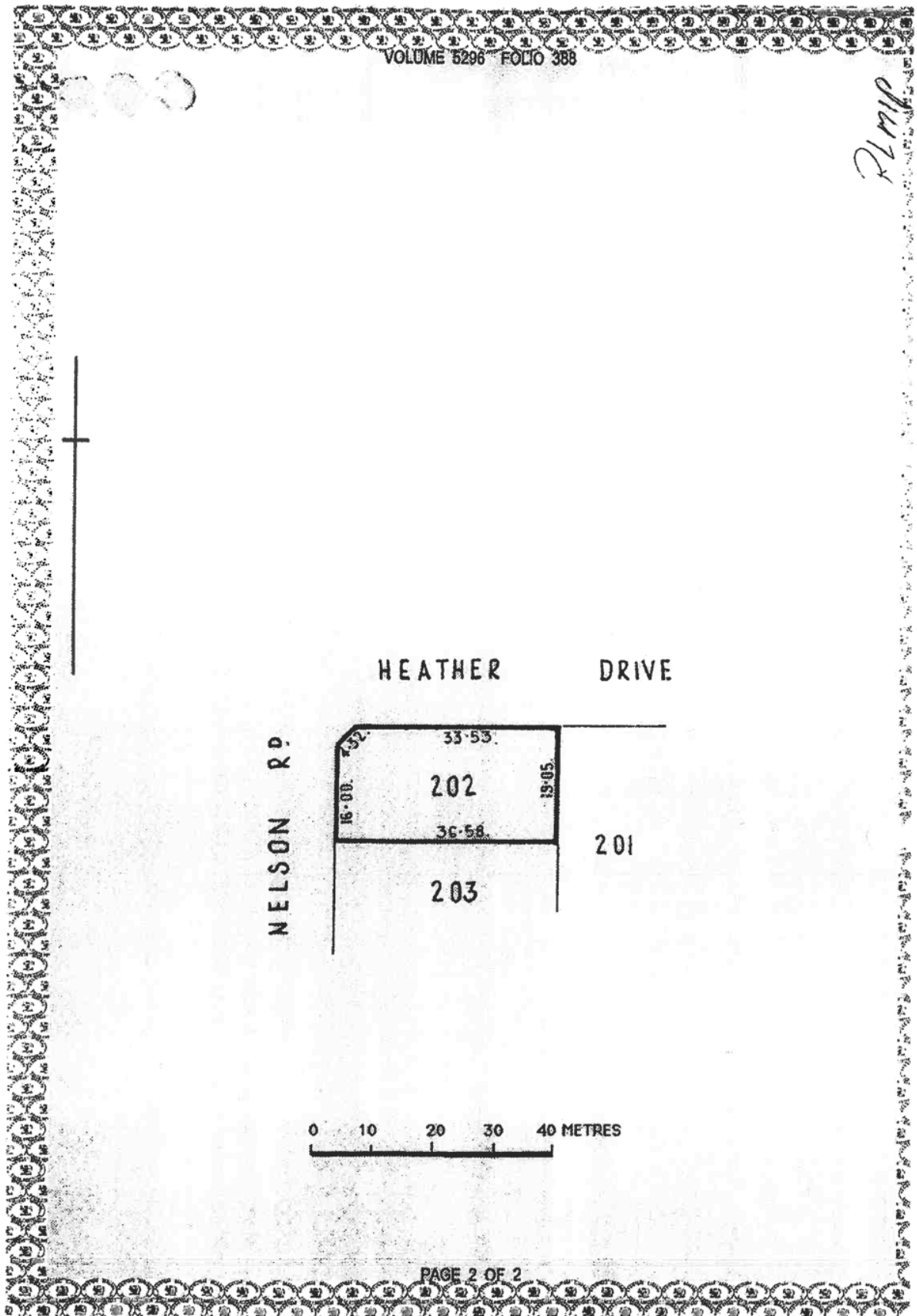
482



PAGE 2 OF 2




Item 5.1.2 - Attachment 1 - Proposal Plans and Supporting Documentation





WARNING - BEER OF DEALING WITH THIS LAND. SEARCH THE CURRENT CERTIFICATE

**CERTIFICATE OF TITLE**  
REAL PROPERTY ACT, 1866





**South Australia**

VOLUME 5191 FOLIO 415

Edition 8  
Date Of Issue 02/06/1994  
Authority CONVERTED TITLE

I certify that the registered proprietor is the proprietor of an estate in fee simple (or such other estate or interest as is set forth) in the land within described subject to such encumbrances, liens or other interests set forth in the schedule of endorsements.

**REGISTRAR-GENERAL**

**REGISTERED PROPRIETOR IN FEE SIMPLE**

AUSTRALASIAN CONFERENCE ASSOCIATION LTD. OF PO BOX 120 PROSPECT SA 5082

**DESCRIPTION OF LAND**

ALLOTMENT 201 DEPOSITED PLAN 6720  
IN THE AREA NAMED PARA VISTA  
HUNDRED OF YATALA

**EASEMENTS**

NIL

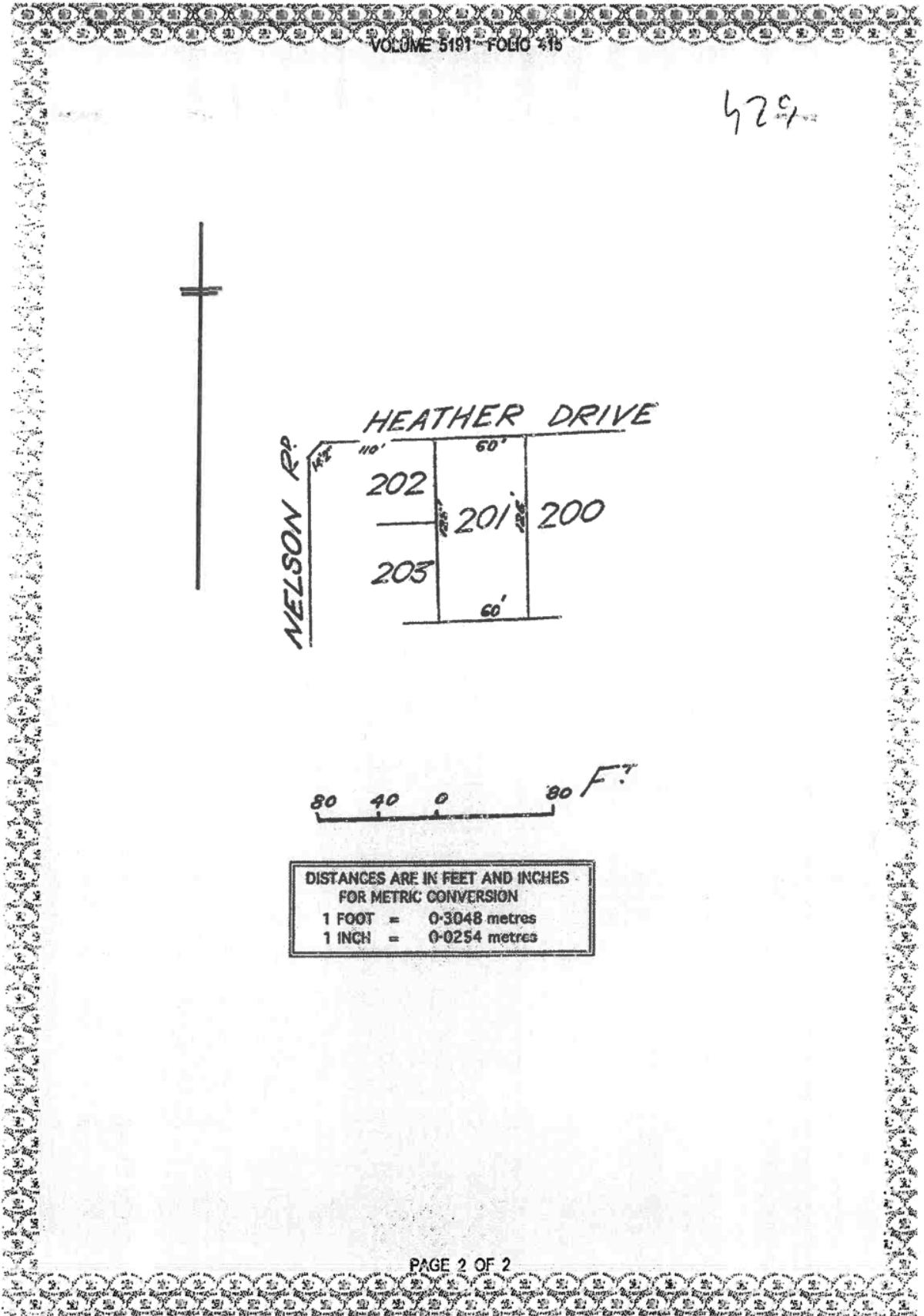
**SCHEDULE OF ENDORSEMENTS**

NIL

3 Heather

PAGE 1 OF 2      End of Text

Item 5.1.2 - Attachment 1 - Proposal Plans and Supporting Documentation



# CERTIFICATE OF TITLE

REAL PROPERTY ACT, 1886



South Australia

VOLUME 5263 FOLIO 300

Edition 4  
Date Of Issue 26/04/1995  
Authority CONVERTED TITLE

I certify that the registered proprietor is the proprietor of an estate in fee simple (or such other estate or interest as is set forth) in the land within described subject to such encumbrances, liens or other interests set forth in the schedule of endorsements.



DEPUTY REGISTRAR-GENERAL

WARNING: SEE 3 DRAWING WITH THIS LAND, SEARCH THE CURRENT CERTIFICATE

**REGISTERED PROPRIETOR IN FEE SIMPLE**

AUSTRALASIAN CONFERENCE ASSOCIATION LTD. OF LOCKED BAG 2014 WAHROONGA NSW 2076

**DESCRIPTION OF LAND**

ALLOTMENT 200 DEPOSITED PLAN 6720  
IN THE AREA NAMED PARA VISTA  
HUNDRED OF YATALA

**EASEMENTS**

NIL

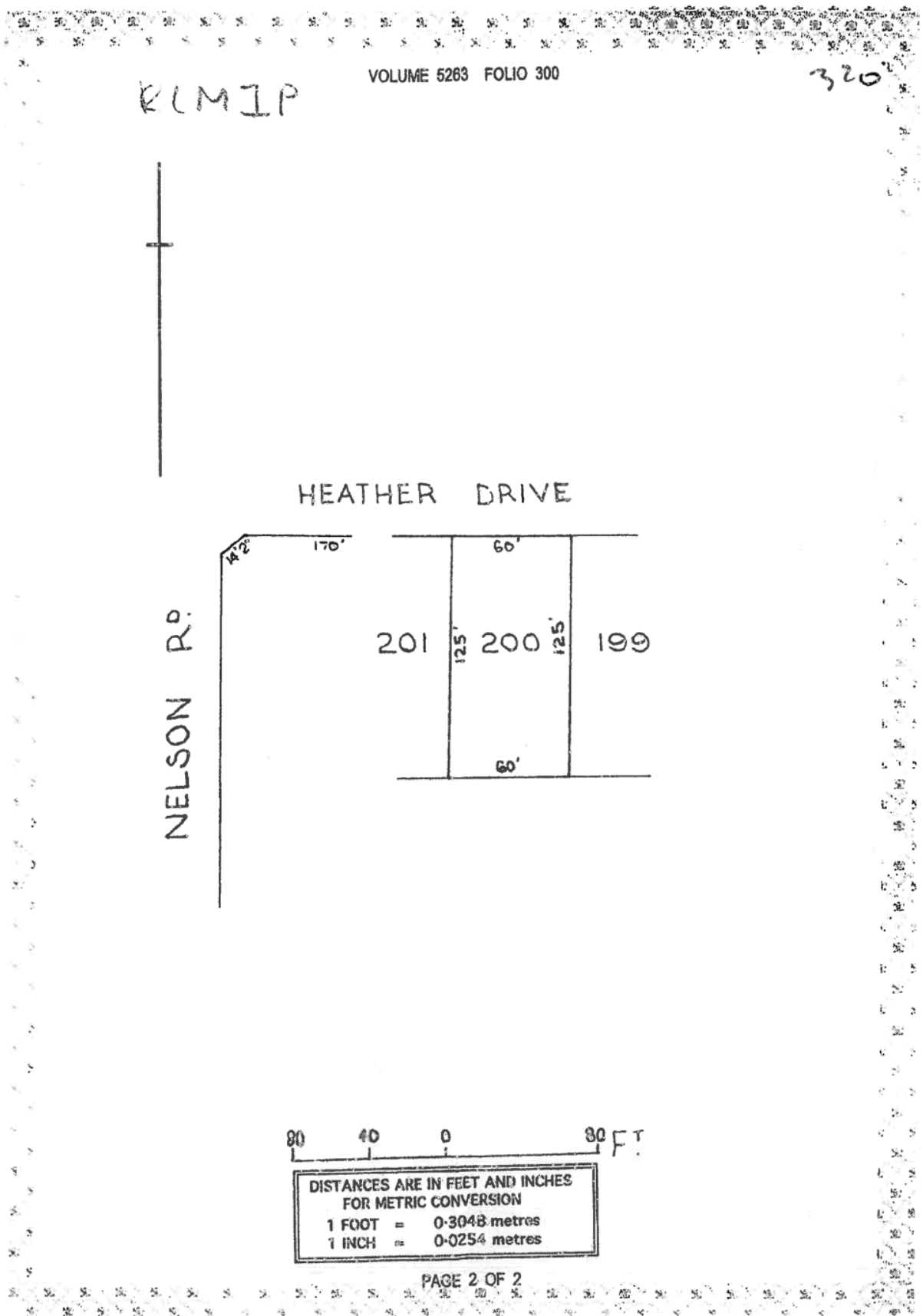
**SCHEDULE OF ENDORSEMENTS**

NIL


5 Heather DRIVE  
PARA VISTA  
School House  
Adjacent to  
CARPARK

PAGE 1 OF 2

End of Text.





**DUPLICATE** *South Australia* Register Book,  
**CERTIFICATE OF TITLE** Volume 4282 Folio 632

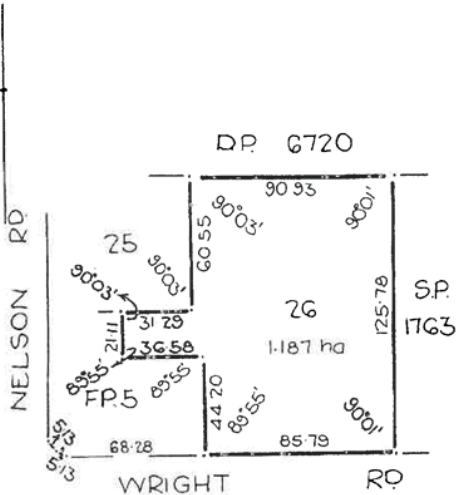


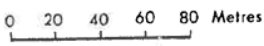
New Certificate for portion of the Land in Vol.3188 Folio 40 and  
the balance of the Land in Vol.4069 Folio 463

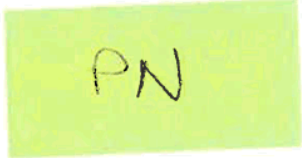
AUSTRALASIAN CONFERENCE ASSOCIATION LIMITED of 148 Fox Valley Road Wahroonga in the  
State of New South Wales 2076 is the proprietor of an estate in fee simple subject nevertheless to  
such encumbrances liens and interests as are notified by memorial underwritten or endorsed hereon  
in ALLOTMENT 26 of portion of Section 1570 HUNDRED OF YATALA in the area named PARA VISTA  
(L.T.R.O. DEPOSITED PLAN No.17760) and delineated on the plan hereon by bold black lines  
In witness whereof I have signed my name and affixed my seal this 9<sup>th</sup> day of October 1986  
Signed the 9<sup>th</sup> day of October  
1986, in the presence of *H. Clemente* }

  
Deputy Registrar-General



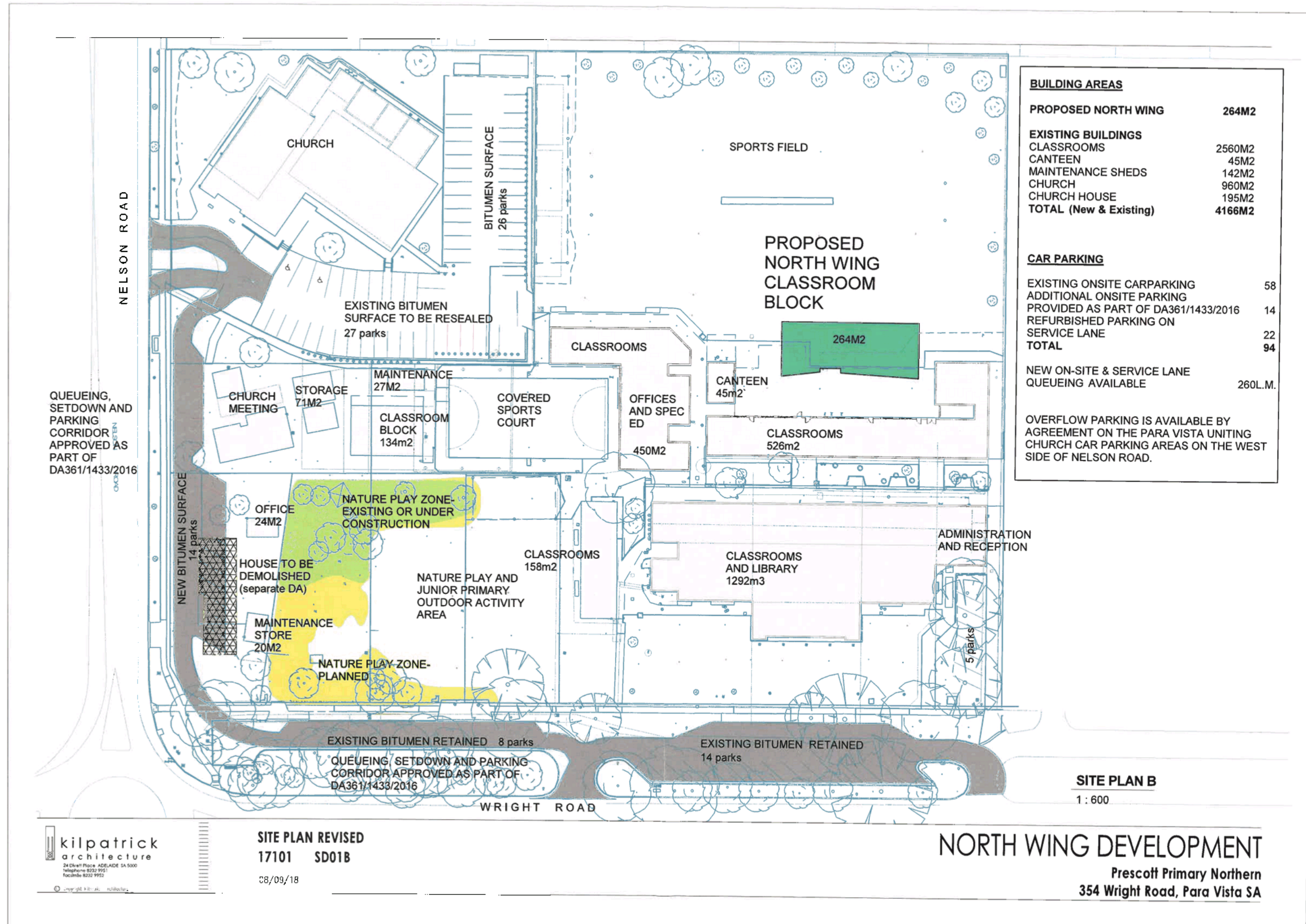






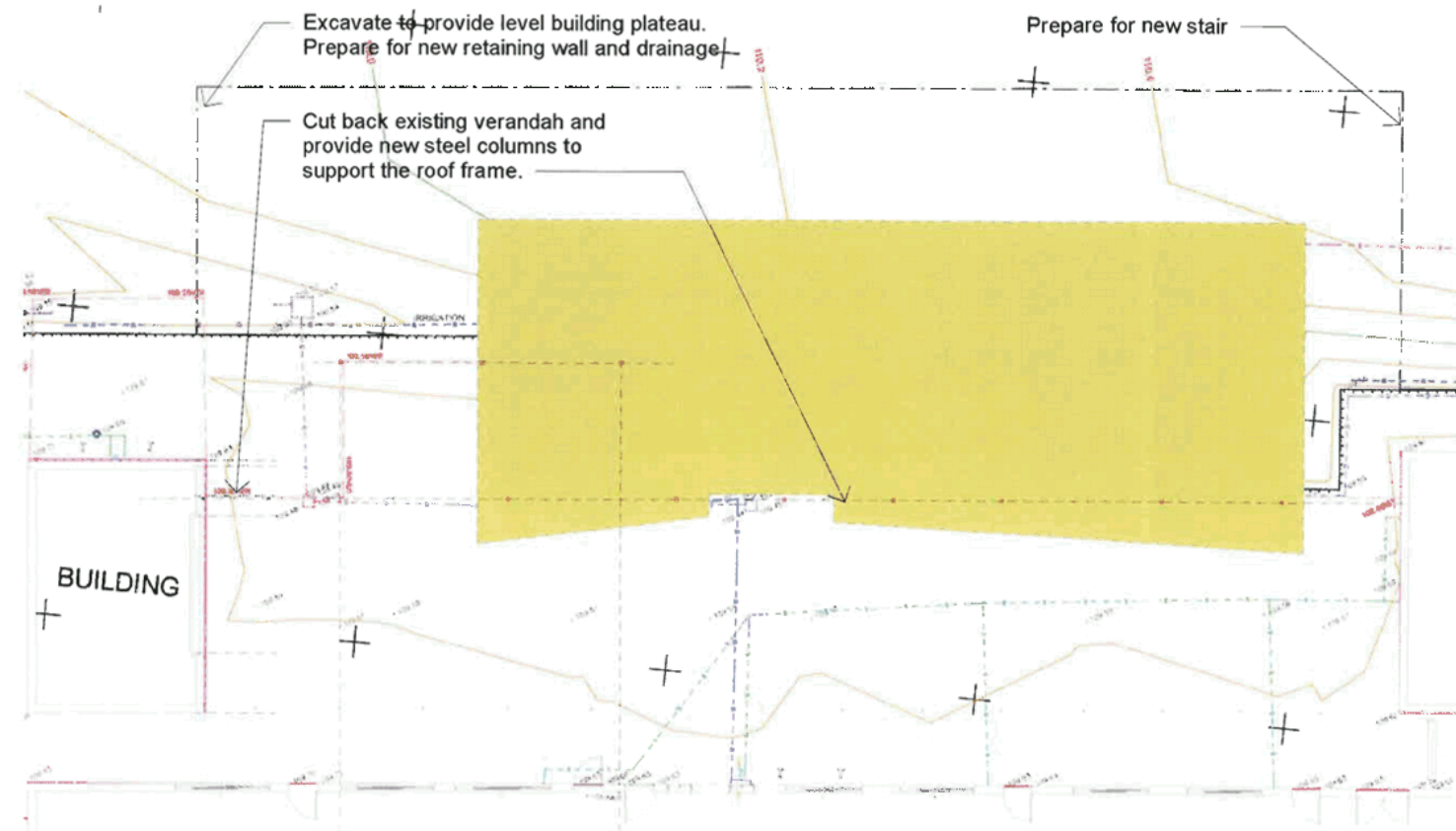
Item 5.1.2 - Attachment 1 - Proposal Plans and Supporting Documentation



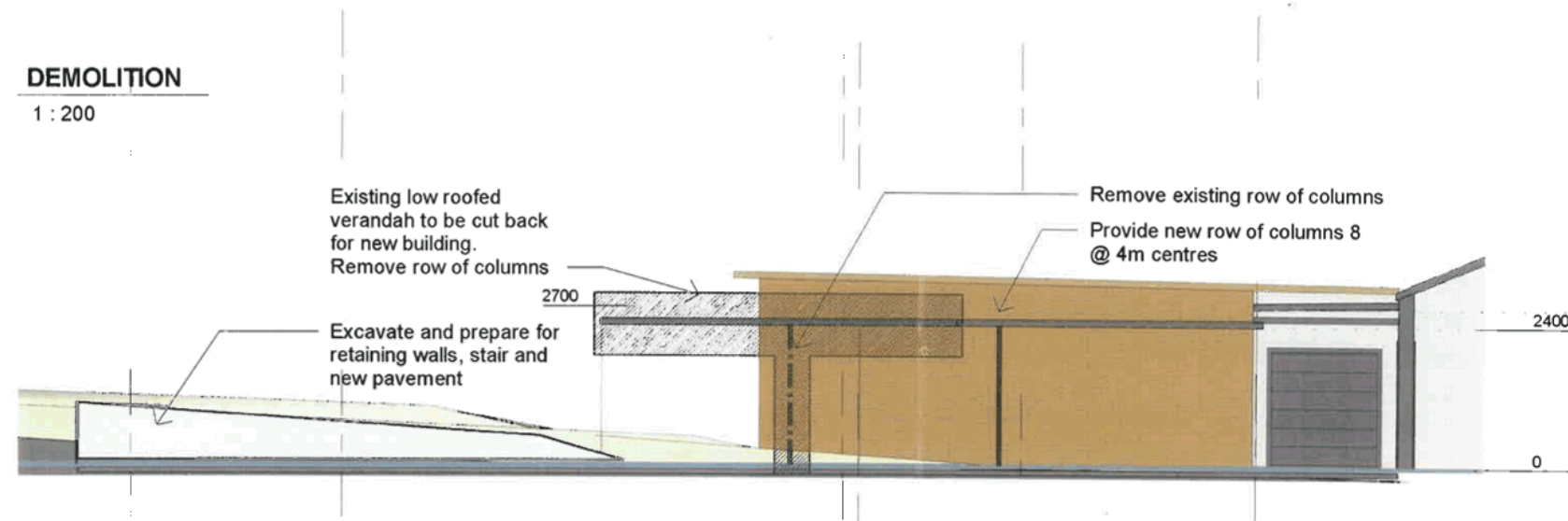








**DEMOLITION**  
1 : 200



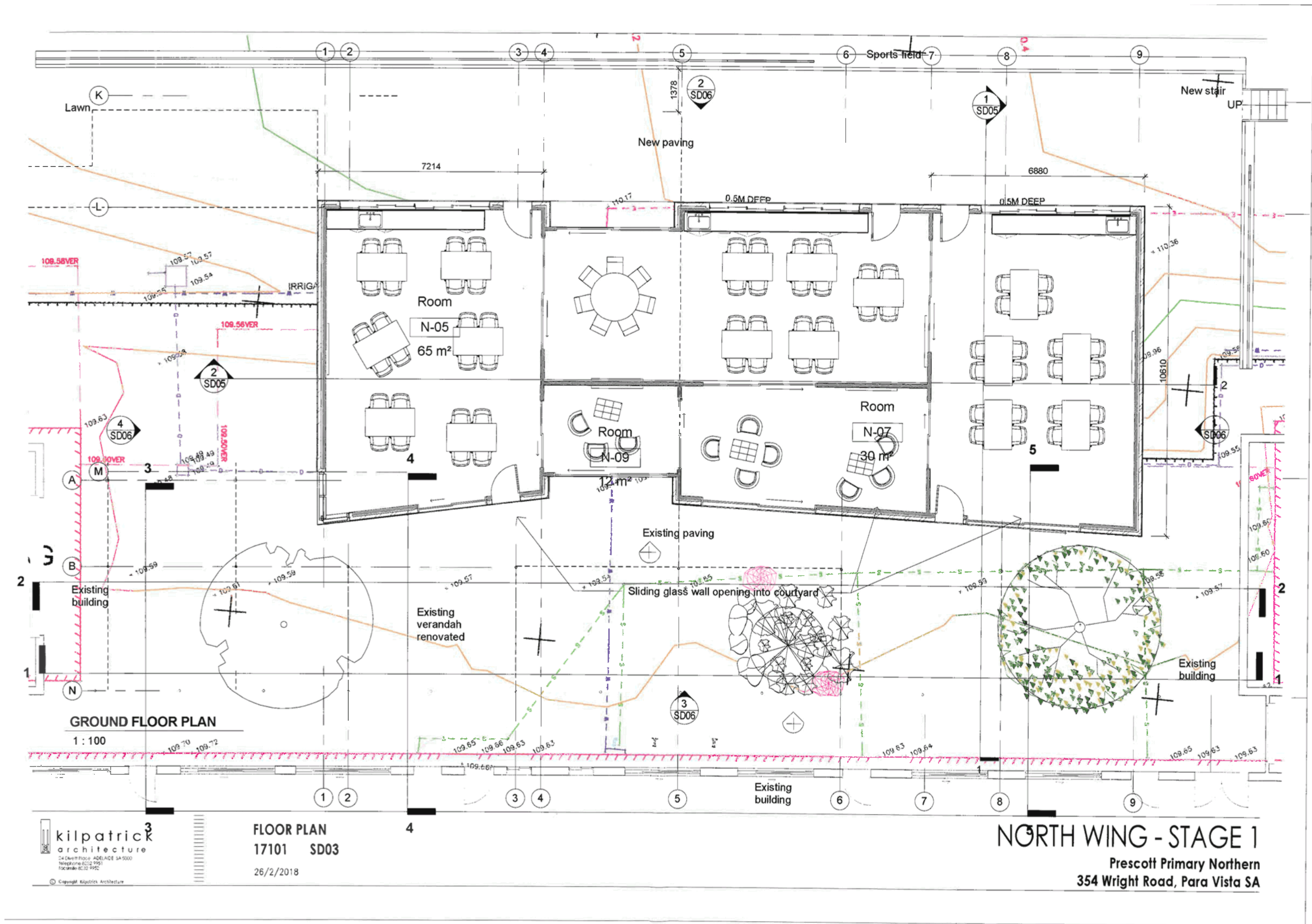
**Section 7**  
1 : 100



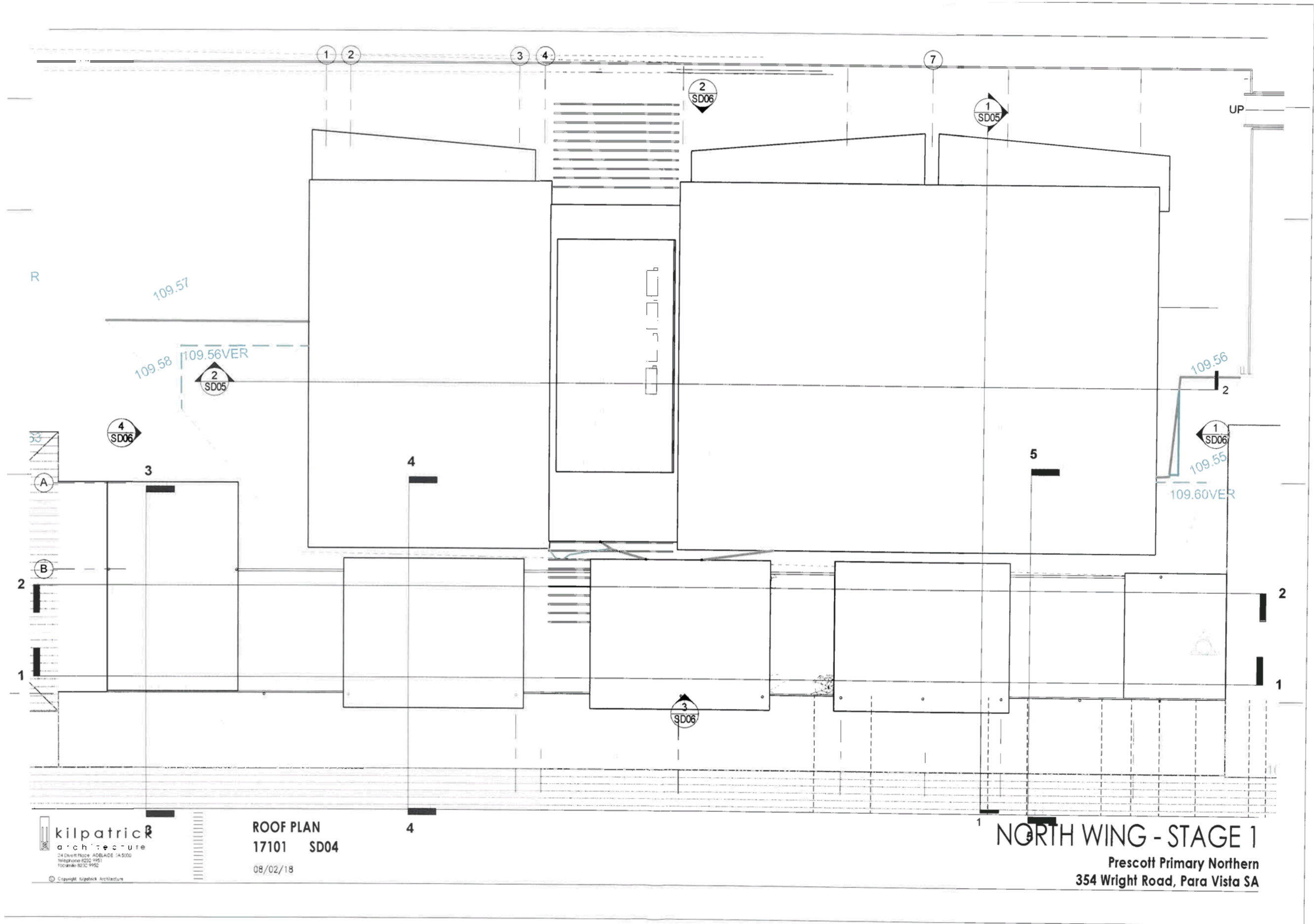
**EXISTING AND DEMOLITION**  
17101 SD02  
3/7/17

**NORTH WING - STAGE 1**  
Prescott Primary Northern  
354 Wright Road, Para Vista SA

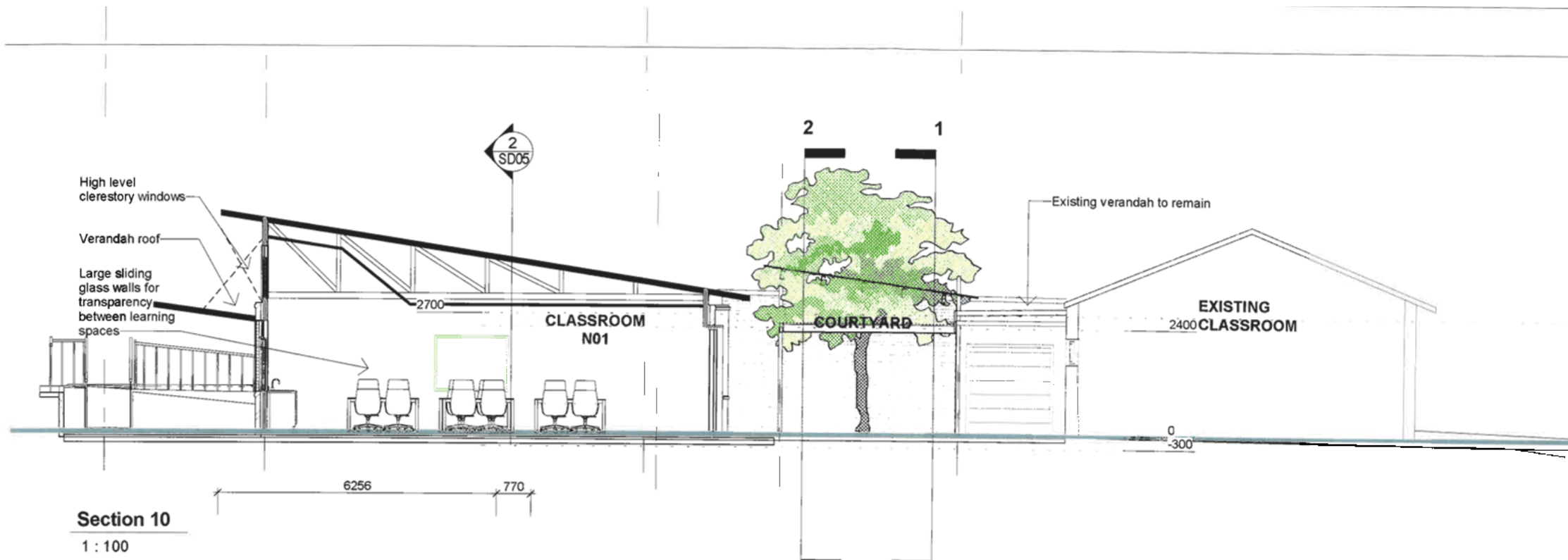




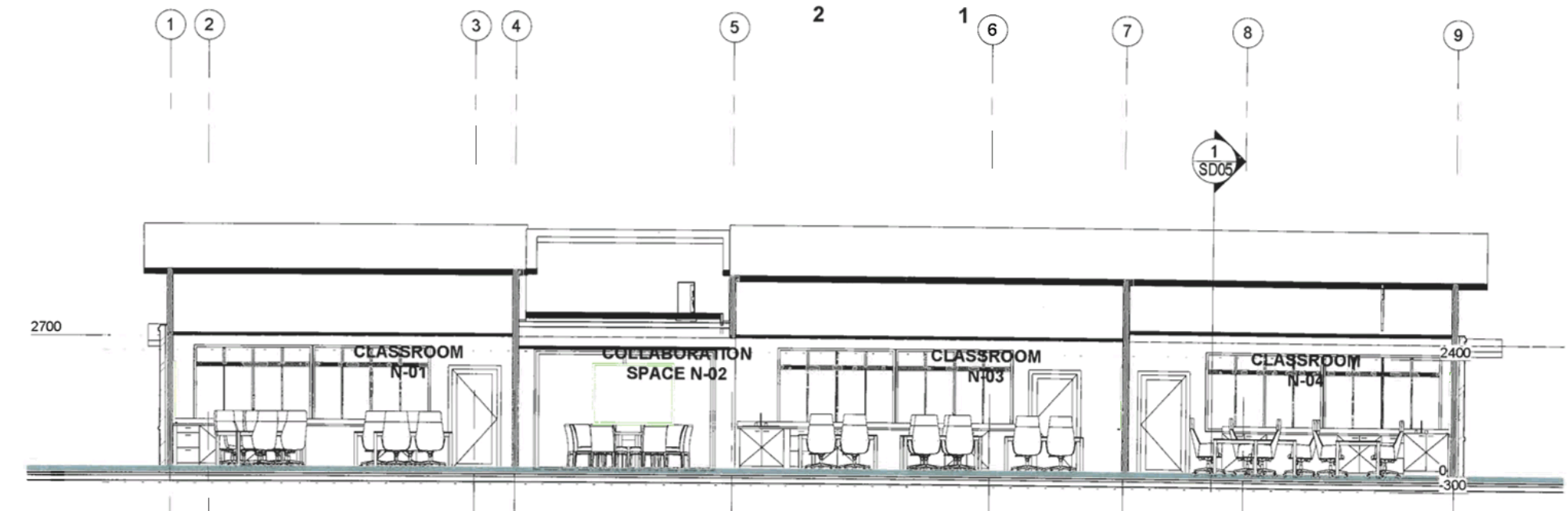








**Section 10**  
1 : 100



**Section 11**  
1 : 100

**kilpatrick**  
architecture  
24 Ewart Place, ADELAIDE SA 5000  
Telephone: 81 91 9151  
Facsimile: 81 91 9152  
© Copyright Kilpatrick Architecture

**SECTIONS**  
17101 SD05  
22/8/2018

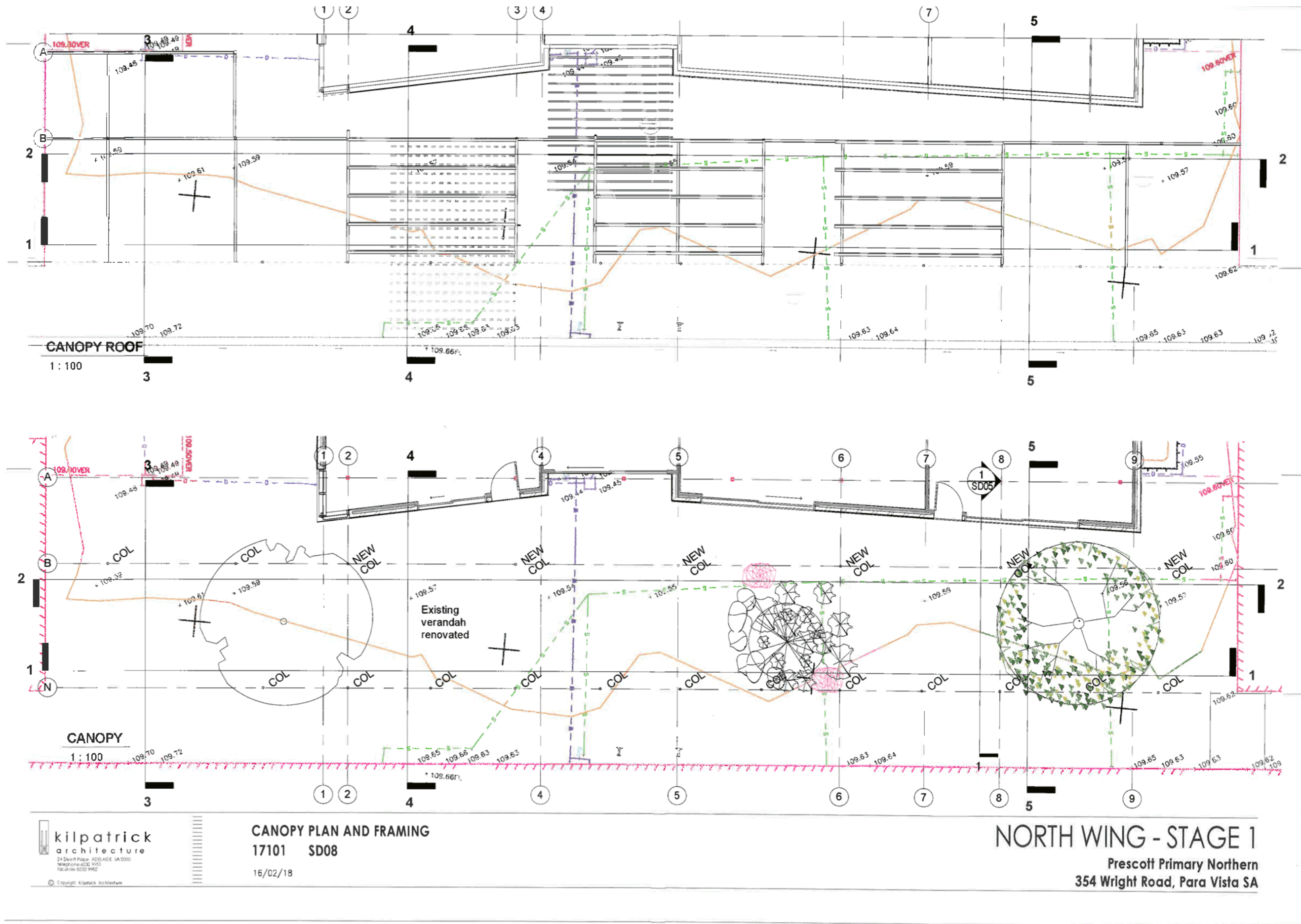
**NORTH WING - STAGE 1**  
Prescott Primary Northern  
354 Wright Road, Para Vista SA



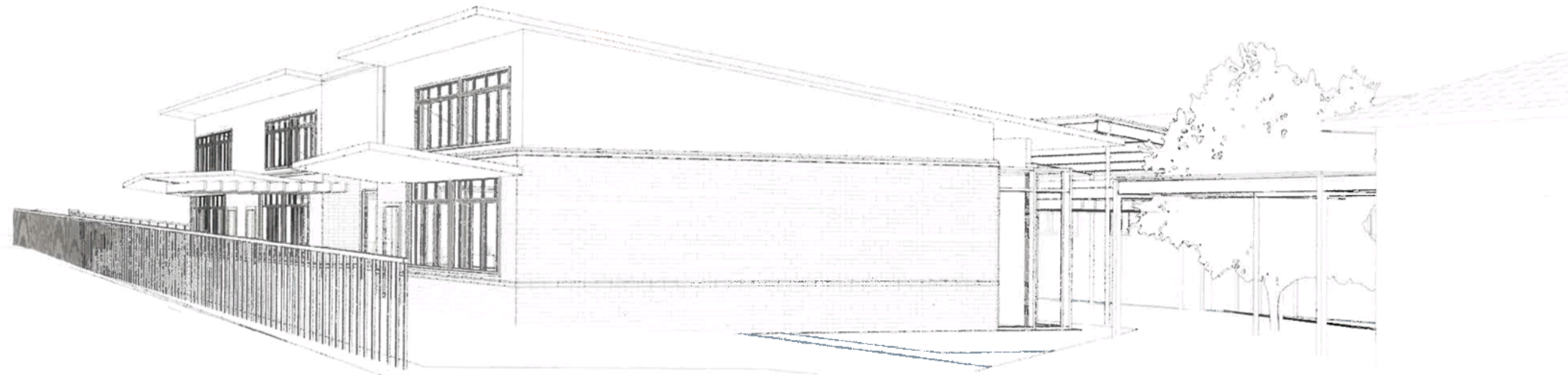




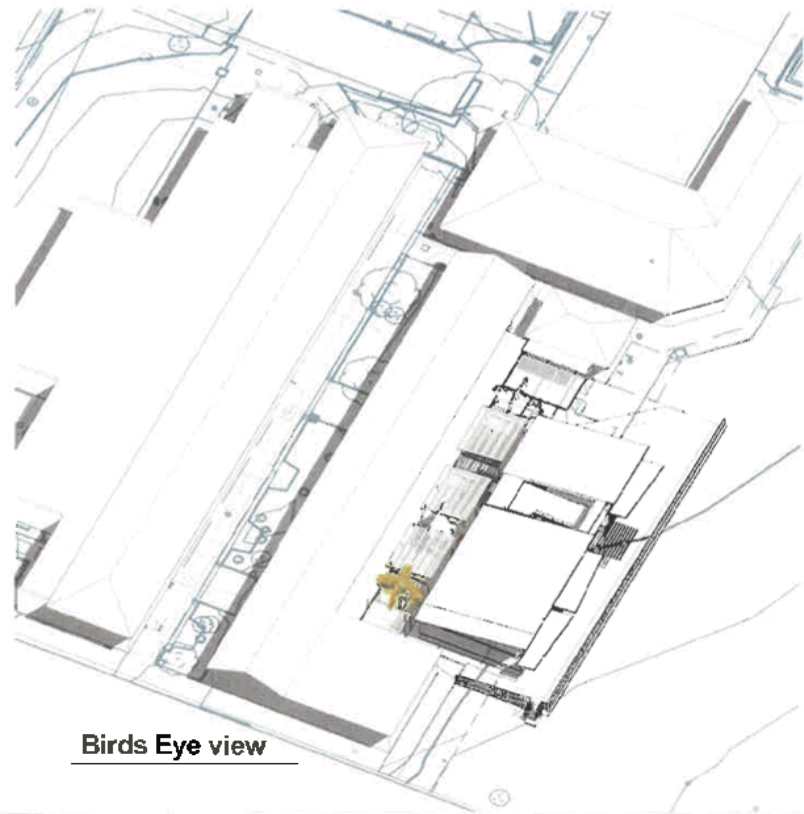




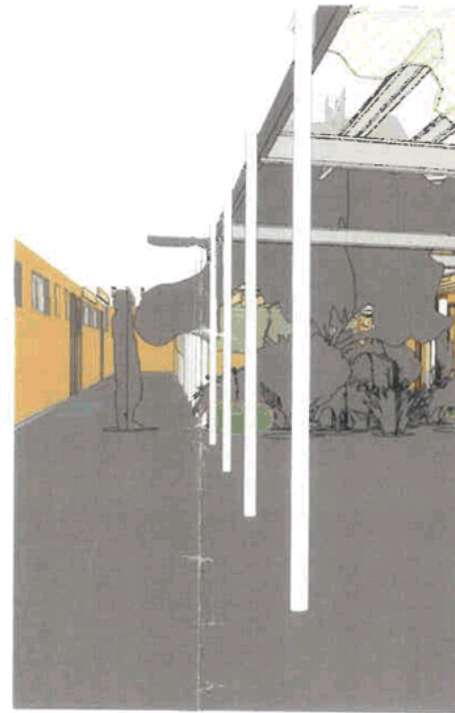




View from North West



Birds Eye view



Courtyard looking West



PERSPECTIVES  
17101 SD20  
1/02/2017

NORTH WING - STAGE 1  
Prescott Primary Northern  
354 Wright Road, Para Vista SA





**Kilpatrick Architecture**

24 Divett Place  
ADELAIDE, S.A.  
Australia 5000

Telephone 8232 9951  
Facsimile 8232 9952

26 February 2018

Planning Department  
City of Salisbury  
12 James Street  
SALISBURY SA 5108

Dear Sir/Ms,

**PRESCOTT PRIMARY NORTHERN SCHOOL  
NORTH WING DEVELOPMENT**

As part of ongoing development at Prescott Primary Northern, the school seeks Provisional Planning Consent to construct a new classroom wing. The works involve construction of a new three-classroom block with two meeting spaces. In addition, there will be siteworks consisting of additional pavement and retaining walls.

The materials will complement the existing cream brick, with a mix of matching cream brick, a contrasting dark red brick and lightweight profiled metal cladding. Trim colours such as barge and fascia will match existing in a red-brown.

This building is not visible from the street. No regulated or significant trees are impacted by this proposal.

The works are proposed to accommodate a slowly growing student population. There will be 440 students for the start of the 2019 school year

Toilet provision is more than sufficient to meet the needs of the anticipated staff and student populations.

Please find enclosed two copies of the drawings for the proposed works. The Development Application Form is also attached.

An Application for Building Rules Consent will be lodged with a Private Certifier when full documentation has been prepared.

Please advise the fee payable for the application and contact the undersigned should you require any further information. Thanks for your assistance.



DK:dk y:\a-projects\17101 ppn north wing\file10-approvals\planning\180226-da-planning.docx

29 FEB 2018



Regards,

A handwritten signature in black ink, appearing to read "David Kilpatrick", written over a faint, larger version of the same signature.

David Kilpatrick  
Kilpatrick Architecture

Enc:  
17101 SD01-SD08 (excl DD05) and SD20  
Development Application Form  
Electricity Act Form  
Certificate of Title





telephone 0232 7701

4 November 2018

Karyn Brown  
Development Officer -Planning  
City of Salisbury  
PO Box 8  
SALISBURY SA 5108

Dear Karyn

**Prescott Primary Northern  
Address: 350-354 Wright Road, Para Vista**

**APPLICATION FOR DEVELOPMENT PLAN CONSENT – DA 361/334/2018/3B**

**REQUEST FOR FURTHER INFORMATION 11 September 2018**

Please find attached revised site plan 17101-SD01B which provides additional information:

- The approved new parking and setdown corridor on the school site which links the Nelson Road carpark entry with the Wright Road service lane.
- Existing and proposed and approved new car parking numbers
- Existing houses which have now been or soon will be demolished since the original site plan was issued.
- Existing buildings and proposed new classroom building and building areas identified
- Setback distance from the eastern boundary to the proposed new classroom building shown.
- New landscaping and Nature Play areas which are under construction.

As discussed, the school has agreed to amend this application by reducing the number of students to 440 from the previously nominated 550.

This application for the new classroom building and approval of the 440 students on site is supported by the existing planning approval for the new setdown and parking traffic corridor through the site (now shown on the site plan). This corridor links the existing entry to the school car parking area on Nelson Road to the Wright Road service lane. Analysis of the proposal by Traffic Engineer Cirqa, has concluded that this will have a significant impact in reducing traffic congestion on Nelson Road and the Nelson Road/Wright Road roundabout by bringing the queuing lane onto the school site. This design will provide for up to 40 vehicles at a time, which might otherwise be waiting on the public road, to enter the property at Nelson Road and move slowly through the new corridor, setting down or picking up students in the pull-off parking, then exiting onto Wright Road, thus avoiding the roundabout.

The existing parking on the site totals 58 spaces. The design provides an additional 14 car parking spaces in the new corridor and will refurbish the service lane resulting in a further 22 parks, either reinstated or new spaces; a total of 94 car parking spaces on completion of the new vehicle corridor.



Director David Kilpatrick BSc Hons (LaTrobe) BArch Hons (Adel) R A I A

DK:dk y:la-projects\17101 ppn north wing\file\10-approvals\planning\161004-l-rfi response.doc



These car parks are in addition to the on-going use of the bus parking and setdown areas on the Wright Road service lane. It is important to note that the school has gradually increased the bus fleet to 5 small buses (with an option of a sixth when required), with the recent purchase of another 25-seater. The buses collect and return students from a wide area each day. Running close to their 126-seat capacity, these buses have a significant impact on reducing congestion at peak times. The school's commitment to the use of buses means 50-70 fewer cars delivering and collecting students at the site.

Overflow parking is also available on the adjacent Para Vista Church site on Nelson Road opposite the existing school parking area. An informal agreement is in place between the two churches for on-going use of the parking area at peak times, further easing the flow of vehicles around the public roadways. We are waiting on a formal letter from the Uniting Church and will forward this when it arrives.

Please contact me if you require any further information.

Yours sincerely

A handwritten signature in black ink, appearing to read 'David Kilpatrick', written over a light blue circular stamp.

David Kilpatrick  
Director  
Kilpatrick Architecture

For:  
Prescott Primary Northern School  
354 Wright Road, Para Vista

Enc: Site plan drawing 17101:SD01B



**PARA VISTA UNITING CHURCH**  
NELSON ROAD, PARA VISTA, 5093.

Postal Address:  
PO Box 336,  
Modbury North.  
S.A. 5093.

The Principal.  
Prescott Primary Northern.

Dear Mark,

At our last Church Council Meeting, our council decided to continue giving you permission to park cars during the week in our car park when there is an overflow of cars for your property.

Yours sincerely,

Meri Warneke

Chairperson- Para Vista Uniting Church Council.



**Attachment 2**  
Notice of Category 3 Development,  
Copies of Representations,  
Applicant's Response,  
Notification of Amended Plan and  
Copy of Response from Representor

**DEVELOPMENT ACT 1993  
CITY OF SALISBURY**

**NOTICE OF APPLICATION FOR CATEGORY 3 DEVELOPMENT**  
Pursuant to Section 38(5) of the Development Act, 1993

An application for development has been lodged with the Council for assessment. The details are as follows:

<b>APPLICATION NO:</b>	361/334/2018/3B
<b>APPLICANT:</b>	Kilpatrick Architecture 24 Divett Pl ADELAIDE SA 5000
<b>NATURE OF DEVELOPMENT:</b>	AMENDMENT TO DEVELOPMENT APPROVAL 361/2876/2005/1B (INCREASE STUDENT NUMBER CAP FROM 335 TO 550 STUDENTS) AND NEW CLASSROOM
<b>LOCATION:</b>	Prescott Northern School 346 & 348 and 350-354 Wright Road , Para Vista SA 5093 130, 132 & 134-144 Nelson Road , Para Vista SA 5093
<b>CERTIFICATE OF TITLE:</b>	CT-5454/316, CT-5224/902, CT-5467/729, CT-5454/315, CT-5256/738, CT-5359/835
<b>ZONE:</b>	Residential

The application may be examined at the Office of the Council located at 12 James Street, Salisbury during business hours (8.30am – 5pm Monday to Friday) and on Council's web site at [www.salisbury.sa.gov.au](http://www.salisbury.sa.gov.au). Any person or body may make representations in writing, or by email to [development@salisbury.sa.gov.au](mailto:development@salisbury.sa.gov.au), concerning this application and should address their representation to the Chief Executive Officer at PO Box 8, Salisbury or [representations@salisbury.sa.gov.au](mailto:representations@salisbury.sa.gov.au). Representations must be received **no later than Friday 18<sup>th</sup> May 2018**.

Each person making a submission should indicate whether they wish to appear personally, or be represented by another party, in support of their submission. Please note that should you nominate to be heard in support of your representation, you will be required to attend a Development Assessment Panel meeting held at the Council offices, scheduled on the fourth Tuesday of each month at 6.00pm (unless otherwise advised).

Please note that, pursuant to Section 38(8) of the Development Act, a copy of each representation received will be forwarded to the applicant to allow them to respond to all representations received.

Signed: Karyn Brown, Development Officer

Date: 3 May 2018

***THIS IS THE FIRST AND ONLY PUBLICATION OF THIS NOTICE***

Frances Bedford MP JP State Member for Florey



436 Montague Road  
Modbury North  
South Australia 5092  
(opp Cloverreath Shopping  
Centre)  
Phone 8263 2666  
Fax 8263 2166  
florey@parliament.sa.gov.au  
PO Box 143  
Modbury North  
South Australia 5092

Community Counts!!

Mr J Harry, CEO  
City of Salisbury  
PO Box 8  
SALISBURY SA 5108

Dear Mr Harry

**RE: APPLICATION #361/334/2018/3B, Prescott Northern School**

My assistance has been sought by constituents who reside in the vicinity of Prescott Northern School in relation to the above application which seeks to expand the school to accommodate 550 students, an increase of 215 above the current school's 335 student body.

Over many years, I have dealt with complaints about traffic issues around this school which in association with the traffic generated by the nearby Para Vista Primary School, creates havoc at peak times, particularly along Nelson Road.

The number of car movements by the school community is compounded by the traffic on Nelson Road as residents commute to work. This being the case, I cannot see how a development of this magnitude can be permitted without major traffic measures being put in place to assure the safety of all concerned and traffic flow.

Bus stops are another factor on both Nelson and Wright Roads. And parents parking on all adjacent streets – Jeff Street, Heather Drive and Nelson Road – often ignoring yellow lines means real problems for all local residents.

I would be grateful if you would note my interest and concern in this matter and seek an urgent meeting and/or advice on what will be done to ensure all traffic issues are resolved before consideration is given to approval on any other grounds.

Yours sincerely

**FRANCES BEDFORD**

Enc.

11 May, 2018

CC Tony Zappia (MP for Makin), Cr Julie Woodman, Cr Sean Bedford



**STATEMENT OF REPRESENTATION**  
**Pursuant to Section 38 of the Development Act**

*Valid*

**To:** City of Salisbury  
 PO Box 8, SALISBURY SA 5108  
 Email: [representations@salisbury.sa.gov.au](mailto:representations@salisbury.sa.gov.au)

18 MAY 2018

THIS SHEET PROVIDES YOU WITH THE OPPORTUNITY TO MAKE COMMENTS IN RELATION TO A PROPOSED DEVELOPMENT. PLEASE FIND ATTACHED DETAILS OF THE PROPOSED DEVELOPMENT.

<b>Application Number:</b>	361/334/2018/3B
<b>Applicant</b>	Kilpatrick Architecture
<b>Nature of Development:</b>	AMENDMENT TO DEVELOPMENT APPROVAL 361/2876/2005/1B (INCREASE STUDENT NUMBER CAP FROM 335 TO 550 STUDENTS) AND NEW CLASSROOM
<b>Location:</b>	Prescott Northern School 346 & 348 and 350-354 Wright Road , Para Vista SA 5093 130, 132 & 134-144 Nelson Road , Para Vista SA 5093

**YOUR DETAILS:** (this information must be provided to ensure that this is a valid representation)

NAME(S): IAN STRADLING, LUYVELYS STRADLING  
 ADDRESS: 18 HEATHER DRIVE, PARA VISTA SA 5093  
 PHONE NO: [REDACTED] EMAIL: [REDACTED]

I am: (please tick one of the following boxes as appropriate)

- The owner/occupier of the property located at: 18 HEATHER DRIVE, PARA VISTA
- Other (please state): .....

**YOUR COMMENTS:**

I/We: (please tick the most appropriate box below)

- Support the proposed development.
- Oppose the proposed development.



361/334/2018/3B

**Whether you support or oppose this proposal you must provide written reasons below to ensure that this is a valid representation.**

THERE HAS BEEN NO MENTION OF A TRAFFIC MANAGEMENT PLAN. THERE ARE ALREADY TRAFFIC CONGESTION ISSUES AROUND THE DROP OFF & COLLECTION OF KIDS AT THIS SCHOOL LEADING TO POTENTIALLY HAZARDOUS SITUATIONS. THIS WILL BE MADE WORSE WITH INCREASED STUDENT NUMBERS LEADING TO AN INCREASE IN VEHICULAR TRAFFIC.

THE CURRENT SITUATION CREATES A TRAFFIC NUISANCE TO OTHER RESIDENTS, THIS WILL ALSO BE COMPOUNDED WITH ADDITIONAL GROWTH OF THIS SCHOOL.

**My concerns would be addressed by:** *(state changes/actions to the proposal sought)*

THE SCHOOL PUTTING FORWARD A VALID PROPOSAL FOR SAFE TRAFFIC MANAGEMENT FOR REVIEW & DEVELOPMENT

PTO

Regulation 35(e) of the Development Regulations 2008 requires that a representation must indicate a person's desire to be heard. Please note that if you do not indicate that you wish to be heard, it will be taken that you do not wish to be heard by the Panel.

I/We:

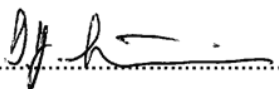
- Do not wish to be heard in support of my representation.
- Wish to be heard in support of my representation, and I will be:
  - Appearing personally,
  - OR**
  - Represented by the following person: .....
  - Contact details: .....

*(Please note, matters raised in your written representation will be considered during the assessment and do not need to be repeated at the hearing).*

**Your written representation must be received by Council no later than 11.59pm on Friday 18<sup>th</sup> May 2018, to ensure that it is a valid representation and taken into account.**

**Represntor's Declaration:**

I am aware that the representation will become a public document as prescribed in the Freedom of Information Act 1991, and will be made available to the applicant, agencies and other bodies pursuant to the Development Act 1993, and may be uploaded to the Council's website as an attachment to the hearing agenda.

Signature:  ..... Date: 18 / 5 / 18

**Please complete this checklist to ensure your representation is valid:**

- Name and address of person (or persons).
- If more than one person, details of person making the representation.
- Detail of reasons for making the representation.
- Indication whether or not the person (or persons) wishes to be heard.
- Submitted no later than 11.59pm on **Friday 18<sup>th</sup> May 2018.**



**STATEMENT OF REPRESENTATION**  
Pursuant to Section 38 of the Development Act



**To:** City of Salisbury  
PO Box 8, SALISBURY SA 5108  
Email: [representations@salisbury.sa.gov.au](mailto:representations@salisbury.sa.gov.au)

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<b>Applicant</b>	Kilpatrick Architecture
<b>Nature of Development:</b>	AMENDMENT TO DEVELOPMENT APPROVAL 361/2876/2005/1B (INCREASE STUDENT NUMBER CAP FROM 335 TO 550 STUDENTS) AND NEW CLASSROOM
<b>Location:</b>	Prescott Northern School 346 & 348 and 350-354 Wright Road , Para Vista SA 5093 130, 132 & 134-144 Nelson Road , Para Vista SA 5093

**YOUR DETAILS:** (this information must be provided to ensure that this is a valid representation)

NAME(S): SAIVOUR & SUSAN MAMO

ADDRESS: 146 NELSON RD PARA VISTA 5093

PHONE NO: [REDACTED] EMAIL: .....

I am: (please tick one of the following boxes as appropriate)

- The owner/occupier of the property located at: 146 NELSON RD PARA VISTA
- Other (please state): .....

**YOUR COMMENTS:**

I/We: (please tick the most appropriate box below)

- Support the proposed development.
- Oppose the proposed development.

Item 5.1.2 - Attachment 2 - Notice of Category 3 Development, Copies of Representations, Applicant's Response, Notification of Amended Plan and Copy of Response from Representor

**Whether you support or oppose this proposal you must provide written reasons below to ensure that this is a valid representation.**

Our main concern with this development is the increase of Traffic, this has been an ongoing problem with parking, Nelson rd is not wide enough to cope now when the school children are being dropped off or picked up, council addressed our concerns once before by painting a yellow line out the front of our property, this made no difference as the cars still park there

**My concerns would be addressed by:** (state changes/actions to the proposal sought)

The school needs to provide a safe area where the children can be dropped off and picked up safely and without creating a traffic hazard, maybe no standing signs between the drop off and pick up times on Nelson rd might help

PTO

Regulation 35(e) of the Development Regulations 2008 requires that a representation must indicate a person's desire to be heard. Please note that if you do not indicate that you wish to be heard, it will be taken that you do not wish to be heard by the Panel.

I/We:

- Do not wish to be heard in support of my representation.
- Wish to be heard in support of my representation, and I will be:
- Appearing personally,
- OR**
- Represented by the following person: .....
- Contact details: .....

*(Please note, matters raised in your written representation will be considered during the assessment and do not need to be repeated at the hearing).*

**Your written representation must be received by Council no later than 11.59pm on Friday 18<sup>th</sup> May 2018, to ensure that it is a valid representation and taken into account.**

**Representor's Declaration:**

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Signature: S Mow .....

Date: 18 / 5 / 2018

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**Please complete this checklist to ensure your representation is valid:**

- Name and address of person (or persons).
- If more than one person, details of person making the representation.
- Detail of reasons for making the representation.
- Indication whether or not the person (or persons) wishes to be heard.
- Submitted no later than 11.59pm on **Friday 18<sup>th</sup> May 2018**.



**STATEMENT OF REPRESENTATION**  
**Pursuant to Section 38 of the Development Act**



**To:** City of Salisbury  
 PO Box 8, SALISBURY SA 5108  
 Email: [representations@salisbury.sa.gov.au](mailto:representations@salisbury.sa.gov.au)

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<b>Applicant</b>	Kilpatrick Architecture
<b>Nature of Development:</b>	AMENDMENT TO DEVELOPMENT APPROVAL 361/2876/2005/1B (INCREASE STUDENT NUMBER CAP FROM 335 TO 550 STUDENTS) AND NEW CLASSROOM
<b>Location:</b>	Prescott Northern School 346 & 348 and 350-354 Wright Road , Para Vista SA 5093 130, 132 & 134-144 Nelson Road , Para Vista SA 5093

**YOUR DETAILS:** (this information must be provided to ensure that this is a valid representation)

NAME(S): MRS MRS T F FIELDER

ADDRESS: 7 HEATHER DRIVE PARAVISTA SA 5093

PHONE NO: [REDACTED] EMAIL: .....

*MOBILE*  
 I am: (please tick one of the following boxes as appropriate)

SA 5093

- The owner/occupier of the property located at: 7 HEATHER DRIVE PARAVISTA
- Other (please state): .....

**YOUR COMMENTS:**

*X/We:* (please tick the most appropriate box below)

- Support the proposed development.
- Oppose the proposed development.

361/334/2018/3B

Whether you support or oppose this proposal you must provide written reasons below to ensure that this is a valid representation.

We have major concerns regarding the increase from 335 to 5502 pupils and new classrooms Prescott of Golden School. With consultation with Francis Bedford a member of FLORE, M.P. J.P. a lot of survey in relation the traffic issues location, numbers of students, traffic along Nelson Road causing congestion, very dangerous for students being dropped off to school and unauthorised parking spaces for parents.

This has resulted in cars being parked in Jeff Street, Death Drive, Nelson Road behind the entrance to the school. Some local residents have contacted Salisbury Council for yellow lines to be placed in front of their house and parents taking students to the school ignore the signs and congestion of local

My concerns would be addressed by: (state changes/actions to the proposal sought)

residents <sup>HAVE</sup> ~~and~~ occurred and Police informed of the frustration. Between 8-30 AM - 9 AM it is almost impossible to turn left or right from Death Drive to enter Nelson Road with the volume of traffic. Para Vista Primary School is on the corner of Nelson Road and Montague Road. There are major traffic issues at the moment.

Accidents have occurred in the past, two people being taken to hospital by ambulance.

It has resulted in consider parking in the Uniting Church on Nelson Road, installing Traffic Lights to solve major concerns & issues. By the school committee "please see over page

PTO

Item 5.1.2 - Attachment 2 - Notice of Category 3 Development, Copies of Representations, Applicant's Response, Notification of Amended Plan and Copy of Response from Representor

The Sports Field is not of a large scale. The students have not a big sports area for recreation and curriculum criteria for 335 students. The extra students, it would crowd the area resulting in poor standards of education and personal development for recreation and sport.



Regulation 35(e) of the Development Regulations 2008 requires that a representation must indicate a person's desire to be heard. Please note that if you do not indicate that you wish to be heard, it will be taken that you do not wish to be heard by the Panel.

I/We:

- Do not wish to be heard in support of my representation.
- Wish to be heard in support of my representation, and I will be:
  - Appearing personally,
- OR**
- Represented by the following person: .....
- Contact details: .....

*(Please note, matters raised in your written representation will be considered during the assessment and do not need to be repeated at the hearing).*

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**Representor's Declaration:**

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Signature:  .....

Date: 8 15 2018

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- Detail of reasons for making the representation.
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**STATEMENT OF REPRESENTATION**  
**Pursuant to Section 38 of the Development Act**

**To:** City of Salisbury  
 PO Box 8, SALISBURY SA 5108  
 Email: [representations@salisbury.sa.gov.au](mailto:representations@salisbury.sa.gov.au)

THIS SHEET PROVIDES YOU WITH THE OPPORTUNITY TO MAKE COMMENTS IN RELATION TO A PROPOSED DEVELOPMENT. PLEASE FIND ATTACHED DETAILS OF THE PROPOSED DEVELOPMENT.

<b>Application Number:</b>	361/334/2018/3B
<b>Applicant</b>	Kilpatrick Architecture
<b>Nature of Development:</b>	AMENDMENT TO DEVELOPMENT APPROVAL 361/2876/2005/1B (INCREASE STUDENT NUMBER CAP FROM 335 TO 550 STUDENTS) AND NEW CLASSROOM
<b>Location:</b>	Prescott Northern School 346 & 348 and 350-354 Wright Road , Para Vista SA 5093- 130, 132 & 134-144 Nelson Road , Para Vista SA 5093

**YOUR DETAILS:** (this information must be provided to ensure that this is a valid representation)

NAME(S): LI QUN KHO .....

ADDRESS: 3/360 WRIGHT ROAD PARA VISTA SA 5093 .....

PHONE NO: [REDACTED] ..... EMAIL: .....

I am: (please tick one of the following boxes as appropriate)

- The owner/occupier of the property located at: 3/360 WRIGHT ROAD PARAM VISTA SA 5093 .....
- Other (please state): .....

**YOUR COMMENTS:**

I/We: (please tick the most appropriate box below)

- Support the proposed development.
- Oppose the proposed development.

- 9 MAY 2018

361/334/2018/3B

**Whether you support or oppose this proposal you must provide written reasons below to ensure that this is a valid representation.**

I ~~SUPP~~ SUPPORT THE DEVELOPMENT OF A SCHOOL AND EDUCATION OF THE CHILDREN

**My concerns would be addressed by: (state changes/actions to the proposal sought)**

PLEASE ENSURE THERE ARE SUFFICIENT CARPARKS WITH THE ADDITION OF ~~CLASS~~ CLASSROOMS, STUDENTS AND CARS FOLLOWING THE INCREASE OF CLASSROOMS. IT HAS BEEN INCONVENIENT TO US FOR PARENTS HAVE BEEN PARKING THEIR CARS AND BLOCKING THE TRAFFIC IN OUR RESIDENTIAL AREA.

PTO

Regulation 35(e) of the Development Regulations 2008 requires that a representation must indicate a person's desire to be heard. Please note that if you do not indicate that you wish to be heard, it will be taken that you do not wish to be heard by the Panel.

I/We:

Do not wish to be heard in support of my representation.

Wish to be heard in support of my representation, and I will be:

Appearing personally,

**OR**

Represented by the following person: .....


Contact details: .....

*(Please note, matters raised in your written representation will be considered during the assessment and do not need to be repeated at the hearing).*

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Signature: ..... 

Date: 5 1 5 1 1 8

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- Detail of reasons for making the representation.
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**STATEMENT OF REPRESENTATION**  
Pursuant to Section 38 of the Development Act

To: City of Salisbury  
PO Box 8, SALISBURY SA 5108  
Email: [representations@salisbury.sa.gov.au](mailto:representations@salisbury.sa.gov.au)

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<b>Nature of Development:</b>	AMENDMENT TO DEVELOPMENT APPROVAL 361/2876/2005/1B (INCREASE STUDENT NUMBER CAP FROM 335 TO 550 STUDENTS) AND NEW CLASSROOM
<b>Location:</b>	Prescott Northern School 346 & 348 and 350-354 Wright Road , Para Vista SA 5093 130, 132 & 134-144 Nelson Road , Para Vista SA 5093

**YOUR DETAILS:** (this information must be provided to ensure that this is a valid representation)

NAME(S): DARYL & SHARON BENNETT

ADDRESS: 4 PITANA AVE, PARA VISTA, SA 5093

PHONE NO: [REDACTED] EMAIL: [REDACTED]

I am: (please tick one of the following boxes as appropriate)

- The owner/occupier of the property located at: UNIT 12/360 WRIGHT RD, PARA VISTA, SA 5093
- Other (please state):

**YOUR COMMENTS:**

I/We: (please tick the most appropriate box below)

- Support the proposed development.
- Oppose the proposed development.

Item 5.1.2 - Attachment 2 - Notice of Category 3 Development, Copies of Representations, Applicant's Response, Notification of Amended Plan and Copy of Response from Representor

361/334/2018/3B

**Whether you support or oppose this proposal you must provide written reasons below to ensure that this is a valid representation.**

We support this proposal. The building does not face our property and is not going to have any impact on our tenant.

**My concerns would be addressed by:** *(state changes/actions to the proposal sought)*

PTO

Regulation 35(e) of the Development Regulations 2008 requires that a representation must indicate a person's desire to be heard. Please note that if you do not indicate that you wish to be heard, it will be taken that you do not wish to be heard by the Panel.

I/We:

- Do not wish to be heard in support of my representation.
- Wish to be heard in support of my representation, and I will be:
- Appearing personally,
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- Represented by the following person: .....
- Contact details: .....

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Signature:  .....

Date: 8 / 5 / 18

---

**Please complete this checklist to ensure your representation is valid:**

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- Detail of reasons for making the representation.
- Indication whether or not the person (or persons) wishes to be heard.
- Submitted no later than 11.59pm on **Friday 18<sup>th</sup> May 2018.**



**Kilpatrick Architecture**

24 Divett Place  
ADELAIDE, S.A.  
Australia 5000

Telephone 8232 9951

7 June 2018

Karyn Brown  
Development Officer -Planning  
City of Salisbury  
PO Box 8  
SALISBURY SA 5108

Dear Karyn

**Prescott Primary Northern  
Address: 350-354 Wright Road, Para Vista**

**APPLICATION FOR DEVELOPMENT PLAN CONSENT – DA 361/334/2018**

Thank you for the opportunity to respond to the representations received from the public notification for the above proposed development. Our response follows.

As representors have indicated that they wish to be heard by the Development Assessment Panel, I too, on behalf of the school, wish to address the Panel.

Please, contact the undersigned should you require any additional information or clarification.

Yours sincerely

David Kilpatrick  
**Director  
Kilpatrick Architecture**

For:  
Prescott Primary Northern School  
354 Wright Road, Para Vista



Director David Kilpatrick BSc Hons (LaTrobe) BArch Hons (Adel) R A I A

DK:ds:\la-project\st17101 ppn north wing\file\10-approvals\planning\180607-4-cat 3 response.doc





#### **BRIEF HISTORY**

Prescott Primary Northern was established in 1973 and has expanded progressively and continues to be a growing and thriving learning institution and a respected part of the local Para Vista Community.

The school is located within a predominantly Residential Zone as defined by the Development Plan of the City of Salisbury. It is important to note that the Development Plan (Dec 2016) states that part of the desired character of the Residential Zone is that *"in addition to housing and open space, the zone will also contain a variety of community related, non-residential land uses that contribute to the creation of pleasant, connected, well serviced and socially inclusive neighborhoods"*. One of the Principles of Development Control (Item 1) states that *"small scale non-residential use that serves the local community..."* such as primary or secondary school development is envisaged in the zone.

Further Principle of Development Control (Item 4) is also satisfied:

*Non-residential development such as shops, schools and consulting rooms should be of a nature and scale that:*

- (a) serves the local community*
- (b) is consistent with the character of the locality*
- (c) does not detrimentally impact on the amenity of nearby residents*

As part of its long-term planning, Prescott Primary Northern School intends to develop the infrastructure and facilities to allow for anticipated growth over the next two decades. A Master Plan has been prepared to provide for timely, incremental development of the site to accommodate the expansion, including the traffic and parking requirements.

The School has identified an immediate need for 3 extra classrooms. However, in 2005 a condition was imposed that limited student numbers to only 330 students. As at 2018/19 the school would like to increase the numbers to 440 students. Further projections anticipate a natural student growth to 550 by 2022.

The school has for some time been concerned about the traffic and safety issues for both their students and the public. Following the comprehensive master planning review, Prescott Primary Northern developed a traffic management strategy in association with Traffic Engineers Cirqa Pty Ltd which will see more vehicles queueing and setting down on site rather than on the street. Cirqa prepared a report which assessed the impact of this proposed traffic management strategy.

In 2017 a Planning Application was lodged and subsequently approved by Salisbury Council, for a new one-way, drive-through kiss and drop zone. Users will enter the new drive-through lane from the existing Nelson Road cross-over and exit onto Wright Road at the eastern boundary of the school. This will significantly reduce the number of vehicles waiting to turn into the property from the public road by providing a 250-metre-long queueing and set-down lane on the western perimeter of the property and connecting with the Wright Road service lane along the southern boundary of the school. Approval of the proposal was granted on 28 September 2017. Design and documentation for this approved scheme is underway and construction of the work is expected to commence early in 2019.

#### **2018 APPLICATION**

The current application seeks to establish 3 additional classrooms on site to cater for up to 440 students. The School recognized that this could lead to further issues with access at school drop off and pick-up times. Cirqa Pty Ltd prepared a design and traffic impact report in May 2018 to assess the effect of the increased student population on the traffic, taking into account the approved scheme using the proposed new on-site drive-through lane. The advice from Cirqa was that with the implementation of the approved scheme, from a traffic point of view, meant that the school could expand to properly cater for 550 students by 2022. That advice explains why the 2005 condition is not sought to be varied from 330 to 550 rather than an intermediate variation to 440 students as from 2019.



The foregoing summary of the steps supporting the submission for the proposed increase in student numbers has been presented to make clear that the proposal is the result of long-term planning and arises from a concern to provide a better community service and improve the amenity for the surrounding residents. With this in mind, we consider the submissions from the local citizens. We will not respond to the submissions separately but summarise the issues.

#### THE REPRESENTATIONS

There have been five representations in total. Two of these are in favour. A letter was also received from Frances Bedford MP. The three formal submissions against the proposal refer primarily to the increased traffic on surrounding streets. The respondents indicated their concerns would be addressed with an improved traffic management scheme on the site.

#### Additional Classrooms

There does not seem to be any real concern expressed as regards the three classrooms proposed.

#### Traffic Concerns

Without repeating in detail, the Traffic Engineering Report on the Traffic Management Strategy providing the new on-site drive-through and setdown lane concluded that the proposal will reduce the number of egress movements to Nelson Road, improve circulation, reduce the need for reversing movements and increase parking availability on the site, thus improving safety and efficiency of traffic movement. Further, the analysis in the Cirqa investigation into the proposed increase in student numbers, indicated that the *"additional traffic generated as a result of student population increases will be readily accommodated at the site's access points. While recognising that there are relatively high levels of queueing and delays experienced on approaches to the Nelson Road/Wright Road roundabout during the peak periods, the additional movements associated with the School will generally retain existing conditions"*. The Traffic Management Strategy and conclusions of the Traffic Engineering Report should allay concerns raised by the representors.

It may be that the representors were not aware of the traffic management strategy and the Cirqa report. In addition, the analysis undertaken by Cirqa not only addresses the current concerns, noting the approved scheme is yet to be implemented, but also assesses the increase in student numbers to 550. It is clear that the needs and concerns raised by the representors in terms of

- S and S Mamo – "The School needs to provide a safe area where children can be dropped off and picked up safely ..."  
and
- I and L Stradling - "The School putting forward a valid proposal for safe traffic management ..."  
and generally
- the comments from the Hon Frances Bedford MP

have been addressed in the Cirqa Report and the proposed traffic management strategy.

The operation of the school on this site is a long-standing and appropriate existing use. It is a use recognized in the Zone. Further the School has existing use rights. So as to continue to provide a valuable community service and to remain viable, the School needs to be able to facilitate reasonable growth and development of that existing use. The new classrooms will cater for the demand being experienced by the School. We believe that concerns raised by the Representors regarding the detrimental impact of increased in traffic on surrounding road as a result of the proposal have been thoroughly and professionally investigated by experts in the area of traffic management and engineering. The expert advice has concluded that the increased student numbers while resulting in an increase in vehicle movements, will not significantly impact on surrounding roads when viewed in conjunction with the approved scheme; i.e. the soon to be established drive-through setdown lane. This traffic management strategy will bring benefits of improved traffic flow for the broader community and



will not severely impact the operation Nelson and Wright Roads. For all these reasons the application (DA 361/334/2018) should be granted Development Plan Consent.



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16 October 2018

Dear

**Applicant:** Kilpatrick Architecture  
**Application No:** 361/334/2018/3B  
**Subject Site:** Prescott Northern School, 350-354 Wright Road, Para Vista SA 5093  
**Proposed Development:** Amendment To Development Approval 361/2876/2005/1B (Increase Student Number Cap From 335 To 440 Students) And New Classroom

Recently you completed a Statement of Representation in regards to a development application for a new classroom and an increase in student numbers from 335 to 550 students at Prescott Northern School.

Since the development application was publicly notified, the applicant has provided an amended Site Plan and additional information. Specifically:

- The original proposal sought to increase the student number cap from 335 to 550 students. This has been reduced to a cap of 440 students;
- The Site Plan has been amended to show the on-site set-down/pick-up loop and car parking corridor granted planning consent in Development Application 361/1433/2016/3B. This will link the Nelson Road car park entry with the Wright Road service road and provide 14 car parking spaces. This is in addition to the existing 53 spaces on the site and a further 22 spaces in the service road;
- A subsequent traffic analysis has been prepared by CIRQA. Their analysis concludes the on-site set-down/pick-up loop in addition to the additional car parking spaces will improve accommodation of parking demands and assist with easing congestion currently experienced on the adjacent road network; and
- An agreement has been entered into with the Para Vista Uniting Church to allow for overflow car parking for the school.

Council are interested in your views on the amended Site Plan and the additional information provided by the applicant. Please review the attached information and should you have any comments, please provide these in writing to Council no later than **31 October 2018**.

It is also noted in your Statement of Representation that you indicated a desire to be heard in support of your representation. Should you no longer wish to be heard by the Council Assessment Panel, can you please also advise Council in your comments.

I would be happy to assist you with the consideration of the above. If you have any queries or would like further information, please do not hesitate to contact me.

Yours sincerely

A handwritten signature in blue ink, appearing to read "Karyn Brown".

**Karyn Brown**  
Development Officer - Planning  
Phone: 08 8406 8323  
Email: development@salisbury.sa.gov.au

7 Heather Drive  
Para Vista  
South Coast 5093

24th October 2018

25 OCT 2018

M/S J. Brown Development Office Planning

Dear M/S Brown,

Thank you for letter dated 16th October 2018.  
I refer to application no 361/334/2018/3B Prescott Northern School.

I believe a formal agreement is required between the Uniting Church and Prescott Northern School in regard to overflow of cars on the property. How will school students be managed and safely controlled to cross the busy Nelson Road before and after school hours. White red safety rails be placed on the edge of Nelson Road to enable steps to cross the road.

We do not want queuing of cars on Heather Drive and Lorraine Drive. We at present have cars driving along these roads after collecting students from Para Vista Primary School.

Do you believe the analysis of the proposal by Traffic Engineer is to be correct and would have a significant impact reducing traffic congestion on Nelson Road / Wright Road Roundabout.

I have noticed a lot of car queuing on Montague Road before they collect students from Good Shepherd Lutheran School Para Vista. What analysis and traffic reports have they used.

I believe careful consideration is needed before this application is approved.

The development if not researched correctly would result on major traffic problems. Local residents would be effected by the increase in the impact of the proposed development of the school.

Our quality of life would be effected by vehicles being parked on the streets. Parents pushing to collect their children from school.

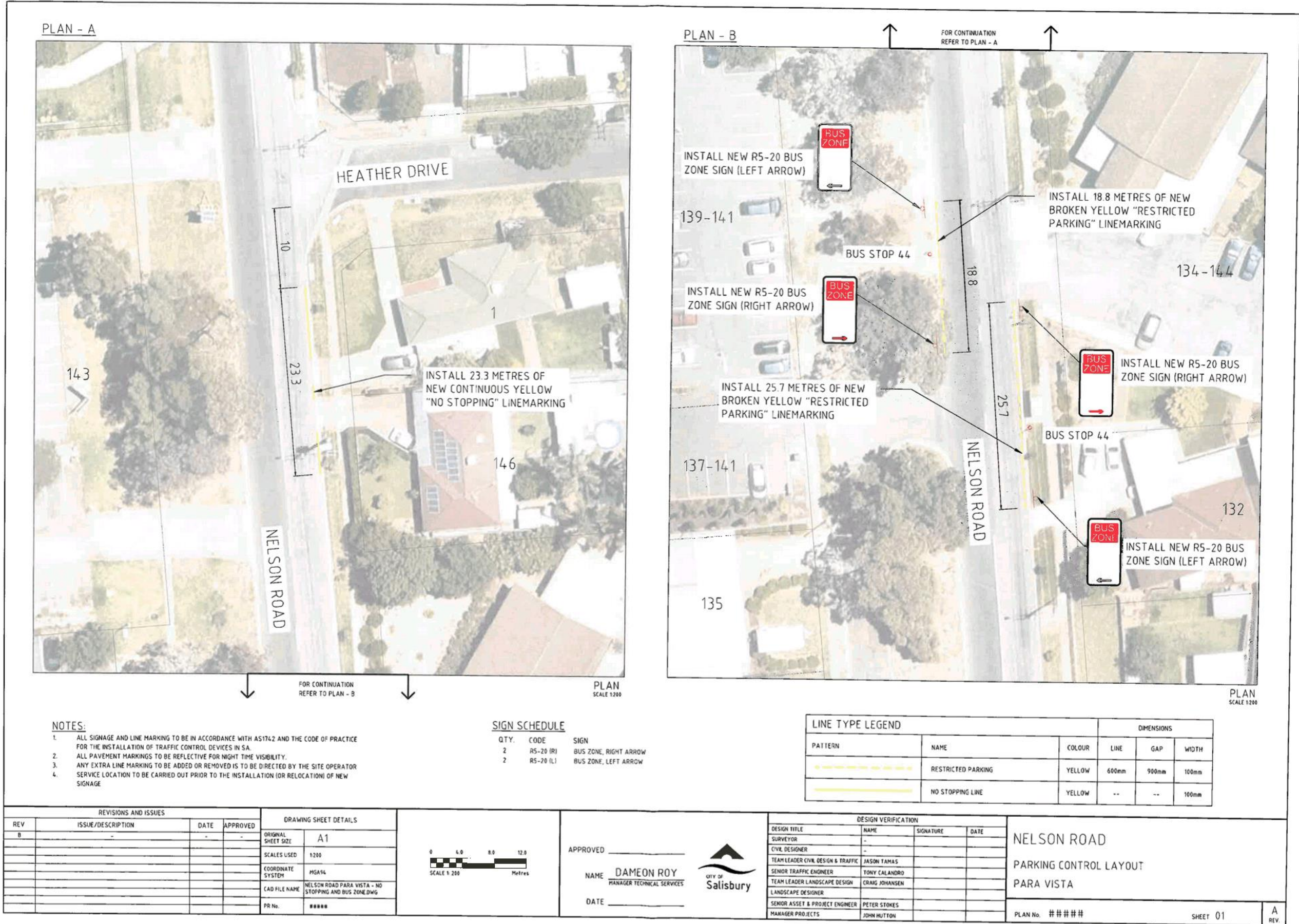
Yours Sincerely  
T. Fielder  
T. FIELDER

# **Attachment 3**

## **Council On-Street Car Parking Control Plan**









**Attachment 4**  
Copies of Applicant's CIRQA  
Traffic and Parking Report and  
Supplementary Advice



**Ref: 18100|BNW**

7 June 2018

Mr David Kilpatrick  
Kilpatrick Architecture  
24 Divett Place  
ADELAIDE SA 5000

Dear David,

### **PRESCOTT PRIMARY NORTHERN PROPOSED EXPANSION**

I refer to the proposed expansion in student numbers at the Prescott Primary Northern School at Wright Road, Para Vista. As requested, I have undertaken a review of traffic impacts associated with the proposed additional student numbers.

#### **BACKGROUND**

The subject site is located on the north-eastern side of the Nelson Road / Wright Road roundabout, Para Vista. The school is currently accessed via an access point on Nelson Road as well as via a service road adjacent to Wright Road. The school has a current student population of 415 students.

A previous traffic impact assessment was prepared by CIRQA in relation to proposed transport infrastructure upgrades for the school. Specifically, it is proposed to construct a new set-down/pick-up facility within the site with ingress via Nelson Road and egress via Wright Road (to ease existing congestion experienced on and adjacent the site). It is understood that the proposed set-down/pick-up facility has been approved by Council and DPTI. The previous assessment indicated these access upgrades will improve conditions at the site with reduced congestion at the site's access points and within the associated parking areas.

It is proposed to construct new learning space referred to as the North Wing (DA 361/334/2018/3B) which will accommodate an increased student population of 440 students. As part of community consultation process for the application, six representations were received. Two of the representors were in support of the proposal, whereas the remaining four representors raised concerns including traffic impacts associated with the proposal.

---

CIRQA Pty Ltd | ABN: 12 681 029 983 | PO Box 144, Glenside SA 5065 | M: 0412 835 711 | E: info@cirqa.com.au  
CIRQA\Projects\18100 David Kilpatrick 21MAY18



In addition, by 2023 the School's Master Plan identified a potential for up to 550 students (albeit does not form part of the current application). Accordingly, an updated traffic assessment has been prepared to review the impacts of the proposed increases in student numbers (both interim and ultimate). The assessment has been prepared based on the previous analysis prepared by CIRQA as part of the set-down/pick-up facility assessment.

It should be noted that the increase to 550 students does not form part of the current application. As part of the Master Plan, additional works would be undertaken on site to accommodate the ultimate increase which would be the subject of further development applications (and associated traffic and parking assessments). Of particular note, the School's Master Plan has considered the ability to provide additional parking on-site to accommodate additional demands associated with the ultimate student population. This will be subject to further design inputs and analysis as and when proposed.

#### **TRAFFIC GENERATION & DISTRIBUTION**

Previous surveys undertaken at the site identified peak hour traffic generation rates of 0.85 trips and 0.49 trips per student in the am and pm peak hours, respectively (including staff movements). Applying the above rates to the proposed student increases results in the following theoretical traffic generation associated with the two population scenarios:

- **Current (North-Wing) Proposal** – 440 students will result in an additional 21 am and 12 pm peak hour movements; and
- **Ultimate (2023) Student Population** – up to 550 students will result in an additional 115 am and 67 pm peak hour movements (compared to existing volumes).

Based on the existing data observed at the site's access points and the intersection of Nelson Road/Wright Road, a forecast distribution of the above additional volumes has been prepared. Figures 1 and 2 illustrate the forecast future volumes at the access points and adjacent intersections as a result of the two increased student population scenarios.

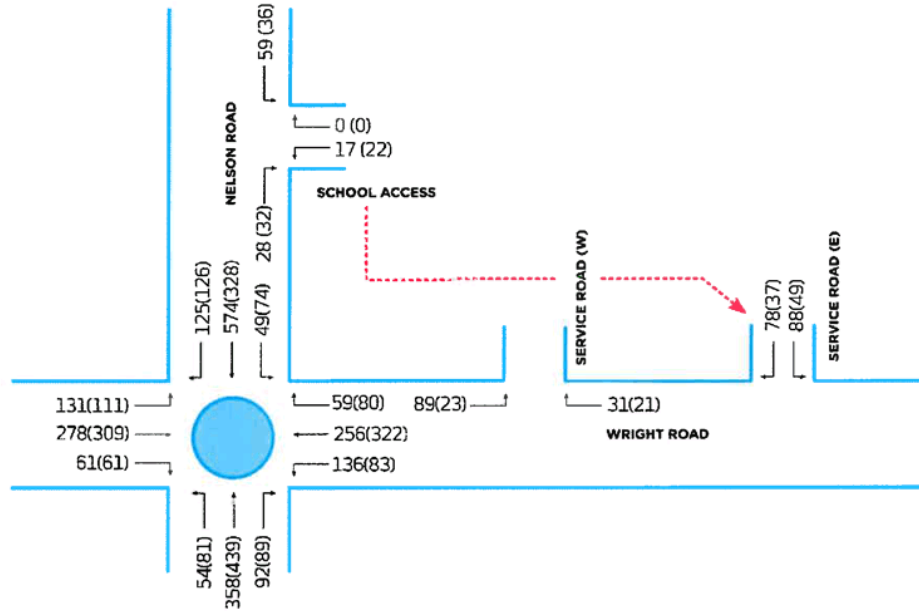


Figure 1 - Future am and (pm) traffic peak hour volumes based on 440 student population

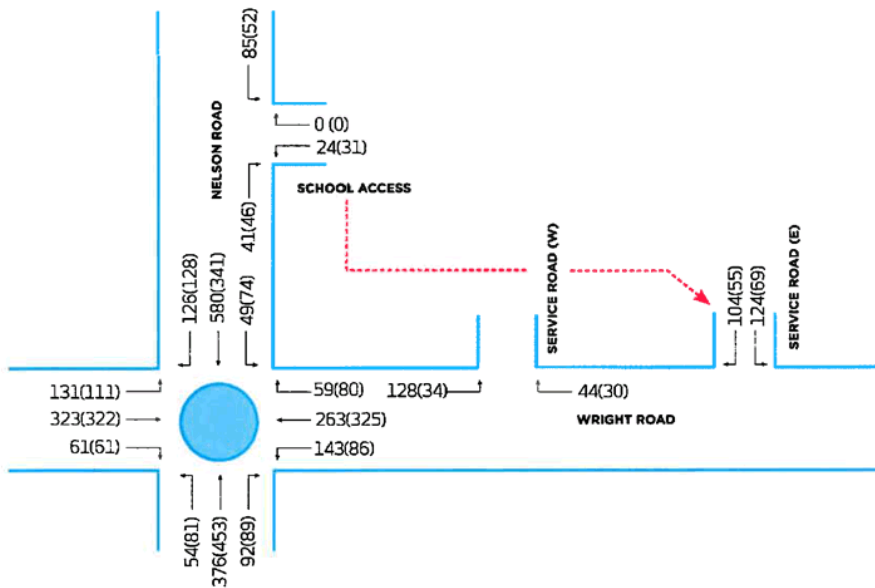


Figure 2 - Future traffic volumes based on 550 student population



In reality, the above forecasts are likely to be conservative, given that increases in student numbers would be expected to result in 'economies of scale' which would reduce the overall traffic generation. For instance, the likelihood of siblings between year levels would be expected to increase and the actual traffic generation rate (per student) would be likely to be lower than theoretically assessed above. Nevertheless, these forecasts have been adopted to provide a conservative assessment.

### IMPACT ASSESSMENT

Based on the above forecast traffic volumes, updated SIDRA intersection analysis has been prepared for the site access points and adjacent intersections. The results of the SIDRA analysis are attached to this letter, however key outcomes are as follows:

- the Nelson Road access point and associated connection to the new set-down/pick-up facility will operate with acceptable levels of queuing and delays;
- there will be minimal change in Degrees of Saturation on the modelled network during both the am and pm periods;
- there will be increases in queues experienced on the Nelson Road (north) approach to the roundabout, however delays will remain similar to existing conditions; and
- there will be increases in queues and delays for the Wright Road (east) approach to the roundabout, however the increases will be low (in the order of two additional vehicles and two to three additional seconds in both peak hours as a result of the increase to 550 students); and
- while there will be increased traffic volumes generated by the additional student numbers, the impact is mitigated (compared to the current situation) by the approved internal access and parking upgrades to be undertaken. Of particular note, the SIDRA modelling suggests conditions will improved during the pm peak hour, even with the 550 student population, compared to current conditions.

On the basis of the SIDRA analysis, the additional traffic volumes associated with the proposed student increases will be able to be accommodated on the adjacent road network. The primary impact will be increased queues on the Nelson Road northern approach to the roundabout during the am peak hour. However, there are already relatively long queues associated with this approach and conditions associated with the school (and increased student numbers) will occur for relatively short periods. Of particular note, the SIDRA analysis indicates that the modelled network's Level of Service will remain as per existing levels (i.e. the additional traffic generation will not significantly impact existing conditions given the upgrades to be undertaken).

In addition to the SIDRA modelling, it is noted that the approved access and parking upgrades to be undertaken at the school will not only improve congestion issues at the site's Nelson Road access point, but will also improve safety through the reduction in the



number of conflict points (turning movements) on Nelson Road. The improvements to the site's access arrangements will mitigate the impact of the additional volumes.

#### SUMMARY

It is proposed to increase the student population at Prescott Primary Northern. The initial increase (associated with the current development application) will result in a population of 440 students (an additional 25 students) and, by 2023, it is anticipated that this could increase up to 550 students (albeit the ultimate increase does not form part of the current application).

A traffic generation and impact assessment has been prepared on the basis of the additional student numbers proposed (both for the current application and ultimate population). The assessment has also taken into account the approved set-down/pick-up facility to be constructed between the site's Nelson Road and Wright Road access points.

The analysis indicates that the additional traffic generated as a result of student population increase to 440 students (current application) will be very low. These movements will be easily accommodated on the adjacent road network, particularly given the approved access and parking upgrades. Ultimately, it is proposed to further increase the student population which will generate further increases in traffic movements associated with the school. Nevertheless, even based on the ultimate student population, the assessment indicates that the additional movements associated with the School will generally retain existing conditions. Nevertheless, the peak periods associated with the School occur for relatively short periods during both the am and pm peak hours. Furthermore, the approved upgrades will improve traffic safety conditions associated with access to and from the school and ameliorate the traffic impact of additional student numbers.

Please feel free to contact me on 0412 835 711 should you require any additional information.

Yours sincerely,

A handwritten signature in blue ink, appearing to read "Ben Wilson".

**BEN WILSON**  
Director | CIRQA Pty Ltd

Enc - SIDRA analysis output sheets

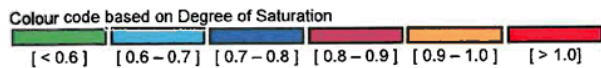
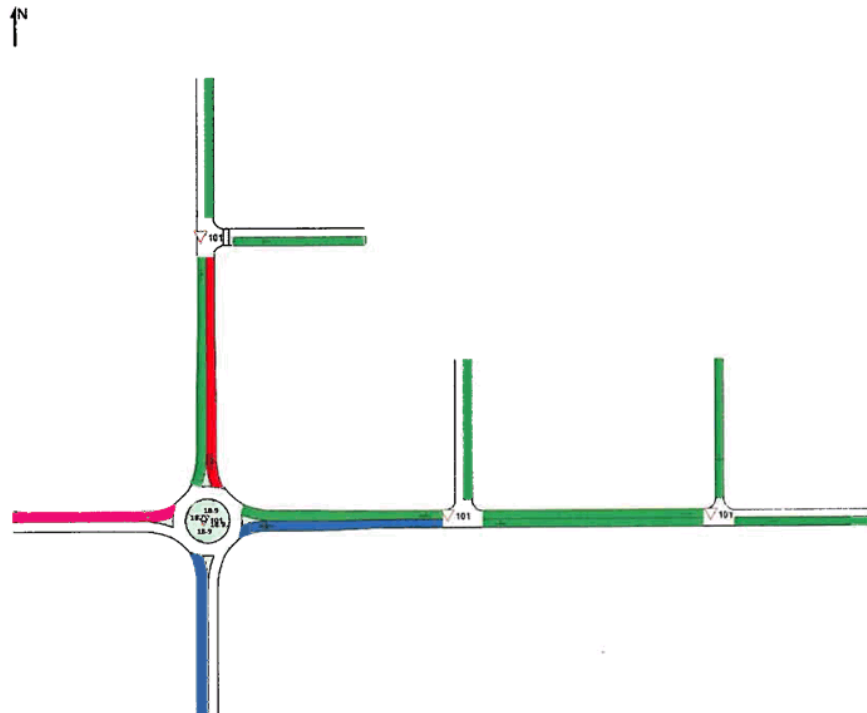


### DEGREE OF SATURATION

Ratio of Demand Volume to Capacity (v/c ratio)

Network: 1 [AM Network]

New Network



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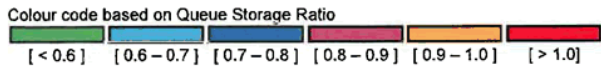
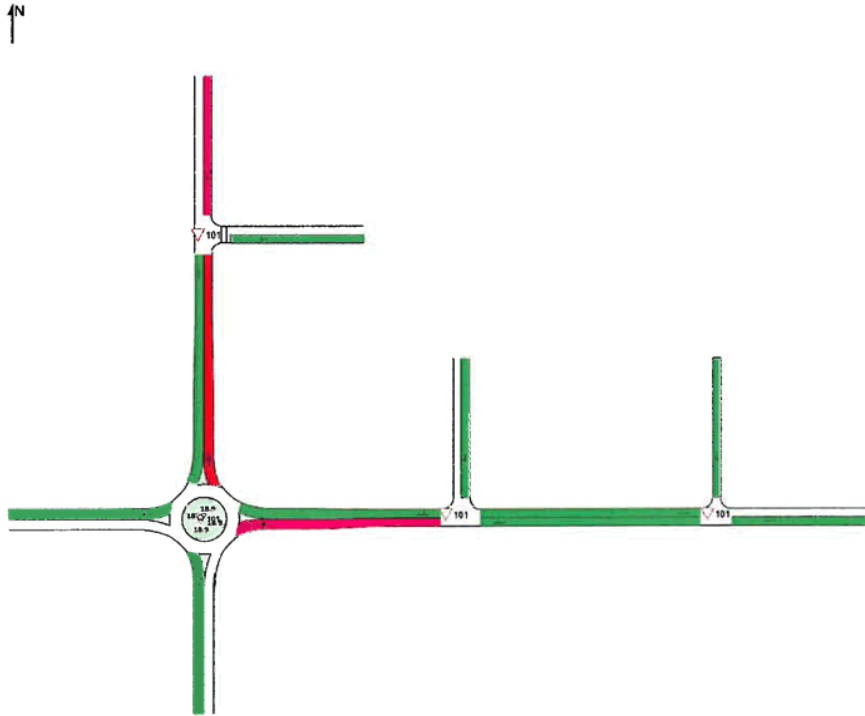
Item 5.1.2 - Attachment 4 - Copies of Applicant's CIRQA Traffic and Parking Report and Supplementary Advice

### QUEUE DISTANCE (%ILE)

95% Back of Queue Distance per lane (metres)

## Network: 1 [AM Network]

New Network



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### NETWORK SUMMARY

## Network: 1 [AM Network]

New Network

Network Performance - Hourly Values			
Performance Measure	Vehicles	Per Unit Distance	Persons
Network Level of Service (LOS)	LOS E		
Travel Time Index	4.14		
Speed Efficiency	0.47		
Congestion Coefficient	2.12		
Travel Speed (Average)	23.6 km/h		23.6 km/h
Travel Distance (Total)	2456.4 veh-km/h		2947.7 pers-km/h
Travel Time (Total)	104.0 veh-h/h		124.7 pers-h/h
Desired Speed	50.0 km/h		
Demand Flows (Total for all Sites)	5032 veh/h		6038 pers/h
Arrival Flows (Total for all Sites)	4999 veh/h		5999 pers/h
Demand Flows (Entry Total)	2194 veh/h		
Midblock Inflows (Total)	40 veh/h		
Midblock Outflows (Total)	-12 veh/h		
Percent Heavy Vehicles (Demand)	2.2 %		
Percent Heavy Vehicles (Arrival)	2.2 %		
Degree of Saturation	1.531		
Control Delay (Total)	56.33 veh-h/h		67.59 pers-h/h
Control Delay (Average)	40.6 sec		40.6 sec
Control Delay (Worst Lane)	257.9 sec		
Control Delay (Worst Movement)	261.5 sec		261.5 sec
Geometric Delay (Average)	1.8 sec		
Stop-Line Delay (Average)	38.7 sec		
Queue Storage Ratio (Worst Lane)	1.00		
Total Effective Stops	4487 veh/h		5385 pers/h
Effective Stop Rate	0.90 per veh	1.8 per km	0.90 per pers
Proportion Queued	0.44		0.44
Performance Index	340.4		340.4
Cost (Total)	3212.82 \$/h	1.31 \$/km	3212.82 \$/h
Fuel Consumption (Total)	289.2 L/h	117.7 mL/km	
Fuel Economy	11.8 L/100km		
Carbon Dioxide (Total)	683.2 kg/h	278.1 g/km	
Hydrocarbons (Total)	0.064 kg/h	0.026 g/km	
Carbon Monoxide (Total)	0.602 kg/h	0.245 g/km	
NOx (Total)	0.708 kg/h	0.288 g/km	

Largest change in Average Back of Queue or Degree of Saturation for any lane during the last three iterations: 3.6 %

Number of Iterations: 10 (maximum specified: 10)

Network Level of Service (LOS) Method: SIDRA Speed Efficiency.

Software Setup used: Standard Left.

Network Performance - Annual Values			
Performance Measure	Vehicles	Persons	
Demand Flows (Total for all Sites)	2,415,158 veh/y	2,898,190 pers/y	
Delay	27,038 veh-h/y	32,446 pers-h/y	
Effective Stops	2,153,811 veh/y	2,584,573 pers/y	
Travel Distance	1,179,069 veh-km/y	1,414,883 pers-km/y	
Travel Time	49,898 veh-h/y	59,878 pers-h/y	
Cost	1,542,154 \$/y	1,542,154 \$/y	
Fuel Consumption	138,818 L/y		
Carbon Dioxide	327,943 kg/y		

Hydrocarbons	31 kg/y
Carbon Monoxide	289 kg/y
NOx	340 kg/y

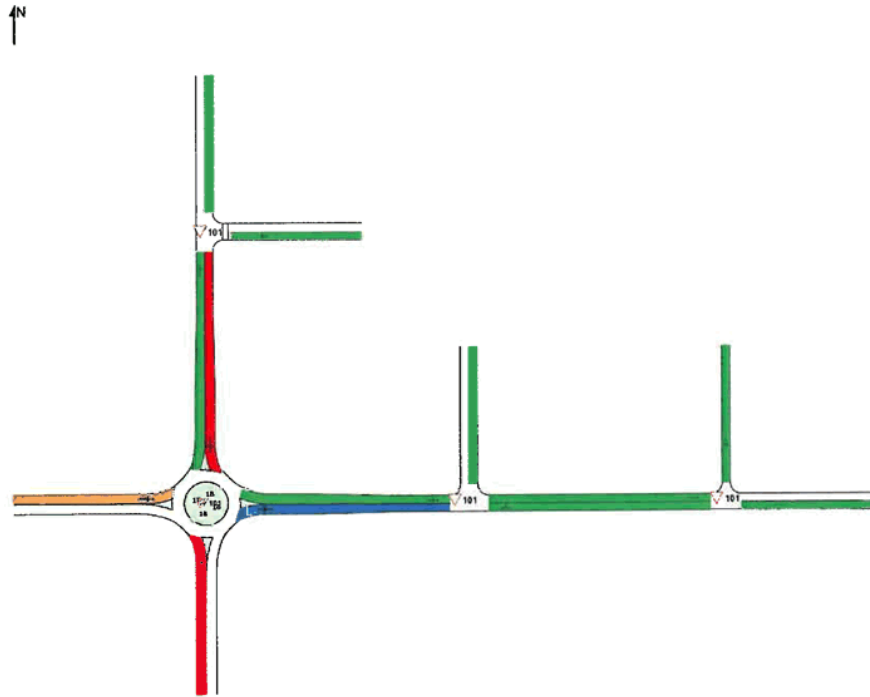
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### DEGREE OF SATURATION

Ratio of Demand Volume to Capacity (v/c ratio)

Network: 1 [PM Network]

New Network



Colour code based on Degree of Saturation



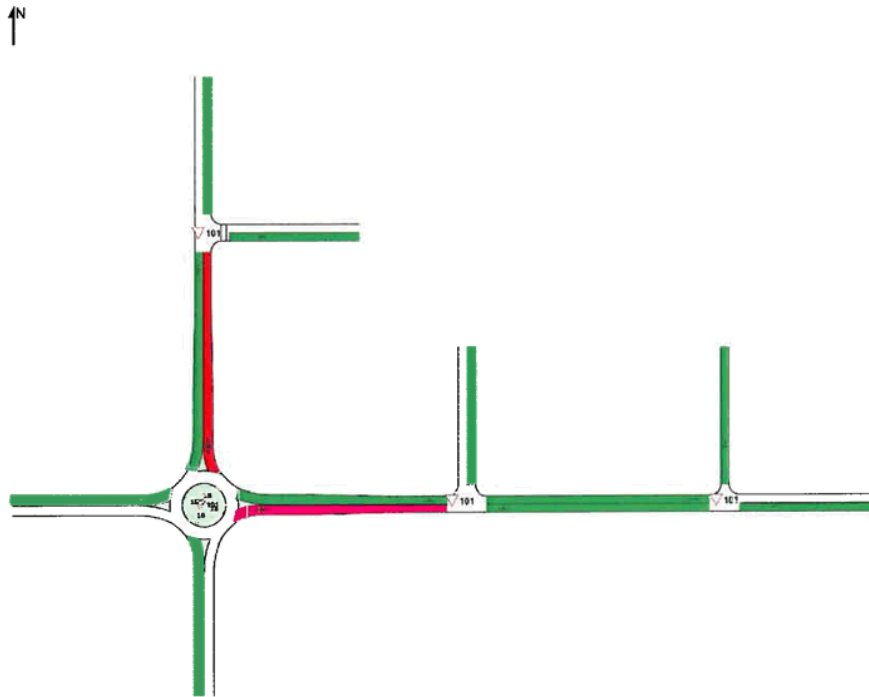
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### QUEUE DISTANCE (%ILE)

95% Back of Queue Distance per lane (metres)

Network: 1 [PM Network]

New Network



Colour code based on Queue Storage Ratio



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### NETWORK SUMMARY

## Network: 1 [PM Network]

New Network

Network Performance - Hourly Values			
Performance Measure	Vehicles	Per Unit Distance	Persons
Network Level of Service (LOS)	LOS D		
Travel Time Index	4.77		
Speed Efficiency	0.53		
Congestion Coefficient	1.89		
Travel Speed (Average)	26.5 km/h		26.5 km/h
Travel Distance (Total)	2435.3 veh-km/h		2922.4 pers-km/h
Travel Time (Total)	92.0 veh-h/h		110.4 pers-h/h
Desired Speed	50.0 km/h		
Demand Flows (Total for all Sites)	5233 veh/h		6279 pers/h
Arrival Flows (Total for all Sites)	5173 veh/h		6208 pers/h
Demand Flows (Entry Total)	2179 veh/h		
Midblock Inflows (Total)	15 veh/h		
Midblock Outflows (Total)	-12 veh/h		
Percent Heavy Vehicles (Demand)	2.2 %		
Percent Heavy Vehicles (Arrival)	2.3 %		
Degree of Saturation	1.345		
Control Delay (Total)	44.88 veh-h/h		53.85 pers-h/h
Control Delay (Average)	31.2 sec		31.2 sec
Control Delay (Worst Lane)	179.6 sec		
Control Delay (Worst Movement)	182.9 sec		182.9 sec
Geometric Delay (Average)	1.6 sec		
Stop-Line Delay (Average)	29.6 sec		
Queue Storage Ratio (Worst Lane)	1.00		
Total Effective Stops	4375 veh/h		5250 pers/h
Effective Stop Rate	0.85 per veh	1.8 per km	0.85 per pers
Proportion Queued	0.43		0.43
Performance Index	363.6		363.6
Cost (Total)	2826.25 \$/h	1.16 \$/km	2826.25 \$/h
Fuel Consumption (Total)	270.9 L/h	111.2 mL/km	
Fuel Economy	11.1 L/100km		
Carbon Dioxide (Total)	640.0 kg/h	262.8 g/km	
Hydrocarbons (Total)	0.058 kg/h	0.024 g/km	
Carbon Monoxide (Total)	0.566 kg/h	0.232 g/km	
NOx (Total)	0.676 kg/h	0.278 g/km	

Largest change in Average Back of Queue or Degree of Saturation for any lane during the last three iterations: 3.6 %  
 Number of Iterations: 10 (maximum specified: 10)  
 Network Level of Service (LOS) Method: SIDRA Speed Efficiency.  
 Software Setup used: Standard Left.

Network Performance - Annual Values		
Performance Measure	Vehicles	Persons
Demand Flows (Total for all Sites)	2,511,664 veh/y	3,013,996 pers/y
Delay	21,542 veh-h/y	25,850 pers-h/y
Effective Stops	2,100,146 veh/y	2,520,176 pers/y
Travel Distance	1,168,962 veh-km/y	1,402,754 pers-km/y
Travel Time	44,161 veh-h/y	52,994 pers-h/y
Cost	1,356,602 \$/y	1,356,602 \$/y
Fuel Consumption	130,015 L/y	
Carbon Dioxide	307,192 kg/y	

Hydrocarbons	28 kg/y
Carbon Monoxide	272 kg/y
NOx	325 kg/y

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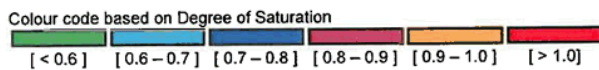
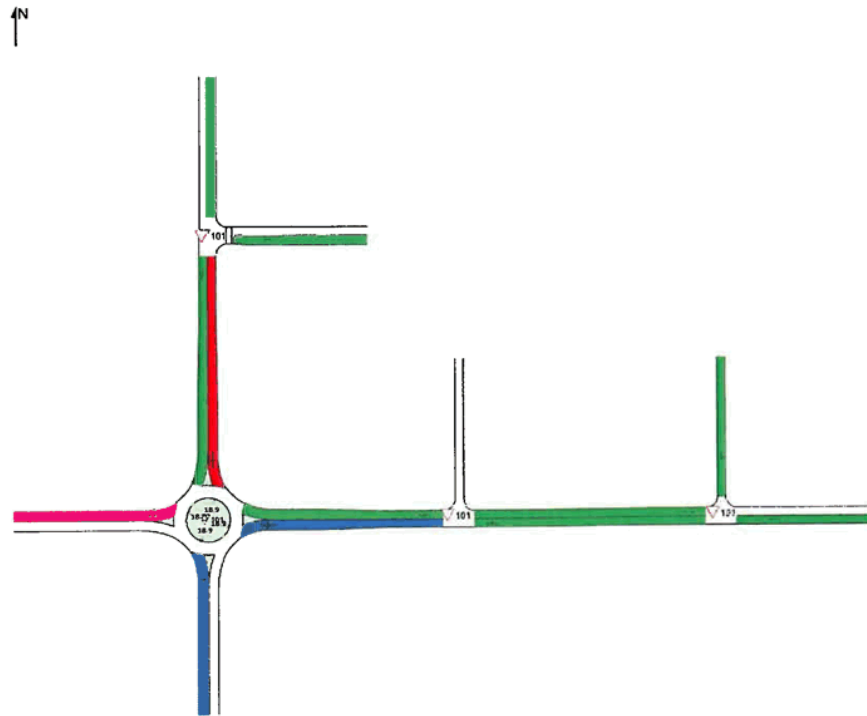


### DEGREE OF SATURATION

Ratio of Demand Volume to Capacity (v/c ratio)

Network: 1 [AM Network]

New Network



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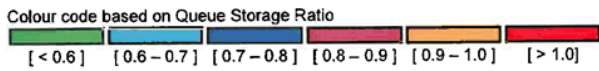
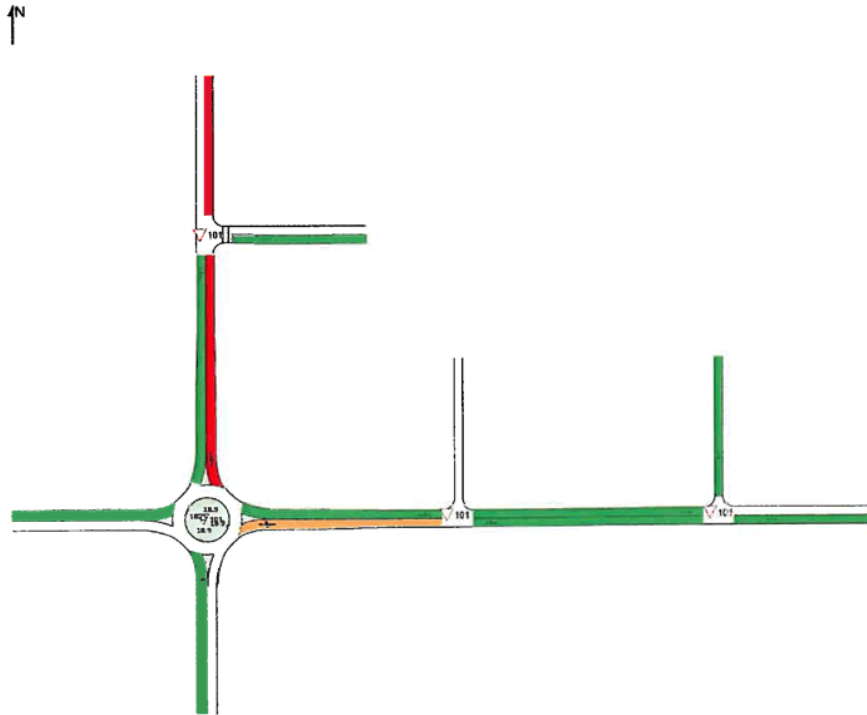
Item 5.1.2 - Attachment 4 - Copies of Applicant's CIRQA Traffic and Parking Report and Supplementary Advice

**QUEUE DISTANCE (%ILE)**

95% Back of Queue Distance per lane (metres)

## Network: 1 [AM Network]

New Network



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### NETWORK SUMMARY

Network: 1 [AM Network]

New Network

Network Performance - Hourly Values			
Performance Measure	Vehicles	Per Unit Distance	Persons
Network Level of Service (LOS)	LOS E		
Travel Time Index	3.48		
Speed Efficiency	0.41		
Congestion Coefficient	2.42		
Travel Speed (Average)	20.7 km/h		20.7 km/h
Travel Distance (Total)	2559.7 veh-km/h		3071.7 pers-km/h
Travel Time (Total)	123.8 veh-h/h		148.6 pers-h/h
Desired Speed	50.0 km/h		
Demand Flows (Total for all Sites)	5192 veh/h		6230 pers/h
Arrival Flows (Total for all Sites)	5157 veh/h		6188 pers/h
Demand Flows (Entry Total)	2222 veh/h		
Midblock Inflows (Total)	142 veh/h		
Midblock Outflows (Total)	-16 veh/h		
Percent Heavy Vehicles (Demand)	2.2 %		
Percent Heavy Vehicles (Arrival)	2.2 %		
Degree of Saturation	1.669		
Control Delay (Total)	74.11 veh-h/h		88.93 pers-h/h
Control Delay (Average)	51.7 sec		51.7 sec
Control Delay (Worst Lane)	318.4 sec		
Control Delay (Worst Movement)	322.0 sec		322.0 sec
Geometric Delay (Average)	1.9 sec		
Stop-Line Delay (Average)	49.9 sec		
Queue Storage Ratio (Worst Lane)	1.00		
Total Effective Stops	5154 veh/h		6185 pers/h
Effective Stop Rate	1.00 per veh	2.0 per km	1.00 per pers
Proportion Queued	0.45		0.45
Performance Index	399.0		399.0
Cost (Total)	3849.55 \$/h	1.50 \$/km	3849.55 \$/h
Fuel Consumption (Total)	323.2 L/h	126.3 mL/km	
Fuel Economy	12.6 L/100km		
Carbon Dioxide (Total)	763.5 kg/h	298.3 g/km	
Hydrocarbons (Total)	0.074 kg/h	0.029 g/km	
Carbon Monoxide (Total)	0.671 kg/h	0.262 g/km	
NOx (Total)	0.777 kg/h	0.303 g/km	

Largest change in Average Back of Queue or Degree of Saturation for any lane during the last three iterations: 3.8 %  
 Number of Iterations: 10 (maximum specified: 10)  
 Network Level of Service (LOS) Method: SIDRA Speed Efficiency.  
 Software Setup used: Standard Left.

Network Performance - Annual Values		
Performance Measure	Vehicles	Persons
Demand Flows (Total for all Sites)	2,491,958 veh/y	2,990,350 pers/y
Delay	35,573 veh-h/y	42,688 pers-h/y
Effective Stops	2,474,114 veh/y	2,968,937 pers/y
Travel Distance	1,228,671 veh-km/y	1,474,406 pers-km/y
Travel Time	59,438 veh-h/y	71,325 pers-h/y
Cost	1,847,786 \$/y	1,847,786 \$/y
Fuel Consumption	155,150 L/y	
Carbon Dioxide	366,482 kg/y	

Hydrocarbons	35 kg/y
Carbon Monoxide	322 kg/y
NOx	373 kg/y

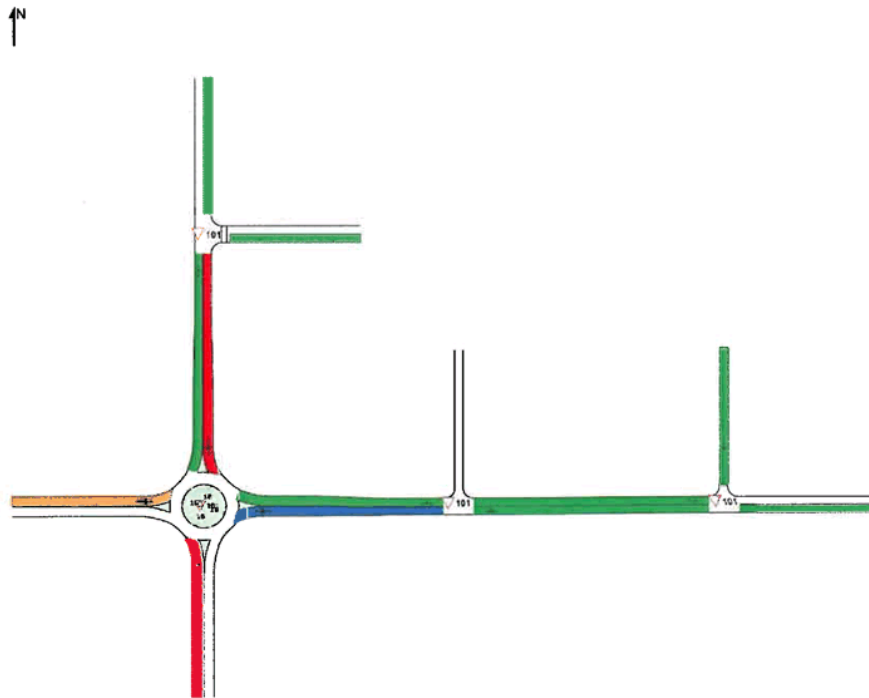
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### DEGREE OF SATURATION

Ratio of Demand Volume to Capacity (v/c ratio)

## Network: 1 [PM Network]

New Network



Colour code based on Degree of Saturation



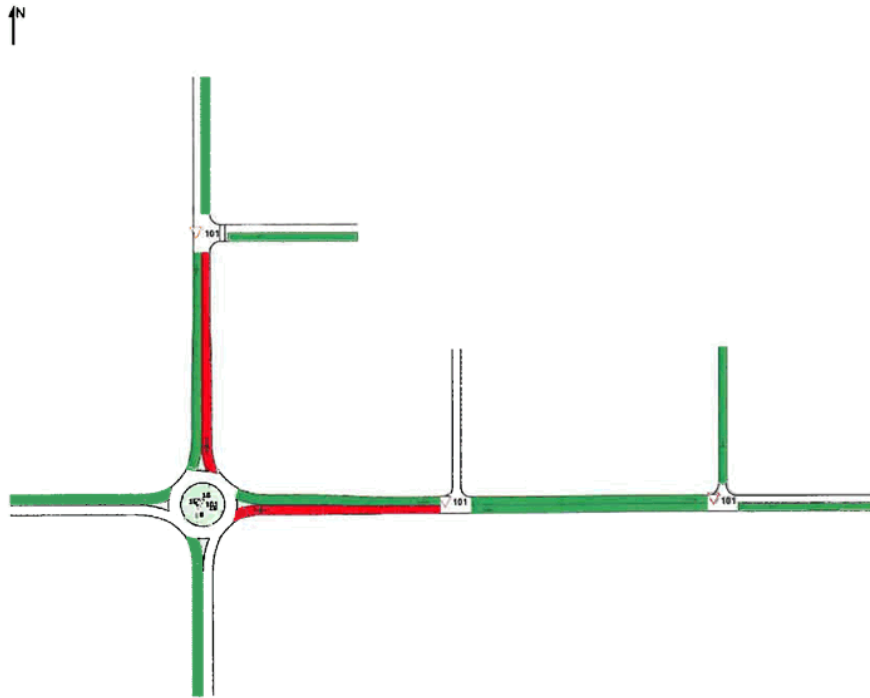
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### QUEUE DISTANCE (%ILE)

95% Back of Queue Distance per lane (metres)

## Network: 1 [PM Network]

New Network



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### NETWORK SUMMARY

## Network: 1 [PM Network]

New Network

Network Performance - Hourly Values			
Performance Measure	Vehicles	Per Unit Distance	Persons
Network Level of Service (LOS)	LOS D		
Travel Time Index	5.12		
Speed Efficiency	0.56		
Congestion Coefficient	1.78		
Travel Speed (Average)	28.0 km/h		28.0 km/h
Travel Distance (Total)	2466.0 veh-km/h		2959.2 pers-km/h
Travel Time (Total)	88.0 veh-h/h		105.6 pers-h/h
Desired Speed	50.0 km/h		
Demand Flows (Total for all Sites)	5268 veh/h		6322 pers/h
Arrival Flows (Total for all Sites)	5197 veh/h		6236 pers/h
Demand Flows (Entry Total)	2187 veh/h		
Midblock Inflows (Total)	43 veh/h		
Midblock Outflows (Total)	-11 veh/h		
Percent Heavy Vehicles (Demand)	2.2 %		
Percent Heavy Vehicles (Arrival)	2.3 %		
Degree of Saturation	1.265		
Control Delay (Total)	40.49 veh-h/h		48.59 pers-h/h
Control Delay (Average)	28.1 sec		28.1 sec
Control Delay (Worst Lane)	146.3 sec		
Control Delay (Worst Movement)	149.5 sec		149.5 sec
Geometric Delay (Average)	1.7 sec		
Stop-Line Delay (Average)	26.4 sec		
Queue Storage Ratio (Worst Lane)	1.00		
Total Effective Stops	4279 veh/h		5134 pers/h
Effective Stop Rate	0.82 per veh	1.7 per km	0.82 per pers
Proportion Queued	0.43		0.43
Performance Index	360.9		360.9
Cost (Total)	2692.39 \$/h	1.09 \$/km	2692.39 \$/h
Fuel Consumption (Total)	266.8 L/h	108.2 mL/km	
Fuel Economy	10.8 L/100km		
Carbon Dioxide (Total)	630.4 kg/h	255.6 g/km	
Hydrocarbons (Total)	0.056 kg/h	0.023 g/km	
Carbon Monoxide (Total)	0.562 kg/h	0.228 g/km	
NOx (Total)	0.662 kg/h	0.269 g/km	

Largest change in Average Back of Queue or Degree of Saturation for any lane during the last three iterations: 3.8 %  
 Number of Iterations: 10 (maximum specified: 10)  
 Network Level of Service (LOS) Method: SIDRA Speed Efficiency.  
 Software Setup used: Standard Left.

Network Performance - Annual Values		
Performance Measure	Vehicles	Persons
Demand Flows (Total for all Sites)	2,528,842 veh/y	3,034,611 pers/y
Delay	19,438 veh-h/y	23,325 pers-h/y
Effective Stops	2,053,794 veh/y	2,464,552 pers/y
Travel Distance	1,183,682 veh-km/y	1,420,419 pers-km/y
Travel Time	42,224 veh-h/y	50,669 pers-h/y
Cost	1,292,349 \$/y	1,292,349 \$/y
Fuel Consumption	128,071 L/y	
Carbon Dioxide	302,580 kg/y	

Hydrocarbons	27 kg/y
Carbon Monoxide	270 kg/y
NOx	318 kg/y

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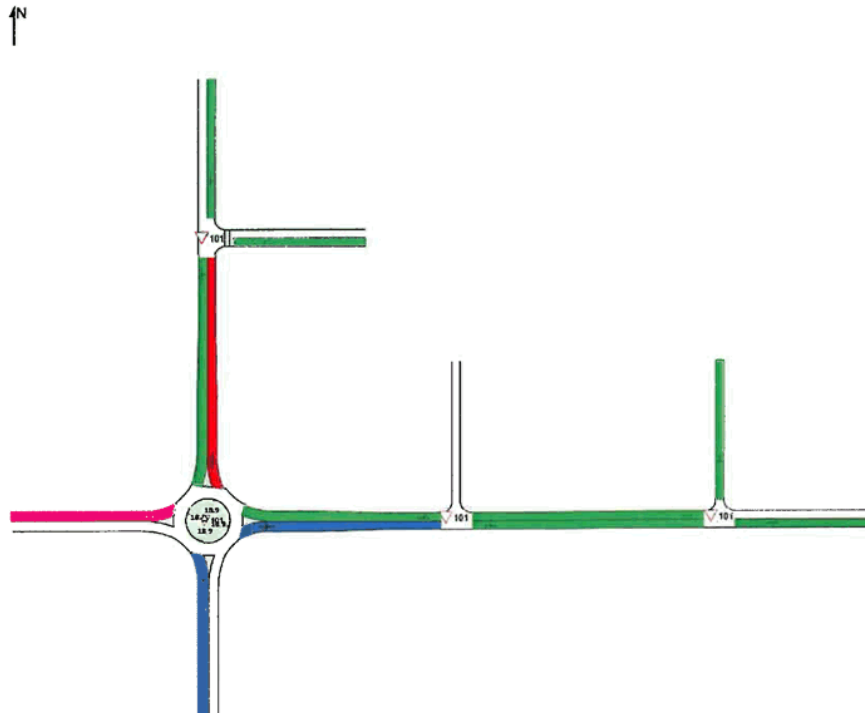


### DEGREE OF SATURATION

Ratio of Demand Volume to Capacity (v/c ratio)

## Network: 1 [AM Network]

New Network



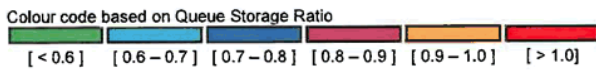
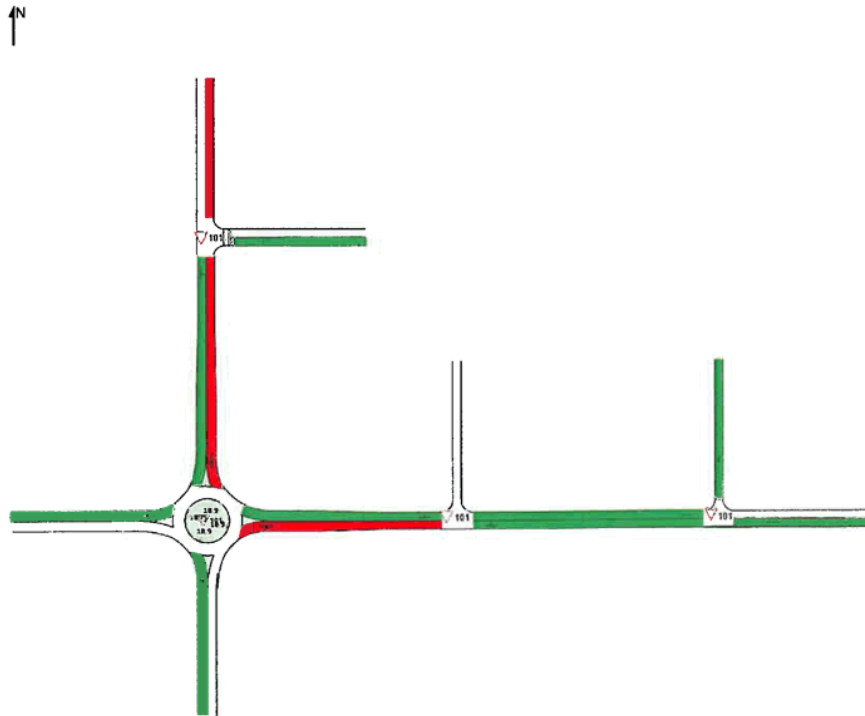
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### QUEUE DISTANCE (%ILE)

95% Back of Queue Distance per lane (metres)

Network: 1 [AM Network]

New Network



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**NETWORK SUMMARY**

## Network: 1 [AM Network]

New Network

Network Performance - Hourly Values			
Performance Measure	Vehicles	Per Unit Distance	Persons
Network Level of Service (LOS)	LOS E		
Travel Time Index	3.29		
Speed Efficiency	0.40		
Congestion Coefficient	2.53		
Travel Speed (Average)	19.8 km/h		19.8 km/h
Travel Distance (Total)	2654.8 veh-km/h		3185.8 pers-km/h
Travel Time (Total)	134.2 veh-h/h		161.0 pers-h/h
Desired Speed	50.0 km/h		
Demand Flows (Total for all Sites)	5355 veh/h		6426 pers/h
Arrival Flows (Total for all Sites)	5319 veh/h		6382 pers/h
Demand Flows (Entry Total)	2321 veh/h		
Midblock Inflows (Total)	141 veh/h		
Midblock Outflows (Total)	-13 veh/h		
Percent Heavy Vehicles (Demand)	2.2 %		
Percent Heavy Vehicles (Arrival)	2.2 %		
Degree of Saturation	1.742		
Control Delay (Total)	82.61 veh-h/h		99.13 pers-h/h
Control Delay (Average)	55.9 sec		55.9 sec
Control Delay (Worst Lane)	350.6 sec		
Control Delay (Worst Movement)	354.3 sec		354.3 sec
Geometric Delay (Average)	1.9 sec		
Stop-Line Delay (Average)	54.0 sec		
Queue Storage Ratio (Worst Lane)	1.00		
Total Effective Stops	5479 veh/h		6574 pers/h
Effective Stop Rate	1.03 per veh	2.1 per km	1.03 per pers
Proportion Queued	0.46		0.46
Performance Index	428.2		428.2
Cost (Total)	4190.13 \$/h	1.58 \$/km	4190.13 \$/h
Fuel Consumption (Total)	344.1 L/h	129.6 mL/km	
Fuel Economy	13.0 L/100km		
Carbon Dioxide (Total)	812.7 kg/h	306.1 g/km	
Hydrocarbons (Total)	0.080 kg/h	0.030 g/km	
Carbon Monoxide (Total)	0.715 kg/h	0.269 g/km	
NOx (Total)	0.811 kg/h	0.306 g/km	

Largest change in Average Back of Queue or Degree of Saturation for any lane during the last three iterations: 3.8 %  
 Number of Iterations: 10 (maximum specified: 10)  
 Network Level of Service (LOS) Method: SIDRA Speed Efficiency.  
 Software Setup used: Standard Left.

Network Performance - Annual Values			
Performance Measure	Vehicles	Persons	
Demand Flows (Total for all Sites)	2,570,274 veh/y	3,084,329 pers/y	
Delay	39,653 veh-h/y	47,584 pers-h/y	
Effective Stops	2,629,707 veh/y	3,155,649 pers/y	
Travel Distance	1,274,326 veh-km/y	1,529,191 pers-km/y	
Travel Time	64,400 veh-h/y	77,281 pers-h/y	
Cost	2,011,261 \$/y	2,011,261 \$/y	
Fuel Consumption	165,164 L/y		
Carbon Dioxide	390,077 kg/y		

Hydrocarbons	38 kg/y
Carbon Monoxide	343 kg/y
NOx	390 kg/y

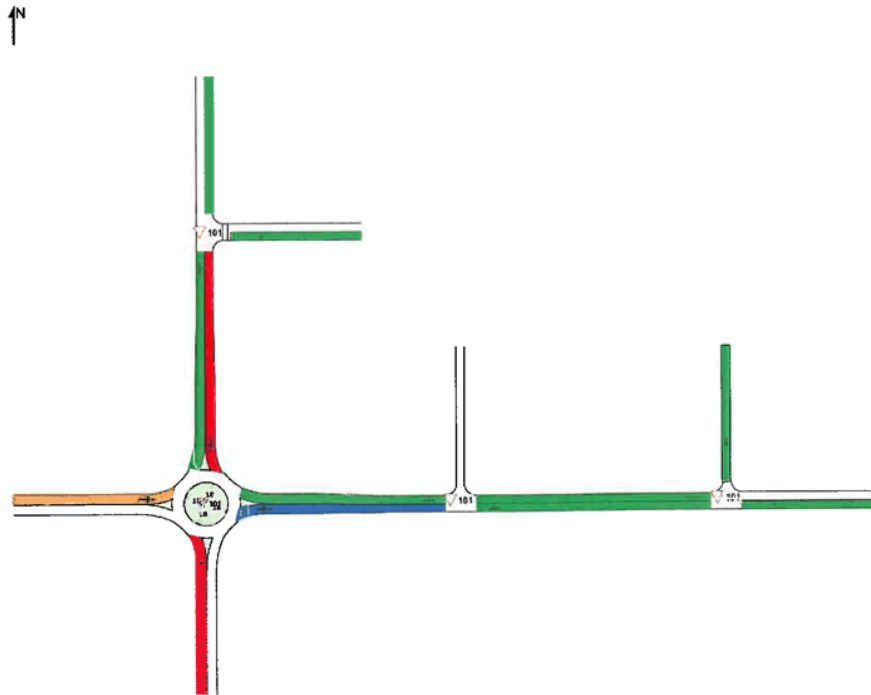
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### DEGREE OF SATURATION

Ratio of Demand Volume to Capacity (v/c ratio)

## Network: 1 [PM Network]

New Network



Colour code based on Degree of Saturation



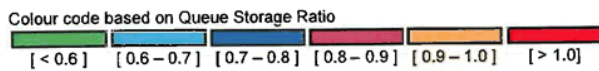
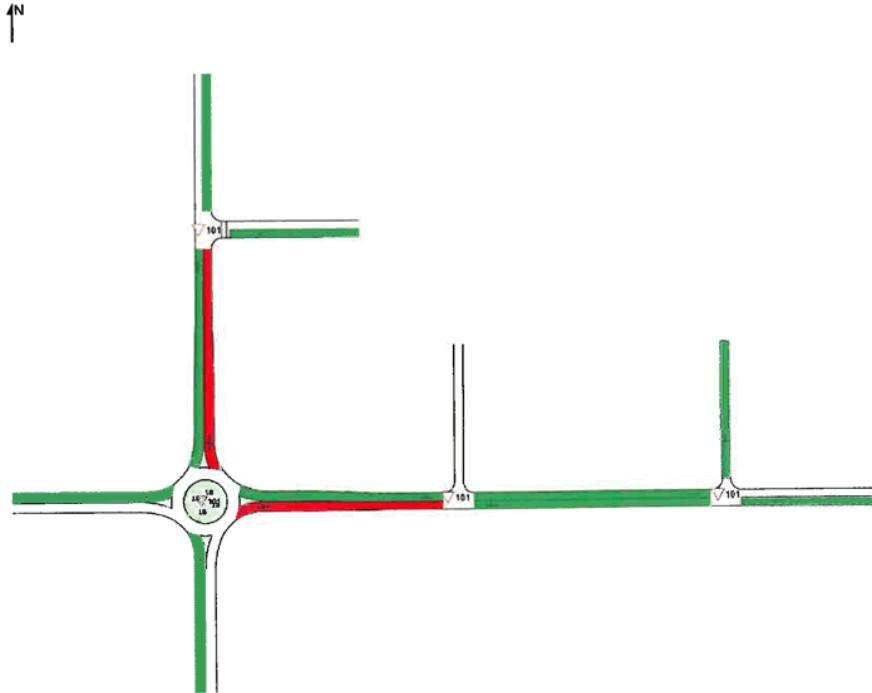
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### QUEUE DISTANCE (%ILE)

95% Back of Queue Distance per lane (metres)

## Network: 1 [PM Network]

New Network



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### NETWORK SUMMARY

#### ## Network: 1 [PM Network]

New Network

Network Performance - Hourly Values			
Performance Measure	Vehicles	Per Unit Distance	Persons
Network Level of Service (LOS)	LOS D		
Travel Time Index	4.93		
Speed Efficiency	0.54		
Congestion Coefficient	1.84		
Travel Speed (Average)	27.2 km/h		27.2 km/h
Travel Distance (Total)	2511.0 veh-km/h		3013.2 pers-km/h
Travel Time (Total)	92.4 veh-h/h		110.9 pers-h/h
Desired Speed	50.0 km/h		
Demand Flows (Total for all Sites)	5359 veh/h		6431 pers/h
Arrival Flows (Total for all Sites)	5274 veh/h		6329 pers/h
Demand Flows (Entry Total)	2244 veh/h		
Midblock Inflows (Total)	42 veh/h		
Midblock Outflows (Total)	-11 veh/h		
Percent Heavy Vehicles (Demand)	2.2 %		
Percent Heavy Vehicles (Arrival)	2.2 %		
Degree of Saturation	1.287		
Control Delay (Total)	44.04 veh-h/h		52.85 pers-h/h
Control Delay (Average)	30.1 sec		30.1 sec
Control Delay (Worst Lane)	155.7 sec		
Control Delay (Worst Movement)	158.9 sec		158.9 sec
Geometric Delay (Average)	1.7 sec		
Stop-Line Delay (Average)	28.4 sec		
Queue Storage Ratio (Worst Lane)	1.00		
Total Effective Stops	4509 veh/h		5411 pers/h
Effective Stop Rate	0.85 per veh	1.8 per km	0.85 per pers
Proportion Queued	0.44		0.44
Performance Index	379.9		379.9
Cost (Total)	2843.78 \$/h	1.13 \$/km	2843.78 \$/h
Fuel Consumption (Total)	276.7 L/h	110.2 mL/km	
Fuel Economy	11.0 L/100km		
Carbon Dioxide (Total)	653.7 kg/h	260.3 g/km	
Hydrocarbons (Total)	0.059 kg/h	0.023 g/km	
Carbon Monoxide (Total)	0.582 kg/h	0.232 g/km	
NOx (Total)	0.680 kg/h	0.271 g/km	

Largest change in Average Back of Queue or Degree of Saturation for any lane during the last three iterations: 3.8 %  
 Number of Iterations: 10 (maximum specified: 10)  
 Network Level of Service (LOS) Method: SIDRA Speed Efficiency.  
 Software Setup used: Standard Left.

Network Performance - Annual Values		
Performance Measure	Vehicles	Persons
Demand Flows (Total for all Sites)	2,572,295 veh/y	3,086,754 pers/y
Delay	21,141 veh-h/y	25,369 pers-h/y
Effective Stops	2,164,435 veh/y	2,597,322 pers/y
Travel Distance	1,205,287 veh-km/y	1,446,344 pers-km/y
Travel Time	44,362 veh-h/y	53,235 pers-h/y
Cost	1,365,014 \$/y	1,365,014 \$/y
Fuel Consumption	132,823 L/y	
Carbon Dioxide	313,771 kg/y	

Hydrocarbons	28 kg/y
Carbon Monoxide	280 kg/y
NOx	327 kg/y

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**Ref: 18100|BNW**

10 October 2018

Mr David Kilpatrick  
Kilpatrick Architecture  
24 Divett Place  
ADELAIDE SA 5000

Dear David,

**PRESCOTT PRIMARY NORTHERN  
PROPOSED ADDITIONAL PARKING**

I refer to the proposed expansion in student numbers at the Prescott Primary Northern School at Wright Road, Para Vista.

CIRQA has provided previous traffic and parking advice for the subject application as detailed in my letter dated 4 September 2018. The previous assessment included consideration of additional student numbers and associated alterations (previously proposed) to access and parking arrangements associated with the School).

In addition to the previously identified on-site set-down/pick-up loop, an additional 11 angled parking spaces are now proposed on the internal loop road. The additional parking spaces shall be provided in accordance with the Australian/New Zealand Standard for "Parking Facilities - Part 1: Off-street car parking" (AS/NZS 2890.1:2004) in that:

- spaces will be angled at 60 degrees to the parking aisle and will be 2.6 m wide depth (measured perpendicular to the travel direction) of 5.1 m; and
- the aisle will be 4.6 m wide (with over 300 mm clearance to the adjacent fence on the western property boundary).

The additional spaces will improve accommodation of parking demands associated with the School on-site and assist with easing congestion currently experienced on the adjacent road network (in addition to the improvements which will result from the provision of the set-down/pick-up loop).

Please feel free to contact me on (08) 7078 1801 should you require any additional information.

---

CIRQA Pty Ltd | ABN: 12 681 029 983 | PO Box 144, Glenside SA 5065 | P: (08) 7078 1801 | E: info@cirqa.com.au  
CIRQA\Projects\18100 David Kilpatrick 10Oct18



Yours sincerely,

A handwritten signature in blue ink, appearing to read "Ben Wilson".

**BEN WILSON**  
Director | CIRQA Pty Ltd

**Attachment 5**  
Relevant Development Plan  
Provisions  
(Consolidated 15 December 2016)

# Development Plan

**Salisbury Council**

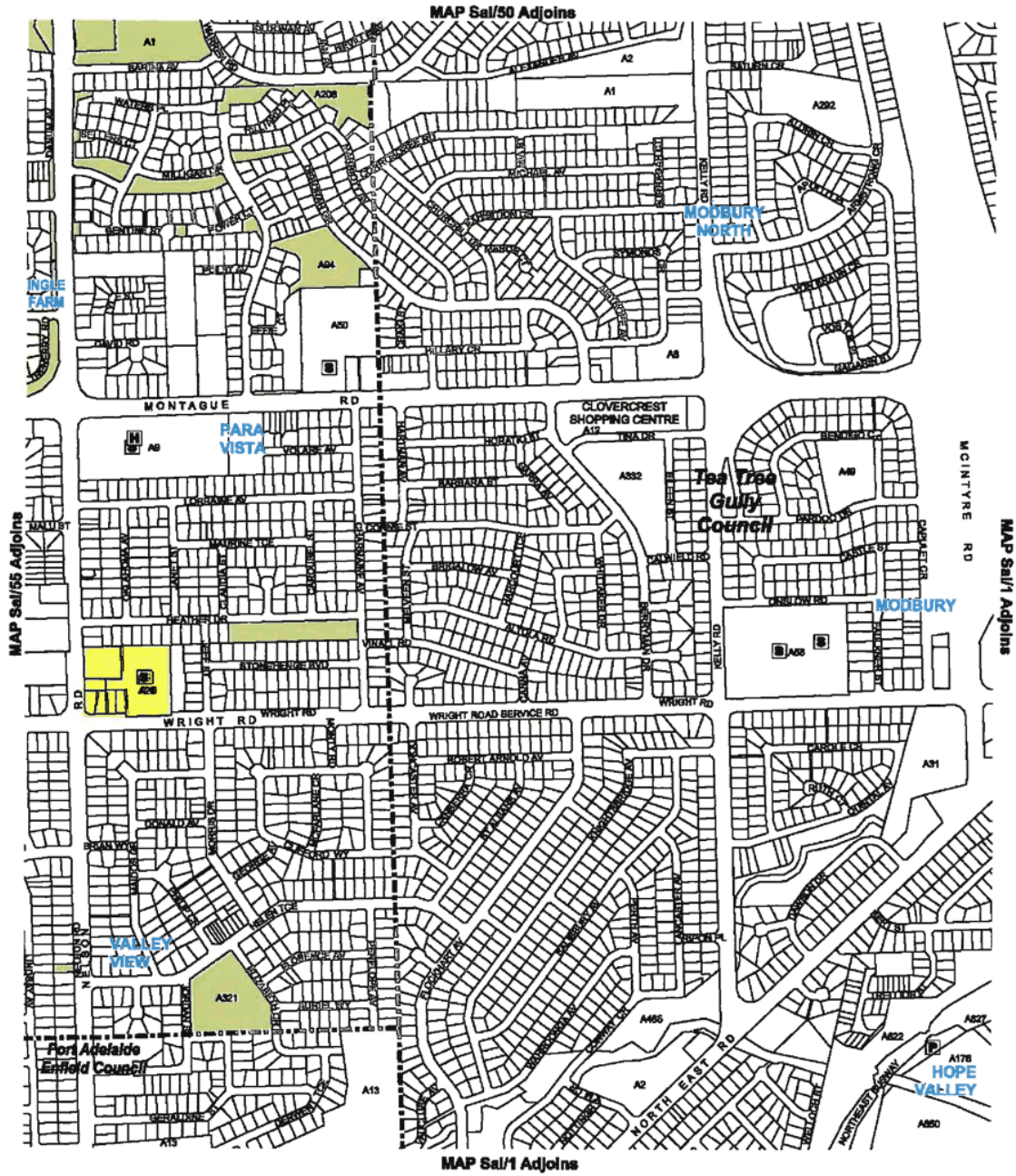
**Consolidated - 15 December 2016**

Please refer to the Salisbury Council page at [www.sa.gov.au/developmentplans](http://www.sa.gov.au/developmentplans) to see any amendments not consolidated.



**Government of South Australia**  
Department of Planning,  
Transport and Infrastructure

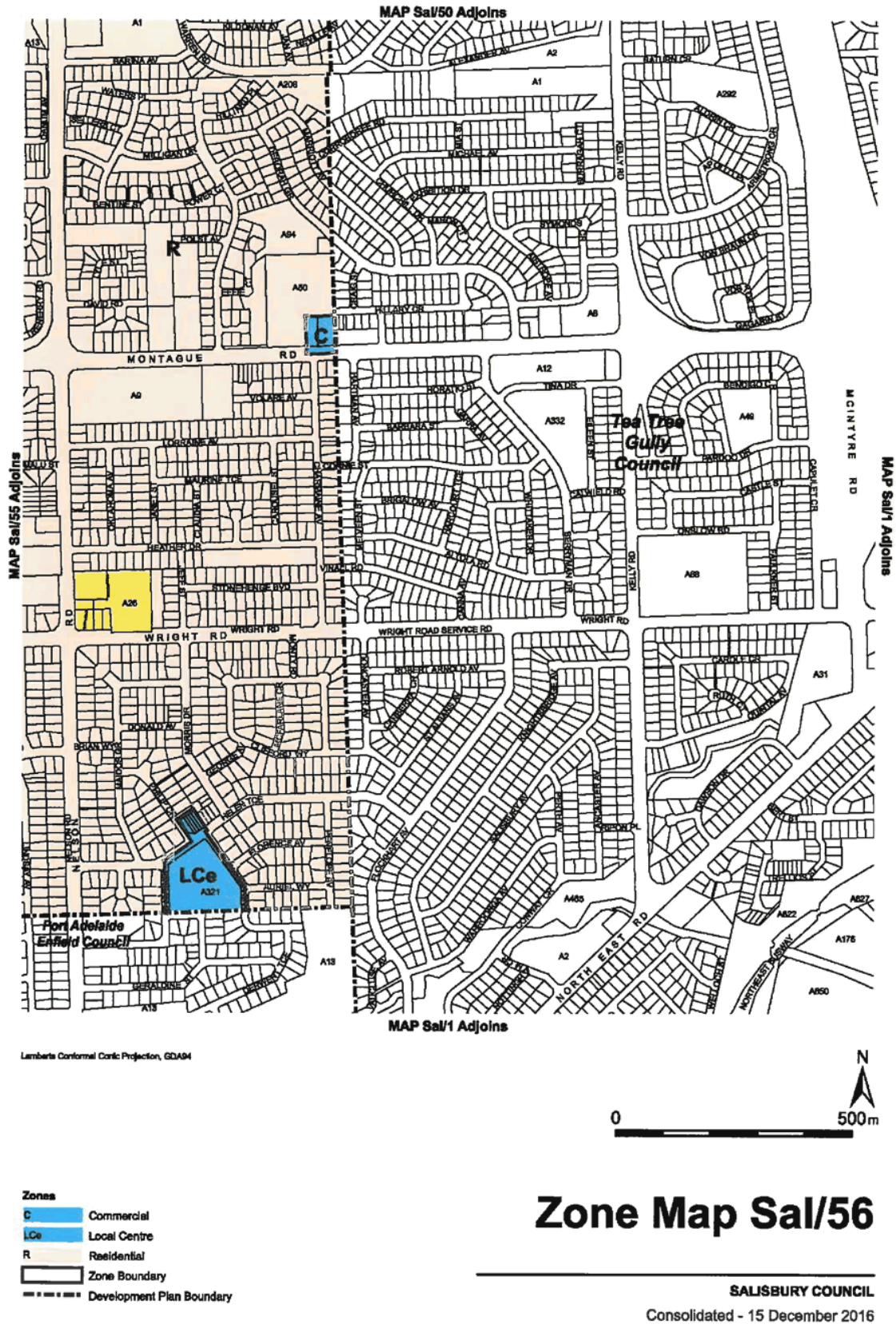
Consolidated - 15 December 2016



- School
- Post Office
- Other Health Services
- Local Reserves
- Development Plan Boundary

## Location Map Sal/56

**SALISBURY COUNCIL**  
Consolidated - 15 December 2016



## Residential Zone

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this zone.

### OBJECTIVES

- 1 A residential zone comprising a range of dwelling types, including a minimum of 15 per cent affordable housing.
- 2 Increased dwelling densities in close proximity to centres, public transport routes and public open spaces.
- 3 A zone primarily accommodating a range of dwellings to meet the diverse needs and preferences of the community.
- 4 Revitalisation of under-utilised suburbs, particularly public housing areas, to facilitate the provision of a greater mix of housing types and improved residential amenity.
- 5 Residential development designed and insulated to minimise effects of predicted aircraft noise and industrial noise.
- 6 Development that contributes to the desired character of the zone.

### DESIRED CHARACTER

The zone covers a substantial portion of the Council area and is home to a large and diverse population. The zone is suitable for a range of low to medium density housing, with higher density in appropriate locations. In addition to housing and open space, the zone will also contain a variety of community related, non-residential land uses that contribute to the creation of pleasant, connected, well serviced and socially inclusive neighbourhoods.

A full range of dwelling types on a variety of allotment sizes will be provided, including affordable housing, special needs housing and housing for the aged in appropriate locations. Housing form will be diverse and cater for different household sizes, life cycle stages and housing preferences and will be cost-effective, energy efficient and will make efficient use of available sites. Older neighbourhoods will experience change through turnover in population and ageing of existing residents which will generate a need for more housing diversity as well as changes in density and different services and facilities to complement these changes.

Residential development will reflect a variety of building styles, yet where a consistent character exists, new buildings will be designed to harmonise with that which is existing in terms of form, mass, scale, colours and textures of materials and setback distances. A cohesive built form will be achieved through design elements such as interesting roof forms, articulated buildings, recessed vehicle garaging, and landscaped spaces between buildings and the public road. Buildings will include windows, doors and balconies that overlook the street and areas of public open space to promote surveillance and community interaction.

Setbacks to local streets will be used to enable landscaping to be provided to soften the built form and improve amenity for residents and pedestrians. The visual and physical impact of vehicle access points will be minimised by reducing both the number of access points and the width of associated crossovers and driveways. Sealed surfaces will be minimised to reduce stormwater run-off and to also provide opportunities for landscaping to improve visual amenity. Development will seek to protect and retain regulated trees that contribute to the character of the area.

In order to achieve a compact urban form an overall increase in the residential density is desirable in select locations, in keeping with the objectives for development in the Zone, although it is anticipated that the predominant form of housing will remain detached dwellings at low to medium density throughout much of the zone. Co-ordinated infill housing, redevelopment and refurbishment of existing housing are encouraged

Salisbury Council  
Zone Section  
Residential Zone

to maximise use of the community investment in facilities and services in existing residential areas that are located close to centres and community services. Opportunities exist to gradually develop under-utilised and vacant land within established areas, particularly when a specific housing need arises within the population, but also as an opportunity for improving access to existing services and facilities. Co-ordinated and efficient development of larger sites containing multiple allotments is encouraged in order to achieve greater housing density and diversity.

Medium density forms of housing including semi-detached dwellings, row dwellings, residential flat buildings and group dwellings are encouraged in areas with good access to services and facilities. Concentrated nodes of medium density development of up to 4 storeys in height are anticipated in areas close to centres, public transport and significant public open space, and are to be developed in a co-ordinated and orderly manner. Typically this will involve the amalgamation of sites and will result in development that compliments the prevailing character. A transition in building height will be necessary to ensure issues of overlooking is minimised and solar access maintained.

Development will be of a form and scale compatible with adjoining residential development. As part of the increase in residential density there will be an increase in the number of dwellings of two or more storeys. Such development will recognise the proximity of adjoining development and will be designed and sited to maintain the privacy of adjoining dwellings and their private open space areas, and will be designed to integrate with the existing built form.

Development of land with a known history of a potentially contaminating activity will occur once the site has been assessed and remediated to the standard necessary to ensure it is safe and suitable for the proposed use.

Development in areas which adjoin industrial land uses and areas affected by aircraft noise as identified in [Concept Plan Map Sal/2 - Edinburgh Defence Airfield Aircraft Noise Exposure](#) should be designed, sited and constructed to minimise the effects of noise.

## PRINCIPLES OF DEVELOPMENT CONTROL

### Land Use

#### 1 The following forms of development are envisaged in the zone:

- affordable housing
- domestic outbuilding in association with a dwelling
- domestic structure
- dwelling
- dwelling addition
- **small scale non-residential use that serves the local community, for example:**
  - child care facility
  - health and welfare service
  - open space
  - **primary or secondary school**
- recreation area
- supported accommodation.

2 Development listed as non-complying is generally inappropriate.

3 Vacant or underutilised land should be developed in an efficient and co-ordinated manner to increase housing choice by providing dwellings at densities higher than, but compatible in form, scale and design with adjoining residential development.

4 Non-residential development such as shops, schools and consulting rooms should be of a nature and scale that:

- (a) serves the local community
- (b) is consistent with the character of the locality



## Design and Appearance

### OBJECTIVES

- 1 Development of a high architectural standard that responds to and reinforces positive aspects of the local environment and built form.
- 2 Roads, open spaces, buildings and land uses laid out and linked so that they are easy to understand and navigate.

### PRINCIPLES OF DEVELOPMENT CONTROL

- 1 The design of a building may be of a contemporary nature and exhibit an innovative style provided the overall form is sympathetic to the scale of development in the locality and with the context of its setting with regard to shape, size, materials and colour.
- 2 Buildings should be designed and sited to avoid creating extensive areas of uninterrupted walling facing areas exposed to public view.
- 3 Buildings should be designed to reduce their visual bulk and provide visual interest through design elements such as:
  - (a) articulation
  - (b) colour and detailing
  - (c) small vertical and horizontal components
  - (d) design and placing of windows
  - (e) variations to facades.
- 4 Where a building is sited on or close to a side boundary, the side boundary wall should be sited and limited in length and height to minimise:
  - (a) the visual impact of the building as viewed from adjoining properties
  - (b) overshadowing of adjoining properties and allow adequate sun light to neighbouring buildings.
- 5 Building form should not unreasonably restrict existing views available from neighbouring properties and public spaces.
- 6 Transportable buildings and buildings which are elevated on stumps, posts, piers, columns or the like, should have their suspended footings enclosed around the perimeter of the building with brickwork or timber, and the use of verandas, pergolas and other suitable architectural detailing to give the appearance of a permanent structure.
- 7 The external walls and roofs of buildings should not incorporate highly reflective materials which will result in glare to neighbouring properties or drivers.
- 8 Structures located on the roofs of buildings to house plant and equipment should form an integral part of the building design in relation to external finishes, shaping and colours.
- 9 Building design should emphasise pedestrian entry points to provide perceptible and direct access from public street frontages and vehicle parking areas.

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- 10 Development should provide clearly recognisable links to adjoining areas and facilities.
- 11 Buildings, landscaping, paving and signage should have a co-ordinated appearance that maintains and enhances the visual attractiveness of the locality.
- 12 Buildings (other than ancillary buildings or group dwellings) should be designed so that their main façade faces the primary street frontage of the land on which they are situated.
- 13 Where applicable, development should incorporate verandas over footpaths to enhance the quality of the pedestrian environment.
- 14 Development should be designed and sited so that outdoor storage, loading and service areas are screened from public view by an appropriate combination of built form, solid fencing and/or landscaping.
- 15 Outdoor lighting should not result in light spillage on adjacent land.
- 16 Balconies should:
- be integrated with the overall architectural form and detail of the building
  - be sited to face predominantly north, east or west to provide solar access
  - have a minimum area of 2 square metres.

#### Development Adjacent Heritage Places

- 17 The design of multi-storey buildings should not detract from the form and materials of adjacent State and local heritage places listed in [Table Sal/4 - State Heritage Places](#).
- 18 Development on land adjacent to a State or local heritage place, as listed in [Table Sal/4 - State Heritage Places](#) should be sited and designed to reinforce the historic character of the place and maintain its visual prominence.

#### Overshadowing

- 19 The design and location of buildings should enable direct winter sunlight into adjacent dwellings and private open space and minimise the overshadowing of:
- windows of habitable rooms
  - upper-level private balconies that provide the primary open space area for a dwelling
  - solar collectors (such as solar hot water systems and photovoltaic cells).

#### Visual Privacy

- 20 Development should minimise direct overlooking of habitable rooms and private open spaces of dwellings through measures such as:
- off-setting the location of balconies and windows of habitable rooms with those of other buildings so that views are oblique rather than direct
  - building setbacks from boundaries (including building boundary to boundary where appropriate) that interrupt views or that provide a spatial separation between balconies or windows of habitable rooms
  - screening devices (including fencing, obscure glazing, screens, external ventilation blinds, window hoods and shutters) that are integrated into the building design and have minimal negative effect on residents' or neighbours' amenity.
- 21 Permanently fixed external screening devices should be designed and coloured to complement the associated building's external materials and finishes

## Interface between Land Uses

### OBJECTIVES

- 1 Development located and designed to minimise adverse impact and conflict between land uses.
- 2 Protect community health and amenity from adverse impacts of development.
- 3 Protect desired land uses from the encroachment of incompatible development.

### PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development should not detrimentally affect the amenity of the locality or cause unreasonable interference through any of the following:
  - (a) the emission of effluent, odour, smoke, fumes, dust or other airborne pollutants
  - (b) noise
  - (c) vibration
  - (d) electrical interference
  - (e) light spill
  - (f) glare
  - (g) hours of operation
  - (h) traffic impacts.
- 2 Development should be sited and designed to minimise negative impacts on existing and potential future land uses desired in the locality.
- 3 Development adjacent to a Residential Zone should be designed to minimise overlooking and overshadowing of adjacent dwellings and private open space.
- 4 Residential development adjacent to non-residential zones and land uses should be located, designed and/or sited to protect residents from potential adverse impacts from non-residential activities.
- 5 Sensitive uses likely to conflict with the continuation of lawfully existing developments and land uses desired for the zone should be designed to minimise negative impacts.
- 6 Non-residential development on land abutting a residential zone should be designed to minimise noise impacts to achieve adequate levels of compatibility between existing and proposed uses.

### Noise Generating Activities

- 7 Development that emits noise (other than music noise) should include noise attenuation measures that achieve the relevant *Environment Protection (Noise) Policy* criteria when assessed at the nearest existing noise sensitive premises.
- 8 Development with the potential to emit significant noise (e.g. industry) should incorporate noise attenuation measures that prevent noise from causing unreasonable interference with the amenity of noise sensitive premises.

## Transportation and Access

### OBJECTIVES

- 1 A comprehensive, integrated, affordable and efficient air, rail, sea, road, cycle and pedestrian transport system that will:
  - (a) provide equitable access to a range of public, community and private transport services for all people
  - (b) ensure a high level of safety
  - (c) effectively support the economic development of the State
  - (d) have minimal negative environmental and social impacts
  - (e) maintain options for the introduction of suitable new transport technologies.
- 2 Development that:
  - (a) provides safe and efficient movement for all motorised and non-motorised transport modes
  - (b) ensures access for vehicles including emergency services, public infrastructure maintenance and commercial vehicles
  - (c) provides off street parking
  - (d) is appropriately located so that it supports and makes best use of existing transport facilities and networks.
- 3 A road hierarchy that promotes safe and efficient transportation in an integrated manner throughout the State.
- 4 Provision of safe, pleasant, accessible, integrated and permeable pedestrian and cycling networks.
- 5 Safe and convenient freight movement throughout the State.

### PRINCIPLES OF DEVELOPMENT CONTROL

#### Land Use

- 1 Land uses arranged to support the efficient provision of sustainable transport networks and encourage their use.

#### Movement Systems

- 2 Development should be integrated with existing transport networks, particularly major rail and road corridors as shown on *Location Maps* and *Overlay Maps - Transport*, and designed to minimise its potential impact on the functional performance of the transport networks.
- 3 Transport corridors should be sited and designed so as to not unreasonably interfere with the health and amenity of adjacent sensitive land uses.
- 4 Roads should be sited and designed to blend with the landscape and be in sympathy with the terrain.

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- 5 Land uses that generate large numbers of visitors such as shopping centres and areas, places of employment, schools, hospitals and medium to high density residential uses should be located so that they can be serviced by existing transport networks and encourage walking and cycling.
- 6 Development generating high levels of traffic, such as schools, shopping centres and other retail areas, entertainment and sporting facilities, should incorporate passenger pick-up and set down areas. The design of such areas should ensure interference to existing traffic is minimised and give priority to pedestrians, cyclists and public and community transport users.
- 7 The location and design of public and community transport set-down and pick-up points should maximise safety and minimise the isolation and vulnerability of users.
- 8 Development should provide safe and convenient access for all anticipated modes of transport including cycling, walking, public and community transport, and motor vehicles.
- 9 Development at intersections, pedestrian and cycle crossings, and crossovers to allotments should maintain or enhance sightlines for motorists, cyclists and pedestrians to ensure safety for all road users and pedestrians.
- 10 Driveway crossovers affecting pedestrian footpaths should maintain the level of the footpath.
- 11 Development should discourage commercial and industrial vehicle movements through residential streets and adjacent other sensitive land uses such as schools.
- 12 Industrial/commercial vehicle movements should be separated from passenger vehicle car-parking areas.
- 13 Development should make sufficient provision on site for the loading, unloading and turning of all traffic likely to be generated.

**Cycling and Walking**

- 14 Development should ensure that a permeable street and path network is established that encourages walking and cycling through the provision of safe, convenient and attractive routes with connections to adjoining streets, paths, open spaces, schools, public and community transport stops and activity centres.
- 15 Development should provide access, and accommodate multiple route options, for cyclists by enhancing and integrating with:
  - (a) open space networks, recreational trails, parks, reserves and recreation areas
  - (b) Adelaide's Metropolitan Open Space System.
- 16 Cycling and pedestrian networks should be designed to be permeable and facilitate direct and efficient passage to neighbouring networks and facilities.
- 17 New developments should give priority to and not compromise existing designated bicycle routes.
- 18 Where development coincides with, intersects or divides a proposed bicycle route or corridor, development should incorporate through-access for cyclists.
- 19 Developments should encourage and facilitate cycling as a mode of transport by incorporating end-of-journey facilities including:
  - (a) showers, changing facilities, and secure lockers
  - (b) signage indicating the location of bicycle facilities

- (c) secure bicycle parking facilities provided at the rate set out in [Table Sal/3 - Off Street Bicycle Parking Requirements](#).
- 20 Pedestrian facilities and networks should be designed and provided in accordance with relevant provisions of the *Australian Standards and Austroads Guide to Traffic Engineering Practice Part 13*.
- 21 Cycling facilities and networks should be designed and provided in accordance with the relevant provisions of the *Australian Standards and Austroads Guide to Traffic Engineering Practice Part 14*.

#### Access

- 22 Development should have direct access from an all weather public road.
- 23 Development should be provided with safe and convenient access which:
  - (a) avoids unreasonable interference with the flow of traffic on adjoining roads
  - (b) provides appropriate separation distances from existing roads or level crossings
  - (c) accommodates the type and volume of traffic likely to be generated by the development or land use and minimises induced traffic through over-provision
  - (d) is sited and designed to minimise any adverse impacts on the occupants of and visitors to neighbouring properties.
- 24 Development should not restrict access to publicly owned land such as recreation areas.
- 25 The number of vehicle access points onto arterial roads shown on *Overlay Maps - Transport* should be minimised, and where possible access points should be:
  - (a) limited to local roads
  - (b) shared between developments.
- 26 The number of access points for cyclists and pedestrians onto all adjoining roads should be maximised.
- 27 Development with access from roads with existing or projected traffic volumes exceeding 6000 vehicles per day should be sited to avoid the need for vehicles to reverse on to or from the road.
- 28 Development with access from arterial roads or roads as shown on *Overlay Maps - Transport* should be sited to avoid the need for vehicles to reverse on to or from the road.
- 29 Driveways, access tracks and parking areas should be designed and constructed to:
  - (a) follow the natural contours of the land
  - (b) minimise excavation and/or fill
  - (c) minimise the potential for erosion from run-off
  - (d) avoid the removal of existing vegetation
  - (e) be consistent with *Australian Standard AS 2890 Parking facilities*.

#### Access for People with Disabilities

- 30 Development should be sited and designed to provide convenient access for people with a disability.
- 31 Where appropriate and practical, development should provide for safe and convenient access to the coast and beaches for disabled persons.

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### Vehicle Parking

- 32 Development should provide off-street vehicle parking and specifically marked disabled car parking places to meet anticipated demand in accordance with [Table Sal/2 - Off Street Vehicle Parking Requirements](#) or [Table Sal/2A - Off Street Vehicle Parking Requirements for Designated Areas](#) (whichever applies) unless an agreement is reached between the Council and the applicant for a reduced number of parking spaces where one of the following applies:
- a financial contribution is paid into the Council Car Parking Funds specified by the Council, in accordance with the gazetted rate per car park associated with the 'Car Park Fund Areas' identified on [Concept Plan Map Sal/27 - Salisbury District Centre Car Park Fund Area](#), [Concept Plan Map Sal/29 - Ingle Farm District Centre Car Park Fund Area](#) and [Concept Plan Map Sal/32 - Mawson Lakes Town Centre Car Parking Fund Area](#)
  - it can be demonstrated that fewer car parks would be required to meet the car parking needs associated with the development.
- 33 Development should be consistent with *Australian Standard AS 2890 Parking facilities*.
- 34 Vehicle parking areas should be sited and designed in a manner that will:
- facilitate safe and convenient pedestrian linkages to the development and areas of significant activity or interest in the vicinity of the development
  - include safe pedestrian and bicycle linkages that complement the overall pedestrian and cycling network
  - not inhibit safe and convenient traffic circulation
  - result in minimal conflict between customer and service vehicles
  - avoid the necessity to use public roads when moving from one part of a parking area to another
  - minimise the number of vehicle access points to public roads
  - avoid the necessity for backing onto public roads
  - where reasonably possible, provide the opportunity for shared use of car parking and integration of car parking areas with adjoining development to reduce the total extent of vehicle parking areas and the requirement for access points
  - not dominate the character and appearance of a site when viewed from public roads and spaces
  - provide landscaping that will shade and enhance the appearance of the vehicle parking areas.
- 35 Vehicle parking areas should be designed to reduce opportunities for crime by:
- maximising the potential for passive surveillance by ensuring they can be overlooked from nearby buildings and roads
  - incorporating walls and landscaping that do not obscure vehicles or provide potential hiding places
  - being appropriately lit
  - having clearly visible walkways.
- 36 Where parking areas are not obviously visible or navigated, signs indicating the location and availability of vehicle parking spaces associated with businesses should be displayed at locations readily visible to customers.

- 37 Parking areas that are likely to be used during non daylight hours should provide floodlit entrance and exit points and site lighting directed and shaded in a manner that will not cause nuisance to adjacent properties or users of the car park.
- 38 Parking areas should be sealed or paved in order to minimise dust and mud nuisance.
- 39 To assist with stormwater detention and reduce heat loads in summer, vehicle parking areas should include soft (living) landscaping.
- 40 Parking areas should be line-marked to indicate parking bays, movement aisles and direction of traffic flow.

#### **Vehicle Parking for Residential Development**

- 41 On-site vehicle parking should be provided having regard to:
- (a) the number, nature and size of proposed dwellings
  - (b) proximity to centre facilities, public and community transport within walking distance of the dwellings
  - (c) the anticipated mobility and transport requirements of the likely occupants, particularly groups such as aged persons.
- 42 Vehicle parking areas servicing more than one dwelling should be of a size and location to:
- (a) serve users, including pedestrians, cyclists and motorists, efficiently, conveniently and safely
  - (b) provide adequate space for vehicles, including emergency service vehicles, to manoeuvre between the street and the parking area
  - (c) reinforce or contribute to attractive streetscapes.

#### **Vehicle Parking for Mixed Use and Corridor Zones**

- 43 Loading areas and designated parking spaces for service vehicles should:
- (a) be provided within the boundary of the site
  - (b) not be located in areas where there is parking provided for any other purpose.
- 44 Vehicle parking spaces and multi-level vehicle parking structures within buildings should:
- (a) enhance active street frontages by providing land uses such as commercial, retail or other non-car park uses along ground floor street frontages
  - (b) complement the surrounding built form in terms of height, massing and scale
  - (c) incorporate facade treatments along major street frontages that are sufficiently enclosed and detailed to complement neighbouring buildings consistent with the desired character of the locality.
- 45 In mixed use buildings, the provision of vehicle parking may be reduced in number and shared where the operating hours of commercial activities complement the residential use of the site.

#### **Undercroft and Below Ground Garaging and Parking of Vehicles**

- 46 Undercroft and below ground garaging of vehicles should only occur where envisaged in the relevant zone or policy area or precinct and ensure:
- (a) the overall height and bulk of the undercroft structure does not adversely impact on streetscape character of the locality or the amenity of adjacent properties



<b>ITEM</b>	5.1.3
	<b>COUNCIL ASSESSMENT PANEL</b>
<b>DATE</b>	4 December 2018
<b>APPLICATION NO.</b>	361/1223/2017/3B
<b>APPLICANT</b>	Ace Bitumen Contractors Pty Ltd
<b>PROPOSAL</b>	Change of use to truck parking depot and associated signage
<b>LOCATION</b>	108 Levels Road, Cavan SA 5094
<b>CERTIFICATE OF TITLE</b>	CT-5084/263
<b>AUTHOR</b>	Chris Carrey, Development Officer, City Development

### 1. DEVELOPMENT APPLICATION DETAILS

<b>Zone/Policy Area</b>	Industry Zone Greater Levels Policy Area 8
<b>Application Type</b>	Category 3 Merit
<b>Public Notification</b>	Representations received: Five Representations to be heard: Two
<b>Referrals - Statutory</b>	Nil
<b>Referrals – Internal</b>	Development Engineering
<b>Development Plan Version</b>	Salisbury Council Development Plan Consolidated 15 December 2016
<b>Assessing Officer</b>	Chris Carrey – Development Officer - Planning
<b>Recommendation</b>	Grant Development Plan Consent subject to conditions
<b>Meeting Date</b>	4 December 2018

### 2. REPORT CONTENTS

#### Assessment Report

Attachment 1:	Proposal Plans and Supporting Information (including Acoustic Report)
Attachment 2:	Vehicle Log Details provided by Applicant
Attachment 3:	Notice of category 3 Application and Representations Received
Attachment 4:	Applicant Response to Representations
Attachment 5:	Relevant Development Plan Extracts and Location Maps consolidated 15 December 2016

### 3. EXECUTIVE SUMMARY

The applicant seeks Development Plan Consent for a change in the use of the land to a truck parking depot and associated signage at 108 Levels Road, Cavan.

The site is located within the Industry Zone, adjacent to the Residential Zone. The application was assessed 'on-merit' and was subject to Category 3 public notification. Five (5) representations were received during the advertising period, with two in opposition wishing to be heard.

This report has provided a detailed assessment of the application against the relevant provisions of the Salisbury Council Development Plan (consolidated 15 December 2016). The assessment found that:

- a) The proposed land use is an appropriate form of development within the Industry Zone.
- b) Appropriate noise mitigation measures have been included to minimise noise impacts at the nearest residential dwelling and the proposal should comply with the *Environment Protection (Noise) Policy 2007*.
- c) There are no anticipated traffic impacts and the site can accommodate vehicle entry and exit in a forward direction with ample room available onsite for vehicle maneuvering.
- d) There are no anticipated impacts associated with dust or fumes.
- e) The visual appearance of the site is considered to be acceptable having regard to the proposed landscaping treatments and orderly parking of the vehicles.
- f) Advertising signage is considered to be acceptable and will not result in signage clutter or driver distraction.

Given the above, it is recommended that Development Plan Consent be granted, subject to conditions.

### 4. BACKGROUND

This is a retrospective application and the applicant has been operating from the subject site since around 2009.

During 2017, Council received a complaint from a nearby residential property within Mawson Lakes regarding hours of operation and associated truck noise emanating from the subject site and from the neighbouring property at 106 Levels Road. The complainant raised concern that the activities and associated noise were impacting upon their residential amenity. The complaints regarding the current use of the site have been ongoing since 2017.

Following discussions with Council staff, this application and a separate development application (361/1246/2017/3B) for 106 Levels Road were lodged with Council for assessment.

Development Application 361/1246/2017/3B is also to be considered within this Council Assessment Panel Agenda. While this development application applies to 108 Levels Road only, development application 361/1246/2017/3B applies to both 106 and 108 Levels Road.

The operators of 106 and 108 Levels Road have sought to work together to try and resolve the issues raised by the complainant in relation to vehicles reversing into 106 Levels Road (and the accompanying noise associated with that manoeuvre). That application provides for vehicles to move one way through 106 and 108 Levels Road, with no reliance for on-street reversing.

The current layout of the subject site appears to be largely consistent with an earlier development approval, granted during the 1990s for a 'Workshop and Office' (DA 361/2891/1991 refers). That approval allowed for construction of the existing buildings, provision of access from Levels Road and the general site layout. Therefore, Council understands the existing approved use of the subject site to be as a 'Workshop and Office'.

The existing approval does not preclude 24 hour operation at the subject site.

The subject site is located within an established industrial area which pre-dates the construction of the Mawson Lakes residential area to the north.

## **5. SUBJECT SITE**

The subject site is 3150sqm in area and has a primary frontage to Levels Road of 45 metres. There is an existing six (6) metre wide central concrete driveway which provides access to/from Levels Road.

The site is relatively level and is occupied by an existing office and workshop which are located on the south east side.

The surfaces are fully sealed with bitumen pavement and there are seven (7) standard car parking spaces to the side and front of the office. There is an existing storm water spoon drain which allows for discharge to the Levels Road storm water network.

The site provides for a manoeuvring area which is 31.6 metres in width between the workshop and north west side boundary.

There is a small landscaping strip located along the front property boundary and the site is fenced with a combination of solid metal and open chain mesh fencing.

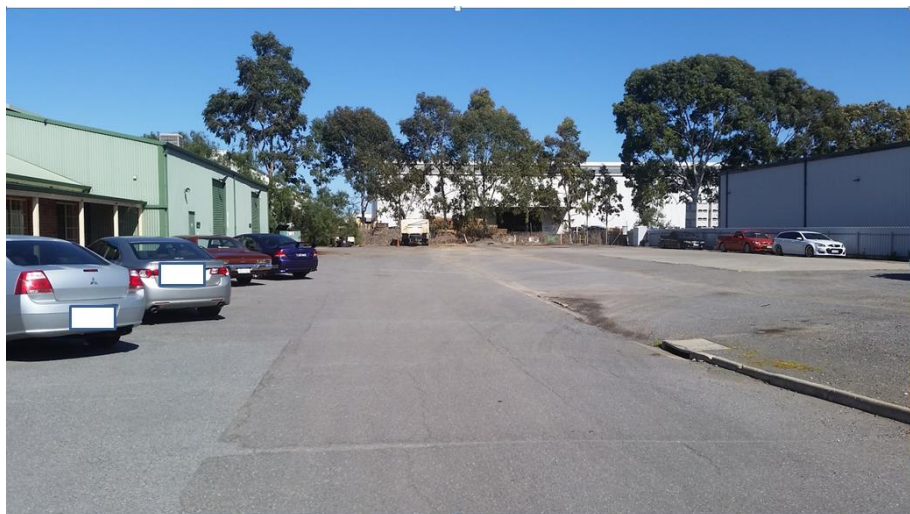
There is one easement registered to the Certificate of Title, however it is not affected by the proposal.

Site photos are provided below.

**Photo 1.**  
*Looking south into the subject site towards the office and workshop*



**Photo 2.**  
*Looking south west directly into the subject site from the access driveway*



**Photo 3.**  
*Looking north west along Levels Road from in front of the subject site*



**Photo 4.**  
*Looking south east along Levels Road from in front of the subject site*



**Photo 5.**  
*Looking north towards the residential area across Levels Road Reserve*



## 6. LOCALITY

The subject site is located within the Industry Zone and Greater Levels Policy Area 8 of Council's Development Plan and is directly adjacent to the Residential Zone boundary which aligns with Levels Road.

The site is bounded by established industrial properties to the north west, south west and south east.

The Mawson Lakes residential area is located to the north, across Levels Road and Levels Road Reserve. Several residential properties on Petrel Crescent and Prion Circuit have their rear yards and back fence abutting this Reserve. The Levels Road Reserve is approximately 15 metres wide and consists of established trees and shrubs. The fence to the rear of the residential properties exceeds 2.1 metres height and extends above eave height of those adjacent single storey dwellings.


Levels Road is gazetted for B-Double heavy vehicle access and predominantly services surrounding industrial sites on Levels Road, Sharpe Court, Cross Keys Road, CB Fisher Drive and Jonal Drive. However; it also allows for standard passenger car access to/from the residential areas to the south east and east via Beechwood Avenue.

Sharpe Court does not allow for a right turn (west) onto Montague Road. Therefore, businesses on Sharpe Court and Levels Road seeking to access Port Wakefield Road (or heading west more generally) are likely to make use of Levels Road, to access Cross Keys Road which then facilitates a right turn onto Montague Road.

Aerial and contextual plans are provided below.

Aerial View



Legend (Source: NearMap)	
	Subject site

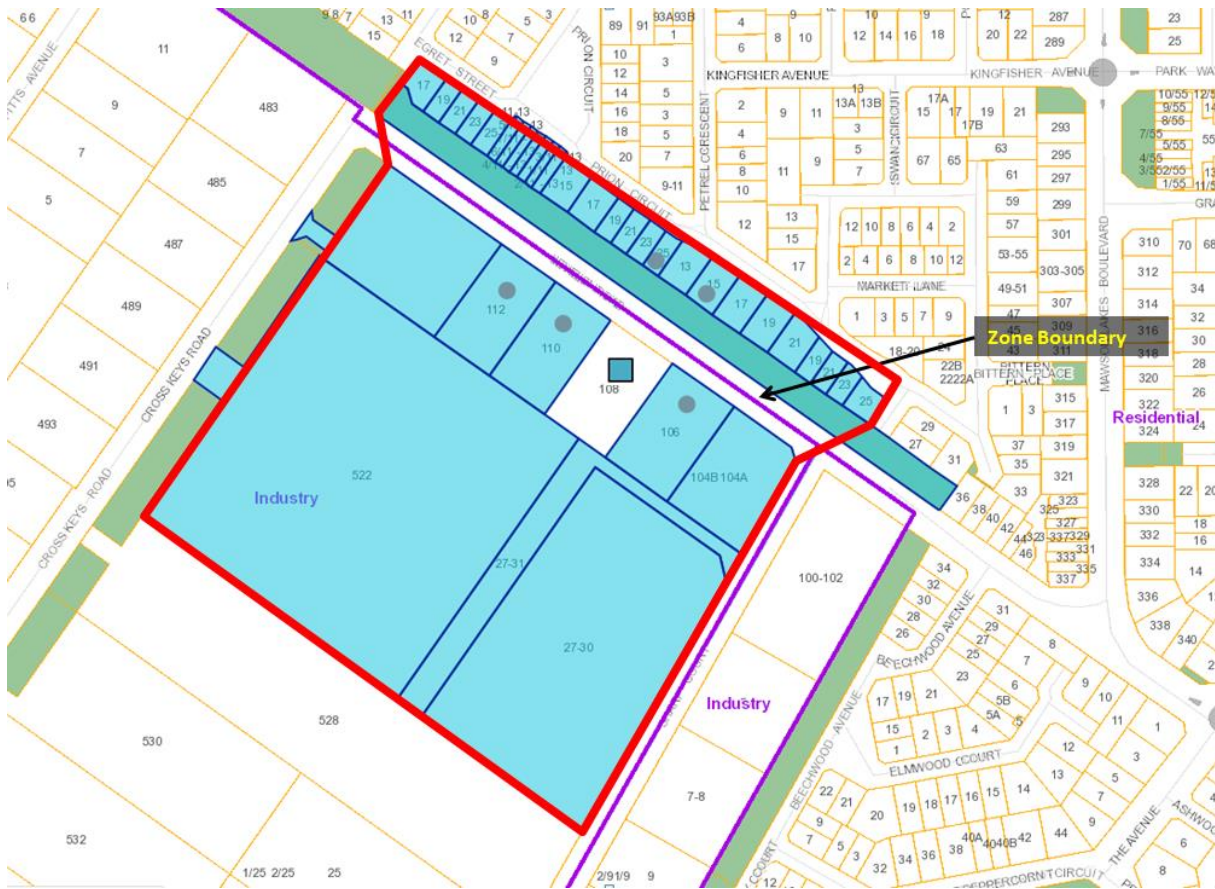
Close Up Aerial View







Source: NearMap

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Locality Plan – Cadastre



Legend (Source: Geocortex)	
	Subject site
	Locality boundary
	Properties notified during Category 3 public notification
	Representations Received during Category 3 public notification



## 7. DESCRIPTION OF THE PROPOSED DEVELOPMENT

The applicant seeks Development Plan Consent for a change in the use from 'Workshop and Office' to a truck parking depot which can be accessed 24 hours a day, seven days a week.

Trucks are to be parked on the north west side to the front and rear, with a central manoeuvring area to be retained. There is also capacity for truck parking to the rear of the workshop.

The proposal also includes attachment of two (2) advertising signs to the front fence and an upgrade to the existing landscape strip located along the front property boundary. The existing office and workshop are to be used in association with the truck parking depot.

No new built form is proposed and the proposal does not require any building rules consent.

The applicant (Ace Bitumen) is a bitumen contractor and they own and operate two trucks and five trailers. The trucks are used for the carting of bitumen hot mix. The trucks and trailers are listed below:

Type of Truck / Trailer	SA Registration No.	GVM*	Length
Isuzu	SB17BS	24,000kg	7.6m
Usual Trailer	SB17BS	-	9.8m
Optional Trailer # 1	TPO781	-	6.3m
Optional Trailer #2	TAB 227	-	7m
Isuzu Tack Truck	SB44GC	10,400kg	8m + trailer of 4.5m

\*GVM – Gross Vehicle Mass - the maximum weight that a truck can carry including its own weight

The maximum combined vehicle length relates to Isuzu SB17BS with the 9.8m trailer which is 17.4m.

The applicant also sub-leases the subject site to Downer EDI Works who use the site for the parking of a further six trucks and associated trailers. These trucks are also used for the carting of bitumen hot mix. The trucks with associated trailers are listed below:

Type of Truck / Trailer	SA Registration No.	GVM	Length
Volvo	VOV123	23,060kg	7.5m + trailer of 8.7m = 16.2m
Scania	SB47BM	26,000kg	7.5m + trailer of 9.6m = 17.1m
Freightliner	SB19AO	24,000kg	8.8m + trailer of 9.6m = 18.4m
Volvo	WRU242	23,500kg	7m + trailer of 9.2m = 16.2m

DAF	SB84GL	23,000kg	7.5m + trailer of 10.3m = 17.8m
Kenworth	SB57KG	24,600kg	7.7m + trailer of 9.3m = 17m

In total, the applicant seeks to park eight trucks and eleven trailers at the subject site.

There is no loading, unloading or material processing and the use extends to the parking and associated movement of trucks only.

The Ace Bitumen trucks generally operate Monday to Friday between 6.30am and 5.00pm. However, the applicant advises that hours of operation can extend slightly beyond this dependent upon weather, workloads, project type etc.

The Downer EDI Works trucks operate a varied schedule which includes night and early morning work across seven (7) days.

The office is occupied by three (3) staff and operates from 9.00am to 5.00pm.

Mechanical repair work extends only to the Ace Bitumen trucks and occurs during normal business hours.

Waste disposal is managed by private contractors, SA Oil Recovery and Suez Waste and occurs during normal business hours.

A copy of the proposal plans and supporting documentation are contained in Attachment 1.

The applicant has also provided a summary of vehicle activity times across several months during 2018. These are contained in Attachment 2.

## **8. CLASSIFICATION**

The subject site is located within the Industry Zone, as identified within Council's Development Plan (consolidated 15 December 2016). In this Zone, a truck parking depot is not listed as a complying or non-complying form of development. As such, the application must be assessed as a merit form of development.

## **9. PUBLIC NOTIFICATION**

The proposed development is neither listed as a Category 1 or 2 form of development within the Industry Zone or under Schedule 9 of the *Development Regulations 2008*. Accordingly, the application was advertised as a Category 3 form of development under Section 38 of the *Development Act 1993*.

Public notification took place between 21 August 2017 and 5 September 2017. The Council received five (5) representations during the notification period. Two (2) representations were in opposition and three (3) were in support of the proposal. The representors are listed in the table below.

<b>Representations received</b>			
<b>Representations received</b>		<b>Support / Oppose</b>	<b>Wish to be Heard</b>
1	D Golley 15 Petrel Crescent Mawson Lakes SA 5095	<b>Oppose</b>	✓
2	A Bailey 25 Prion Circuit Mawson Lakes SA 5095	<b>Oppose</b>	✓ <i>(* see additional comments below)</i>
3	R Frost 106 Levels Road Cavan SA 5094	<b>Support</b>	
4	Kora Bay Pty Ltd 110 Levels Road Cavan SA 5094	<b>Support</b>	
5	The Giuliani Nominees No 2 112 Levels Road Cavan SA 5094	<b>Support</b>	

A copy of the Category 3 public notice and submissions received are contained in Attachment 3. The applicant's response is contained in Attachment 4.

The content of the representations and the applicant's response are summarised in the table below:

<b>Summary of Representations</b>	
<b>Representation</b>	<b>Applicant's Response</b>
The type and level of noise associated with the activity, outside normal business hours and at weekends is not appropriate next to a residential area	<ul style="list-style-type: none"> <li>- The subject site is located within an established industrial area which pre-dates the Mawson Lakes residential area.</li> <li>- It is the responsibility of prospective purchasers and occupiers of those nearby residential properties to be aware of their surrounds, including adjacent commercial and industrial operations.</li> <li>- The established industrial area includes a variety of activities including freight and transport distribution, warehousing and industrial/manufacturing – all of which are permitted activities.</li> <li>- Heavy vehicle movements are both expected and common in this locality.</li> <li>- The subject site is much smaller than other industrial</li> </ul>

	<p>tenancies within the immediate locality, with other much larger freight transport and logistics located nearby. These other companies contribute to heavy vehicle noise in the locality.</p> <ul style="list-style-type: none"> <li>- The applicant only requires trucks to occasionally enter and exit the site prior to normal business hours, dependent upon weather – however, this is not the norm and only occurs when required.</li> <li>- The proposed development will not have an adverse impact on the health and amenity of occupiers in adjoining zones given the small scale nature of the operation in comparison to other industrial land uses within the locality</li> <li>- The development does not involve any processing or manufacturing which would generate high levels of noise, dust, fumes etc.</li> <li>- The subject site is located around 40m from the nearest residential dwelling, separated by a road and 15m wide landscape strip which is densely vegetated</li> <li>- The only noise generating activity associated with the use is the entry and exit of heavy vehicles which are associated with almost every industrial premise within the immediate locality.</li> </ul>
--	--

*Note: Public notification was undertaken using hand drawn plans that were initially provided by the applicant. The applicant later engaged a project designer to provide more detailed plans. The public notification also did not include the Acoustic Report and associated recommendations prepared by Sonus Acoustic Engineers which will be discussed later in this report.*

*The representors in opposition to the proposal have been made aware of the updated documentation.*

*\*The representor Amanda Bailey has advised that she no longer has any concerns and no longer wishes to be heard by Council's Assessment Panel.*

*If any additional comments or concerns are raised, these will be provided to members of Council's Assessment Panel prior to the meeting.*

## **10. REFERRALS – STATUTORY**

The application was not subject to any referrals under Schedule 8 of the Regulations.

**11. REFERRALS – INTERNAL**

DIVISION	COMMENT
<i>Development Engineering</i>	<p><i>There is sufficient space onsite to allow for entry and exit in a forward direction.</i></p> <p><i>As an existing, developed site no additional upgrades are required.</i></p>

**12. ASSESSMENT**

Pursuant to Section 35(2) of the *Development Act 1993*, it is recommended that the Panel determine that the proposed development is not seriously at variance with the Salisbury Council Development Plan, consolidated 15 December 2016. Truck parking and associated truck movements are an established activity within the locality and can be an appropriate form of development within an Industry Zone subject to amenity based considerations.

Assessment

Detailed assessment of the application has taken place against the relevant provisions of the Salisbury Council Development Plan and is described below under headings.

An extract of the relevant Development Plan, Consolidated 15 December 2016 is contained in Attachment 5. The relevant provisions are also highlighted in the Attachment.

Zoning and Land Use

Objective 1 of the Industry Zone states:

*1 A zone primarily accommodating a wide range of industrial, warehouse, storage and transport land uses.*

Objective 1 of the Greater Levels Policy Area 8 states:

*1 A policy area primarily accommodating a range of industrial activities.*

It is therefore considered that development in the form of truck parking (with associated truck movements) which supports an industrial activity can be an appropriate form of land use within the Industry Zone, subject to appropriate amenity based considerations.

Interface Between Land Uses

While the use may be acceptable within the Industry Zone, consideration must be given to the various impacts of the proposal to ensure the health and amenity of surrounding land occupiers and the locality are protected. The interface issues are addressed below.

Noise and Hours of Operation

Principle of Development Control 1(b) & (g) of the General Section: Interface Between Land Uses module states:

*1 Development should not detrimentally affect the amenity of the locality or cause unreasonable interference through any of the following:*

*(b) noise*

*(g) hours of operation*

Principle of Development Control 7 (a), (b) and (d) of the General Section: Industrial Development module states:

*7 Development within 50 metres of the **Residential Zone** boundary should:*

*(a) demonstrate appropriate acoustic performance*

*(b) ensure that all noise sources including machinery, outside loading, unloading and other service areas are located away from the **Residential Zone** boundary*

*(d) limit operating hours to between 7am and 6 pm*

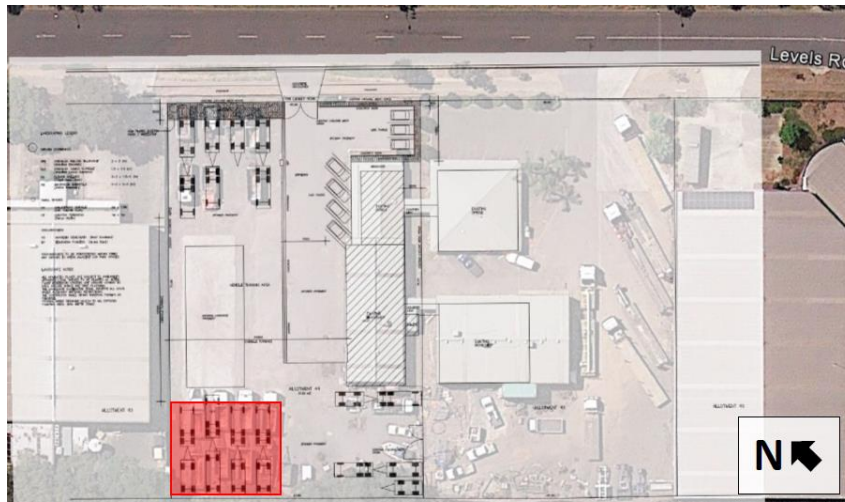
The subject site is located within 50 metres of the Residential Zone boundary and proposes to cater for access 24 hours a day across 7 days. The applicant has advised that 24 hour operation is required for operational reasons and working patterns are often dictated by weather, season and project type demands. The vehicle logs provided by the applicant in Attachment 2 demonstrate the varied start and finish times associated with the use.

The applicant provided an Environmental Noise Assessment Report prepared by Sonus Acoustic Engineers. This considers the noise impact of the proposed development against the noise goals of the *Environment Protection (Noise) Policy 2007* (“Noise Policy”) and details the required acoustic treatments to achieve these targets.

In particular, the report considered the impact of the development on the Residential Zone to the north.

The noise treatment measures recommended by Sonus are:

- *Ensure that trucks are not required to reverse while on site during the night time hours (10pm to 7am);*
- *Ensure no loading or unloading occurs on the site, with the exception of occasional hitching and unhitching of trailers during the day time hours (7am to 10pm) only;*
- *Restrict the amount of time a truck can idle after entering or before leaving the site to be no more than 5 minutes;*
- *Restrict the area where trucks can park on the site to that shown in the following figure, when entering or exiting during the night time hours of the Policy (10pm to 7am).*



(Source: Sonus)

The report concludes that incorporation of these measures will ensure that the maximum predicted noise levels at the nearest residential dwelling will be no more than:

- 48 dB(A) during the daytime (7.00am to 10.00pm). This satisfies the noise goal levels of the Policy which stipulate a maximum of 54 dB(A) during such times; and
- 45 dB(A) during the night time (10.00pm to 7.00am). This satisfies the noise goal levels of the Policy which stipulate a maximum of 45 dB(A) during such times.

The report also considered the maximum noise level from the activity such as truck brake compressed air release, people talking, or the opening and closing doors during the night time hours. The highest predicted noise level ( $L_{max}$ ) at the residences is 57 dB(A), which satisfies the 60 dB(A) goal noise level of the Policy.

On this basis, it is considered that while the development proposes 24 hours activity adjacent the Residential Zone, it has demonstrated appropriate acoustic performance to support the activity. It is also recognised that the existing approved use of the site ('workshop and office') does not preclude 24 hour operation and does not contain any specific controls or conditions relating to noise.

The proposed measures should improve the existing situation and address previous complaints from nearby residential occupiers and address issues relating to noise and hours of operation raised during the public notification period. The applicant has committed to implementing these changes.

In addition, a development plan condition has been recommended that all waste collection occur should occur during appropriate day time hours to limit the impact of noise on nearby residential properties.

### *Traffic Impacts*

Principle of Development Control 3 of the General Section; Industrial Development module states:

*3 Industrial development should enable all vehicles to enter and exit the site in a forward direction.*

Principles of Development Control 13 and 22 of the General Section: Transportation and Access module state:

*13 Development should make sufficient provision on site for the loading, unloading and turning of all traffic likely to be generated.*

*22 Development should have direct access from an all-weather public road.*

Levels Road is gazetted for the use of B-Double heavy vehicles and is of appropriate construction and design to support the size of trucks using the site.

All trucks are able to enter and exit the site in a forward direction with no negative impact on the road network or adjoining industrial sites. There is ample space on the site to accommodate vehicle manoeuvring.

On this basis, it is considered that the proposal accords with the above provisions and will not have a detrimental impact on the amenity of the locality.

#### *Dust and Fumes*

Principle of Development Control 1(a) of the General Section: Interface Between Land Uses module states:

*1 Development should not detrimentally affect the amenity of the locality or cause unreasonable interference through any of the following:*

*(a) the emission of effluent, odour, smoke, fumes, dust or other airborne pollutants*

The site is fully sealed and there is unlikely to be significant amounts of dust generated in association with vehicle movements and manoeuvring.

The only fumes likely to result as a consequence of the proposal are related to truck movements and truck idling. The applicant has confirmed that truck idling is restricted to five (5) minutes and this will occur on the subject site more than 50 metres from the nearest residential dwelling.

On this basis, and having regard to the established industrial character of this locality, there is no anticipated dust or fume impacts associated with the proposal.

#### *Lighting*

PDC 1 (e) of the *General Section: Interface Between Land Uses* module states:

*1 Development should not detrimentally affect the amenity of the locality or cause unreasonable interference through any of the following:*

*(e) light spill.*

No outdoor lighting has been proposed. If outdoor lighting were to be installed (e.g. for security purposes) it is unlikely to have a negative impact on the surrounding locality. Nearby industrial properties are likely to adopt similar arrangements and are not considered to be a 'sensitive' receptor.



Landscaping within the Levels Road Reserve and the existing solid fence provide an appropriate screen to prevent vehicle headlight glare affecting residential properties within the Mawson Lakes residential area.

To reinforce this position, a development plan condition has been recommended to ensure that any outdoor lighting use is restricted to avoid driver distraction or nuisance to adjoining sites.

#### Advertisements

Principles of Development Control 2 and 4 of the *General Section: Advertisements* module state:

*2 The number of advertisements and/or advertising hoardings associated with a development should be minimised to avoid:*

- (a) clutter*
- (b) disorder*
- (c) untidiness of buildings and their surrounds*
- (d) driver distraction.*

*4 The content of advertisements should be limited to information relating to the legitimate use of the associated land.*

The proposal includes the attachment of two advertising signs to the front chain mesh fence.

Both signs are for ‘Ace Bitumen’ and relate to their use of the subject land. The signs are relatively small and inconspicuous and are not a distraction to drivers. On this basis, they are considered to be an appropriate form of supporting development.

#### Visual Impact and Landscaping

As this is a retrospective proposal, the visual impact of the proposal is already apparent. The site is maintained in a neat and tidy condition, with parking occurring in an orderly manner and no outdoor storage of wastes or other materials taking place.

Objective 1 of the *General Section: Landscaping, Fences and Walls* module states:

*1 The amenity of land and development enhanced with appropriate planting and other landscaping works, using locally indigenous plant species where possible.*

While the majority of the site is sealed, the applicant proposes to include a 3m wide landscape strip along the front property boundary in front of the truck parking area. The existing landscape area to the front of the office is to be retained.

While the extent of landscaping is relatively modest, it will assist in softening the appearance of the site when viewed from the street and is generally consistent with surrounding industrial sites. The Reserve area on Levels Road already provides a reasonable amount of established landscaping to this locality and provides the important buffer to the residential area to the north.

### 13. CONCLUSION

The applicant seeks Development Plan Consent for a change in the use of the land to a truck parking depot and associated signage at 108 Levels Road, Cavan.

This report has provided a detailed assessment of the application against the relevant provisions of the Salisbury Council Development Plan (consolidated 15 December 2016). The assessment found that:

- a) The proposed land use is an appropriate form of development within the Industry Zone.
- b) Appropriate noise mitigation measures have been included to minimise noise impacts at the nearest residential dwelling and the proposal should comply with the *Environment Protection (Noise) Policy 2007*.
- c) There are no anticipated traffic impacts and the site can accommodate vehicle entry and exit in a forward direction with ample room available onsite for vehicle manoeuvring.
- d) There are no anticipated impacts associated with dust or fumes.
- e) The visual appearance of the site is considered to be acceptable having regard to the proposed landscaping treatments and orderly parking of the vehicles.
- f) Advertising signage is considered to be acceptable and will not result in signage clutter or driver distraction.

Accordingly, it is recommended that Development Plan Consent be granted, subject to conditions.

**14. STAFF RECOMMENDATION**

That the Council Assessment Panel resolve that:

- A. The proposed development is not considered to be seriously at variance with the Salisbury Council Development Plan – Consolidated 15 December 2016.
- B. Pursuant to Section 33 of the *Development Act 1993*, Development Plan Consent is **GRANTED** to application number 361/1223/2017/3B for Change of use to truck parking depot and associated signage in accordance with the plans and details submitted with the application and subject to the following conditions:

*Development Plan Consent Conditions*

1. The development shall be carried out in accordance with the details submitted with the application and the following stamped approved plans and documents, except where otherwise varied by the conditions herein:

<b>Drawing No.</b>	<b>Plan Type</b>	<b>Date</b>	<b>Prepared By</b>
18/08-A01 Revision C	Site Plan	Date Received Stamped by Council 26 October 2018	David W. Pound Project Design
S5789C1	Environmental Noise Assessment	Date Received Stamped by Council 2 November 2018	Sonus Acoustic Engineers
N/A	Supporting cover letter	Date Received by Council Stamped 10 August 2017	Ace Bitumen Contractors
N/A	Response to Representations	10 October 2017	Masterplan

*Reason: To ensure the proposal is established in accordance with the submitted plans.*

2. The development shall be carried out in accordance with the Environmental Noise Assessment (“Acoustic Report”) approved by Council under Development Plan Condition 1, and shall be adhered to at all times to the reasonable satisfaction of Council. In particular:
  - a. Trucks shall not reverse while on site during night time hours (10.00pm to 7.00am)
  - b. No loading or unloading shall occur onsite, with the exception of occasional hitching and unhitching of trailers during day time hours only (7.00am to 10.00pm)
  - c. Truck idling shall be restricted to no more than 5 minutes after entering or before leaving the site

- d. Trucks shall only park to the rear (south west) of the site, on that area of the site identified on page 13 of the Acoustic Report, when entering or exiting during night time hours (10.00pm to 7.00am)

*Reason: To ensure that noise does not cause nuisance to adjoining residential occupiers or owners.*

3. All vehicles shall enter and exit the site in a forward direction.

*Reason: To ensure that vehicles associated with the land use do not cause disruption or danger to vehicles on adjoining public roads.*

4. Noise from the subject site, when measured at the nearest residential property boundary shall remain within the requirements of the Environment Protection Authority (EPA) Guidelines for development adjacent a residential area.

*Reason: To ensure that noise does not cause nuisance to adjoining residential occupiers or owners.*

5. No materials, goods or containers shall be stored in the designated parking areas, driveways or manoeuvring areas.

*Reason: To ensure the car parking areas are always available for the purpose they are designed. Further that the site be maintained in a clean and tidy state.*

6. All driveways and car parking areas shall be constructed with brick paving, concrete or bitumen to a standard appropriate for the intended traffic volumes and vehicle types. Individual car parking bays shall be clearly line marked. Driveways and car parking areas shall be maintained at all times to the satisfaction of Council.

*Reason: To ensure access and car parking is provided on the site in a manner that maintains and enhances the amenity of the locality.*

7. All loading and unloading of vehicles and manoeuvring of vehicles in connection with the now approved land use shall be carried out entirely within the subject land.

*Reason: To ensure that vehicles associated with the land use do not cause disruption or danger to vehicles on adjoining public roads.*

8. Outside lighting shall be restricted to that necessary for security purposes only and shall be directed and shaded to prevent light overspill and/or nuisance to adjacent occupiers or distraction to drivers on adjacent public roads.

*Reason: To ensure that floodlighting does not cause nuisance or danger to adjoining occupiers or road users thereby reducing the amenity of the locality and/or making road use unsafe.*

9. The designated landscaping areas shall be planted with shade trees, shrubs and ground covers as appropriate to complement the approved buildings and site layout and achieve a high level of amenity. All landscaping shall be maintained (including the replacement of diseased or dying plants and the removal of weeds and pest plants) to the reasonable satisfaction of Council. All landscaping is to be completed within three (3) months of the approved use commencing.

*Reason: To ensure the subject land is landscaped so as to enhance the visual and environmental amenity of the locality.*

10. All waste and other rubbish shall be contained and stored pending removal in covered containers and waste containers and must not be located within designated car parks or maneuvering areas.

*Reason: To maintain the amenity of the locality.*

11. All waste shall be collected between the hours of 7.00am and 7.00pm Monday to Saturday. Waste collection times shall be limited to between 9.00am and 7.00pm on Sundays and Public Holidays, in accordance with the *Environment Protection (Noise) Policy 2007*.

*Reason: To maintain the amenity of the locality.*

#### **CO-ORDINATION**

Officer: GMCiD MDS  
Date: 19.11.18 14.11.18

#### **ATTACHMENTS**

This document should be read in conjunction with the following attachments:

1. Proposal Plans and Supporting Information (including Acoustic Report)
2. Vehicle Log Details provided by Applicant
3. Notice of category 3 Application and Representations Received
4. Applicant Response to Representations
5. Relevant Development Plan Extracts and Location Maps consolidated 15 December 2016



## Attachment 1

Proposal Plans and supporting information (including Acoustic Report)



**CITY OF SALISBURY  
DEVELOPMENT APPLICATION FORM**

361/ 1223 12017/1B

Please use BLOCK LETTERS and Black or Blue Ink

APPLICANT: COMPANY / FIRST NAME		SURNAME	
ACE BITUMEN CONTRACTORS PTY LTD		PTM LTD	
POSTAL ADDRESS: 108 LEVELS ROAD, CAVAN 5094			
OWNER NAME: (This must be completed) GIULIA PROPERTIES PTY LTD			<input type="checkbox"/> as above
OWNER POSTAL ADDRESS: 4 WYFIELD STREET, WATTLE PARK 5066			<input type="checkbox"/> as above
OWNER PHONE NO: 0421 00983	OWNER FAX NO: 8314100	OWNER EMAIL: PAT@PURITIERSAUSTRALIA.COM.AU	
CONTACT PERSON FOR FURTHER INFORMATION			<input type="checkbox"/> as above
NAME: JAN JARRETT		TELEPHONE (W): 8349 8088 (M): 0408 821096	
EMAIL: ACEBITUMEN@BIGBOND.COM		FAX: 83498065	
BUILDER NAME:		BUILDERS EMAIL:	
BUILDER POSTAL ADDRESS:		CONTACT NO.:	
		LICENCE NO.:	
CURRENT USE OF PROPERTY: WORKSHOP, OFFICE & TRAINING FACILITIES WITH ASSOCIATED CAR PARKING & LANDSCAPE			
DESCRIPTION OF PROPOSAL: AMEND USAGE TO INCLUDE TRUCK PARKING		DEVELOPMENT COST \$794,25	
<b>LOCATION OF PROPOSAL</b>			
Street No: 108 ✓	Street: LEVELS ROAD	Suburb: CAVAN	5094
Lot No: 44	Section:	Plan:	Volume: 5094 Folio: 263
<b>OFFICE USE ONLY</b>			
Registration Date: 3 / 7 / 2017	Zone: IN	Ward: Levels	
<b>BUILDING RULES CLASSIFICATION</b>			
Classification sought:			
If Class 5, 6, 7, or 9 classification is sought, state the proposed number of employees			Male: Female:
If Class 9a classification is sought, state the number of persons for whom accommodation is provided:			
If Class 9b classification is sought, state the proposed number of occupants of the various spaces at the premises:			

I acknowledge that copies of this application and supporting documentation may be provided to interested persons in accordance with the Development Regulations, 1993. Developments requiring public notification will be made available to the public for comment via Council's web site at [www.salisbury.sa.gov.au](http://www.salisbury.sa.gov.au)

SIGNATURE: Jan Jarrett

DATE: 3 / 7 / 17





10 August, 2017

City of Salisbury  
Attention: Ms Marie Molinaro  
[development@salisbury.sa.gov.au](mailto:development@salisbury.sa.gov.au)

Dear Marie,

**Re: Development Application No. 361/1223/2017/1B**



We respond to your letter of 7 July, 2017 as follows

1. Mud map attached showing site boundaries, buildings, access point, car parking areas and manoeuvring areas.
  - Surface treatment to manoeuvring areas – bitumen/concrete/road base
2. Description of trucks and photos attached
  - Two trucks owned by Ace Bitumen Contractors
  - 6 trucks privately owned which are contracted to Downer EDI Works to cart hotmix. One or two of these may occasionally be hired by Ace Bitumen to cart hotmix
  - Truck movement depends on the season and workload. For country work Ace need to start at around 6.00 a.m. and also in extreme heat we need to start early to avoid the hottest part of the day for employee safety. Normally we would not leave site before 6.30 a.m. and return not later than 5.00 p.m. Monday – Friday. Four of the hire trucks can work day or night shift and start and finish times depend on many factors, i.e. weather, workload, roster etc. as Downers have a pool of over 50 hire trucks. One of the hire trucks only works day shift and one mainly works away in the country in the warmer weather and is parked during winter.
  - Motor noise and brake noise are the same as most trucks. None of the trucks have loud exhausts and Jake brakes are not used. They all come and go empty. Reversing beepers would normally only be heard during the day but these need to be used for safety. Drivers are conscious of keeping noise to a minimum outside of normal working hours.
  - Mechanical repairs undertaken on site only on Ace vehicles and equipment during normal working hours by subcontractors.
  - Waste disposal – SA Oil Recovery and Suez Waste - during normal working hours
  - Office hours – 9.00 a.m. – 5.00 p.m.
  - Staff numbers – 3 office staff and 10 bitumen workers, only 3 of these workers come to the yard, the other 7 start from home
  - Photos of signage attached

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- 2 -

**City of Salisbury**  
**Attention: Ms Marie Molinaro**

**10 August, 2017**

There is not a steady movement of trucks at our premises as most are away for at least 8 hours. There are no operations carried out at the premises.

We wish to point out that there are several other businesses in the area that operate outside of normal business hours that also have deliveries and use forklifts for loading and unloading. Also, we have evidence of trucks parking outside our premises overnight.

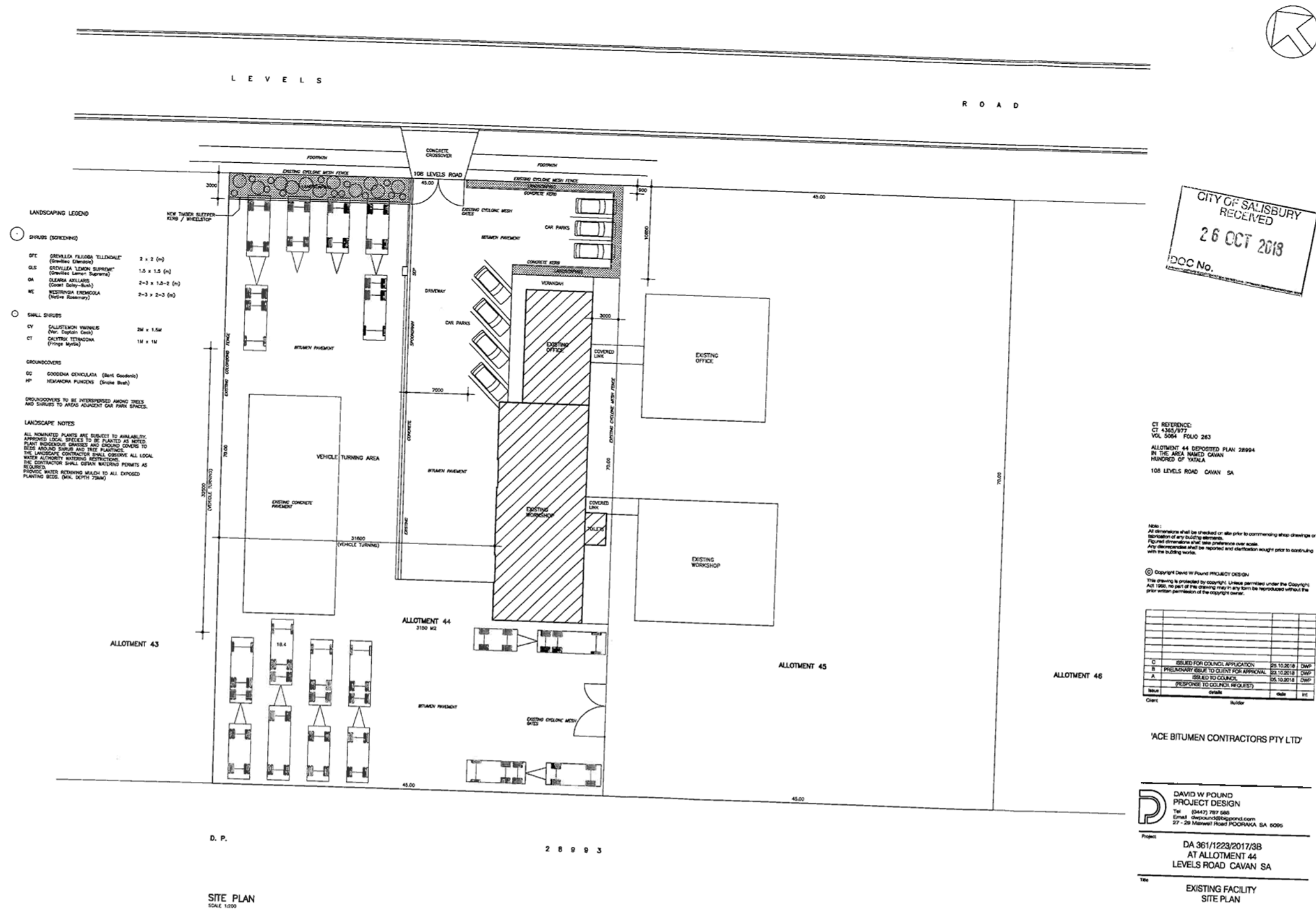
We trust that this information will assist Council to give favourable consideration to our application to amend the property usage to include truck parking.

Should you have any queries please do not hesitate to contact the writer.

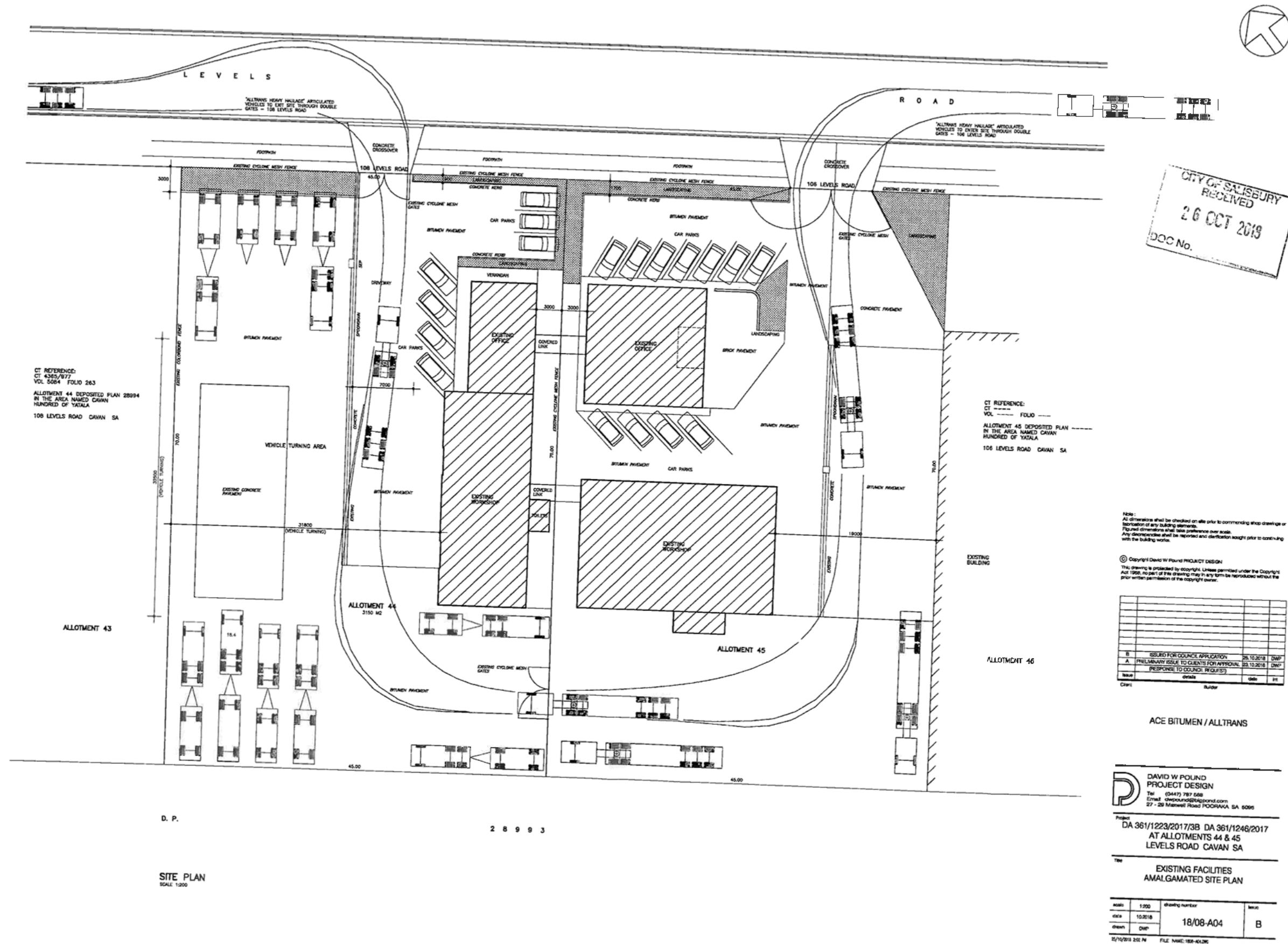
Yours faithfully,  
**Ace Bitumen Contractors Pty Ltd**



(JAN JARRETT)  
**Office Administrator**









# ACE Bitumen Contractors Pty Ltd

108 Levels Road

Environmental Noise Assessment

S5789C1

November 2018

# sonus.

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Prepared by  
Jason Turner (MAAS)

Reviewed by  
Chris Turnbull (MAAS)

ACE Bitumen Contractors Pty Ltd  
Environmental Noise Assessment  
S5789C1  
November 2018

sonus.

**Document Title** : ACE Bitumen Contractors Pty Ltd  
Environmental Noise Assessment

**Document Reference** : S5789C1

**Date** : November 2018

**Author** : Jason Turner, MAAS

**Reviewer** : Chris Turnbull, MAAS



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ACE Bitumen Contractors Pty Ltd  
Environmental Noise Assessment  
S5789C1  
November 2018

sonus.

## INTRODUCTION

An environmental noise assessment has been made of activity at ACE Bitumen Contractors Pty Ltd (ACE Bitumen), 108 Levels Road, Cavan (the site).

The site is proposed to be used as a truck depot which can be accessed at any time of the day and night. A combination of both ACE Bitumen trucks and those of an external company will use the site as a depot. The adjacent truck depot at 106 Levels Road is also proposed to use the site as a thoroughfare to enable a forward in and forward out maneuver through 106 Levels Road.

The closest residences to the site are on Petrel Crescent. The interface between Levels Road, which carries a range of heavy commercial traffic, and the residential area including Petrel Crescent, is addressed by a fence which extends above the eaves height of the dwellings (which is a good acoustic feature). A site layout and the closest residences to the proposal are shown in Appendix A.

The assessment determines appropriate noise criteria in accordance with the Development Plan and ensures that noise from the site achieves the requirements, such that the proposal does not unreasonably impact on the amenity of the nearby residences.

The assessment has been based on:

- A site inspection and measurements of the noise from trucks which will access the site on 18 October 2018;
- The potential for trucks to access the facility at any time of the day or night;
- Discussions with the operator regarding practical noise reduction measures; and,
- No loading or unloading activity occurring at the depot other than the occasional hitching or unhitching of trailers.

## CRITERIA

### Development Plan

The site is located within the Industry Zone of the Salisbury Council Development Plan and the existing residences are located within the Residential Zone. The Development Plan has been reviewed and particular consideration has been given to the following relevant provisions.

#### General Section:

#### Industrial Development

**Objective 4:** *Industrial development occurring without adverse effects on the health and amenity of occupiers of land in adjoining zones.*

#### PRINCIPLES OF DEVELOPMENT CONTROL

6. *Industrial development should minimise significant adverse impact on adjoining uses due to... noise...*

#### Interface between Land Uses

**Objective 1:** *Development located and designed to minimise adverse impact and conflict between land uses.*

**Objective 2:** *Protect community health and amenity from adverse impacts of development.*

**Objective 3:** *Protect desired land uses from the encroachment of incompatible development.*

#### PRINCIPLES OF DEVELOPMENT CONTROL

- 1 *Development should not detrimentally affect the amenity of the locality or cause unreasonable interference through any of the following:*
- ...
- (b) *noise*
- ...
- 2 *Development should be sited and designed to minimise negative impacts on existing and potential future land uses desired in the locality.*
- 6 *Non-residential development on land abutting a residential zone should be designed to minimise noise impacts to achieve adequate levels of compatibility between existing and proposed uses.*



**Noise Generating Activities**

- 7 *Development that emits noise (other than music noise) should include noise attenuation measures that achieve the relevant Environment Protection (Noise) Policy criteria when assessed at the nearest existing noise sensitive premises.*
- 8 *Development with the potential to emit significant noise (e.g. industry) should incorporate noise attenuation measures that prevent noise from causing unreasonable interference with the amenity of noise sensitive premises.*

**Environment Protection (Noise) Policy 2007**

The Development Plan provisions make specific reference to the *Environment Protection (Noise) Policy 2007* (the Policy). The Policy is based on the World Health Organisation Guidelines to prevent annoyance, sleep disturbance and unreasonable interference with the amenity of a locality. Therefore, compliance with the Policy is considered to satisfy the relevant provisions of the Development Plan related to environmental noise.

The Policy provides goal noise levels based on the principally promoted land use in the Development Plan Zones of the noise source (ACE Bitumen) and noise receivers (residences). Based on the Industry and Residential zoning of the development site and residences respectively, the policy applies the following goal noise levels;

- An average ( $L_{eq}$ ) noise level of 54 dB(A) during the day (7am to 10pm);
- An average ( $L_{eq}$ ) noise level of 45 dB(A) at night (before 7am or after 10pm); and,
- An instantaneous maximum ( $L_{max}$ ) noise level of 60 dB(A) at night (10pm to 7am).

When measuring or predicting noise levels for comparison with the average ( $L_{eq}$ ) goal noise levels of the Policy, penalties may be applied to the goal noise levels for each characteristic of tone, impulse, low frequency and modulation of the noise source. To apply a penalty, these characteristics of the noise source must be considered dominant in the context of the existing acoustic environment.

The existing acoustic environment at the site includes noise from trucks and passenger vehicles on Levels Road. The road separates the subject site from the residences (as shown in Appendix A). The residences are therefore subject to trucks and vehicles on Levels Road which are closer in proximity, travelling at greater speed and occurring at a greater frequency of occurrence in comparison to the low speed truck movements on the site.

ACE Bitumen Contractors Pty Ltd  
Environmental Noise Assessment  
S5789C1  
November 2018

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In such a circumstance, the noise from the forward movement of trucks and passenger vehicle activity on the site cannot adversely impact on the amenity of the locality other than in the following circumstances:

- The trucks on the site have to reverse and in so doing, introduce a noise source with a different characteristic (a reversing tone) into the environment;
- The trucks need to idle for long periods of time, which introduces a noise source with a different characteristic into the environment;
- Passenger vehicles opening and closing doors, which is a different characteristic to that which already regularly occurs in the environment.

Based on the above observations and influence of the existing public road network, the site layout has been designed such that trucks will only reverse into the parking bays during the day time period on their return to the site, and will enter and/or exit the site with forward-only movement during the night time hours (refer site plan in Appendix A).



**ASSESSMENT**

The assessment of the proposal has been based on a site inspection and noise measurements of the following activities on 18 October 2018;

- Trucks entering and exiting the site; and,
- Trucks starting up and idling.

It is understood that the site will be used by both ACE Bitumen trucks and those of another company as a depot and trucks from 106 Levels road will drive through the site in a forward direction to access their site. All of these trucks were measured during the site inspection and predictions of the noise from the site have been based on the highest noise level from the trucks. The following level of activity within any 15 minute period (the default assessment period of the Policy) has also been assumed;

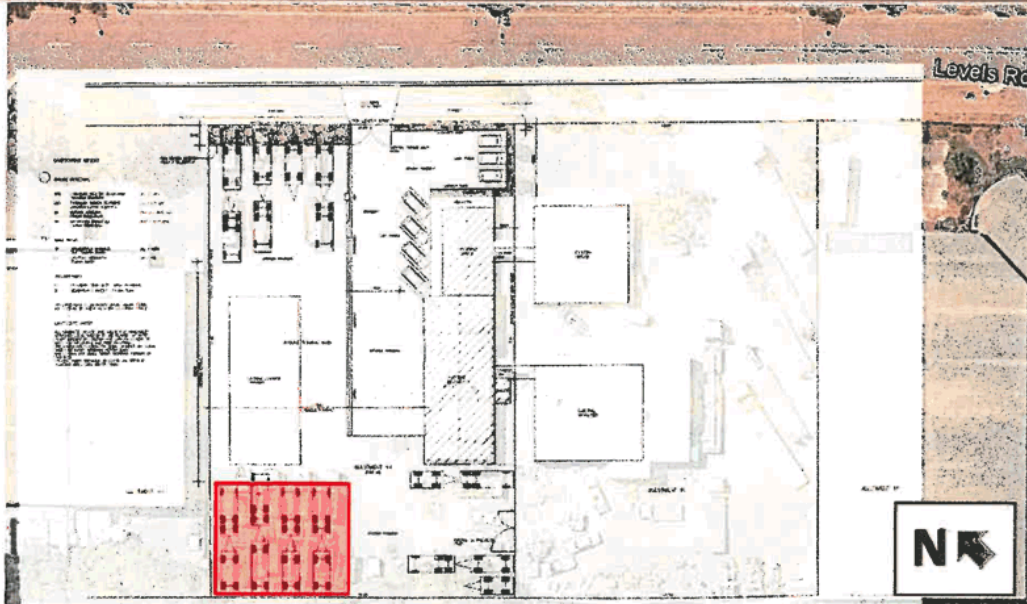
Day Time (7am to 10pm)	Night Time (10pm to 7am)
<ul style="list-style-type: none"> <li>• 2 Trucks either;                             <ul style="list-style-type: none"> <li>○ Enter the site and <b>reverse</b> into their parking position; or,</li> <li>○ Drive from their parked position out of the site.</li> </ul> </li> <li>• 2 trucks idle for up to 5 minutes each</li> <li>• 2 passenger vehicle movements move into or out of the site</li> </ul>	<ul style="list-style-type: none"> <li>• 2 Trucks either;                             <ul style="list-style-type: none"> <li>○ Enter the site and drive into a park in a <b>forward</b> direction only; or,</li> <li>○ Drive from their parked position out of the site.</li> </ul> </li> <li>• 2 trucks idle for up to 5 minutes each</li> <li>• 2 passenger vehicle movements move into or out of the site</li> </ul>

In order to achieve the goal noise levels of the Policy, the following acoustic treatments have been developed for the site;

- Ensure that trucks do not reverse while on site during the night time hours (10pm to 7am);
- Ensure no loading or unloading occurs on the site, with the exception of occasional hitching and unhitching of trailers, which should occur during the day time hours (7am to 10pm) only;
- Restrict the amount of time a truck can idle after entering or before leaving the site to be no more than 5 minutes;
- Restrict the area where trucks can park on the site to that shown in the following figure, when entering or exiting during the night time hours of the Policy (10pm to 7am).

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- Implement a Noise Management Plan which requires truck activity to be in accordance with the restrictions in this report.

With the above acoustic treatments incorporated, the predicted noise level at the existing residences is predicted to be no more than 45 dB(A) during the night time and 48 dB(A) during the day, therefore achieving the goal noise levels of the Policy (being 45 dB(A) and 54 dB(A) respectively).

The maximum noise level from activity such as truck brake compressed air release, people talking or opening and closing doors during the night time hours has also been predicted based on previous noise measurements at other similar sites. The highest predicted ( $L_{max}$ ) noise level at the residences is 57 dB(A), therefore easily achieving the 60 dB(A) goal noise level of the Policy.

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#### **CONCLUSION**

An environmental noise assessment has been made of the proposed ACE Bitumen Contractors Pty Ltd truck depot at 108 Levels Road, Cavan.

The assessment considers noise levels at the existing residences in the vicinity of the proposal based on specific noise measurements of the trucks which will access the site.

Appropriate noise criteria have been derived for the site based on the Development Plan, the *Environment Protection (Noise) Policy 2007*.

Predictions of the noise at residences will achieve the noise criteria where the following measures are implemented;

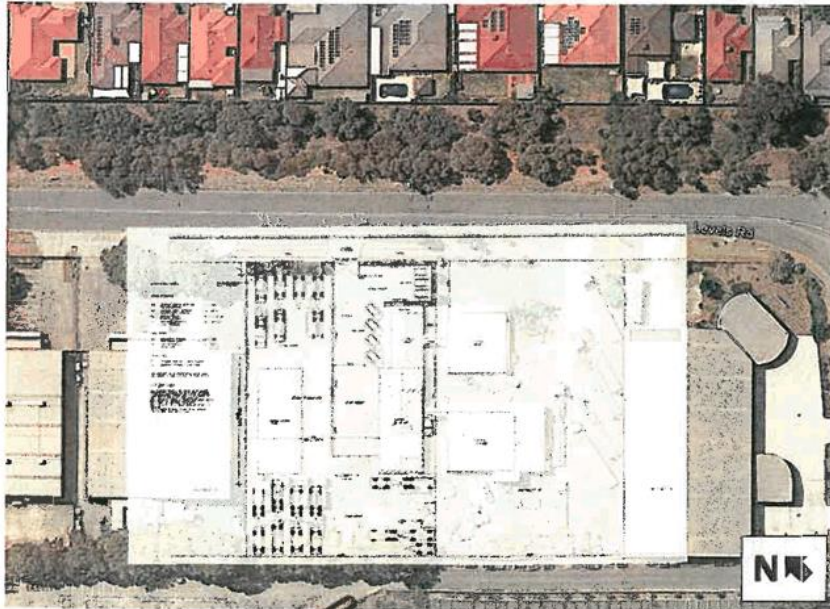
- Limiting the number of trucks using the site is limited;
- Restricting the time of truck idling;
- no loading or unloading occurring on site, with the exception of hitching and unhitching trailers during the day time; and,
- Restricting the area where trucks may park and leave the site from during the night time hours.

Based on the above, the development is considered to have been designed to prevent adverse impact, detrimental effects, and to not unreasonably interfere with the existing acoustic amenity of the community, thereby achieving the relevant provisions of the Development Plan.

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**APPENDIX A: Site and Residence Locality**



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**APPENDIX B: NOISE MANAGEMENT PLAN**

**ACE BITUMEN CONTRACTORS PTY LTD- NOISE MANAGEMENT PLAN**

**1. INTRODUCTION**

*This Noise Management Plan (the Plan) specifies an action plan and procedure for the management of environmental noise from the ACE Bitumen Pty Ltd truck depot at 108 Levels Road, Cavan.*

**2. SCOPE**

*The Plan includes the appropriate design and management of the site, and the requirements of all staff to minimise environmental noise emissions.*

**3. OBJECTIVE**

*The objective of the Plan is to satisfy the "Part 4 Clause 25 – General Environmental Duty" of the Environment Protection Act 1993 (the EP Act).*

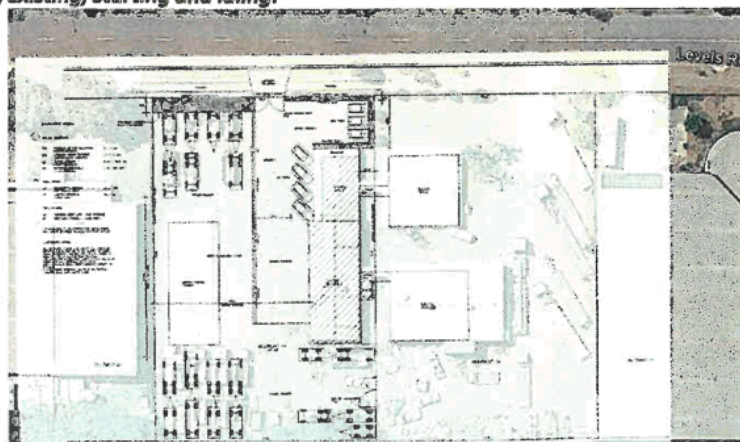
*Compliance with the policies and guidelines made under the EP Act, such as the Environment Protection (Noise) Policy 2007, is one method of ensuring compliance with the Clause 25 provisions, in circumstances where these objective standards cannot be achieved, then the Section 25 provisions can still be satisfied by taking all "reasonable and practicable" noise reduction measures. The extent of "reasonable and practicable" is subject to consideration of factors relevant to the individual circumstance.*

*In order to achieve the Environment Protection (Noise) Policy 2007 a noise level ( $L_{Aeq,10min}$ ) of no more than 45 dB(A) would need to be achieved during the night time hours (10pm to 7am) and no more than 54 dB(A) during the day time hours (7am to 10pm).*

**4. RISK ASSESSMENT**

**4.1. Site Layout and Associated Noise Sources**

*The site layout is shown in the following Figure, with the main noise sources at the site being trucks entering/existing, starting and idling:*

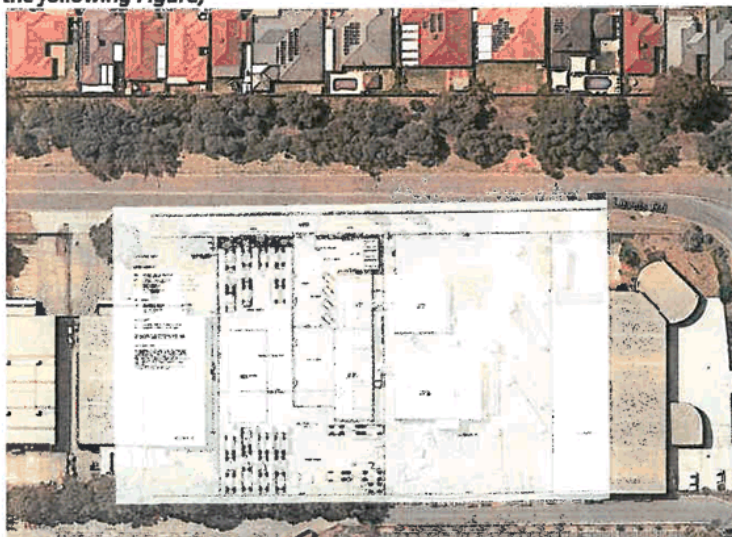


ACE Bitumen Contractors Pty Ltd  
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#### 4.2. Noise Sensitive Receivers

*The closest noise sensitive receivers to the site are the existing residences opposite Levels Road, as shown in the following Figure;*



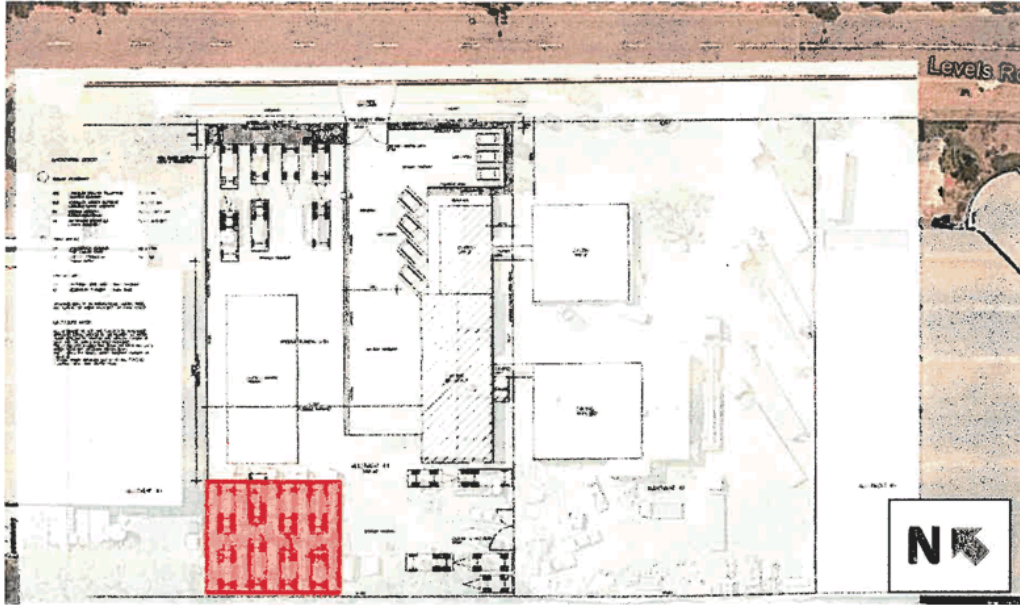
#### 4.3. Mitigation

*In order to satisfy the Act, all reasonable and practicable measures must be taken to minimise the noise impact at the surrounding residences through the implementation of this Plan. As a result of implementing this plan, noise from the site will also be reduced such that compliance with the Environment Protection (Noise) Policy 2007 will be achieved, thereby ensuring the General Environmental Duty is satisfied.*

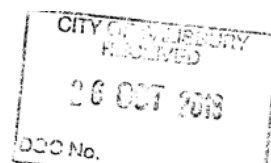
#### 5. NOISE MANAGEMENT MEASURES

*The noise management measures that will be implemented in order to minimise environmental noise impact on the surrounding residences are provided below.*

- *Ensure that trucks are not required to reverse while on site during the night time hours (10pm to 7am);*
- *Ensure no loading or unloading occurs on the site, with the exception of occasional hitching and unhitching of trailers during the day time hours (7am to 10pm) only;*
- *Restrict the amount of time a truck can idle after entering or before leaving the site to be no more than 5 minutes;*
- *Restrict the area where trucks can park on the site to that shown in the following figure, when entering or existing during the night time hours of the Policy (10pm to 7am).*



Item 5.1.3 - Attachment 1 - Proposal Plans and Supporting Information (including Acoustic Report)



1	ACE BITUMEN	ISUZU	SB17BS	FRONT TO BACK OF TRUCK	7.6 MTRS
				TRAILER FROM A FRAME TO BACK OF TRAILER	9.8 MTRS
T1	TRAILER #1	DOG	TPO781	TRAILER FROM A FRAME TO BACK OF TRAILER	6.3 MTRS
T2	TRAILER #2	DOG	TAB227	TRAILER FROM A FRAME TO BACK OF TRAILER	7 MTRS
2	TACK TRUCK	ISUZU	SB44GC	FRONT TO BACK OF TRUCK	8 MTRS
				TRAILER FROM A FRAME TO BACK OF TRAILER	4.5 MTRS
3	KEVIN PETERSEN	VOLVO	VOV123	FRONT TO BACK OF TRUCK	7.5 MTRS
				TRAILER FROM A FRAME TO BACK OF TRAILER	8.7 MTRS
4	ROSS CANDIDO	SCANIA	SB47BM	FRONT TO BACK OF TRUCK	7.5 MTRS
				TRAILER FROM A FRAME TO BACK OF TRAILER	9.6 MTRS
5	MARK LONGO	FREIGHTLINER	SB19AO	FRONT TO BACK OF TRUCK	8.8 MTRS
				TRAILER FROM A FRAME TO BACK OF TRAILER	9.6 MTRS
6	RICK CLARK	VOLVO	WRU242	FRONT TO BACK OF TRUCK	7 MTRS
				TRAILER FROM A FRAME TO BACK OF TRAILER	9.2 MTRS
7	LAURIE DINHAM	DAF	SB84GL	FRONT TO BACK OF TRUCK	7.5 MTRS
				TRAILER FROM A FRAME TO BACK OF TRAILER	10.3 MTRS
8	NICK LONGO	KENWORTH	SB57KG	FRONT TO BACK OF TRUCK	7.7 MTRS
				TRAILER FROM A FRAME TO BACK OF TRAILER	9.3 MTRS

FROM FRONT FENCE TO BACK FENCE 71 MTRS LONG

FROM SHED [2 ROLLER DOORS] TO NEIGHBOURS FENCE IS 32 MTRS

TURN AROUND IS 33MTRS x 34 MTRS

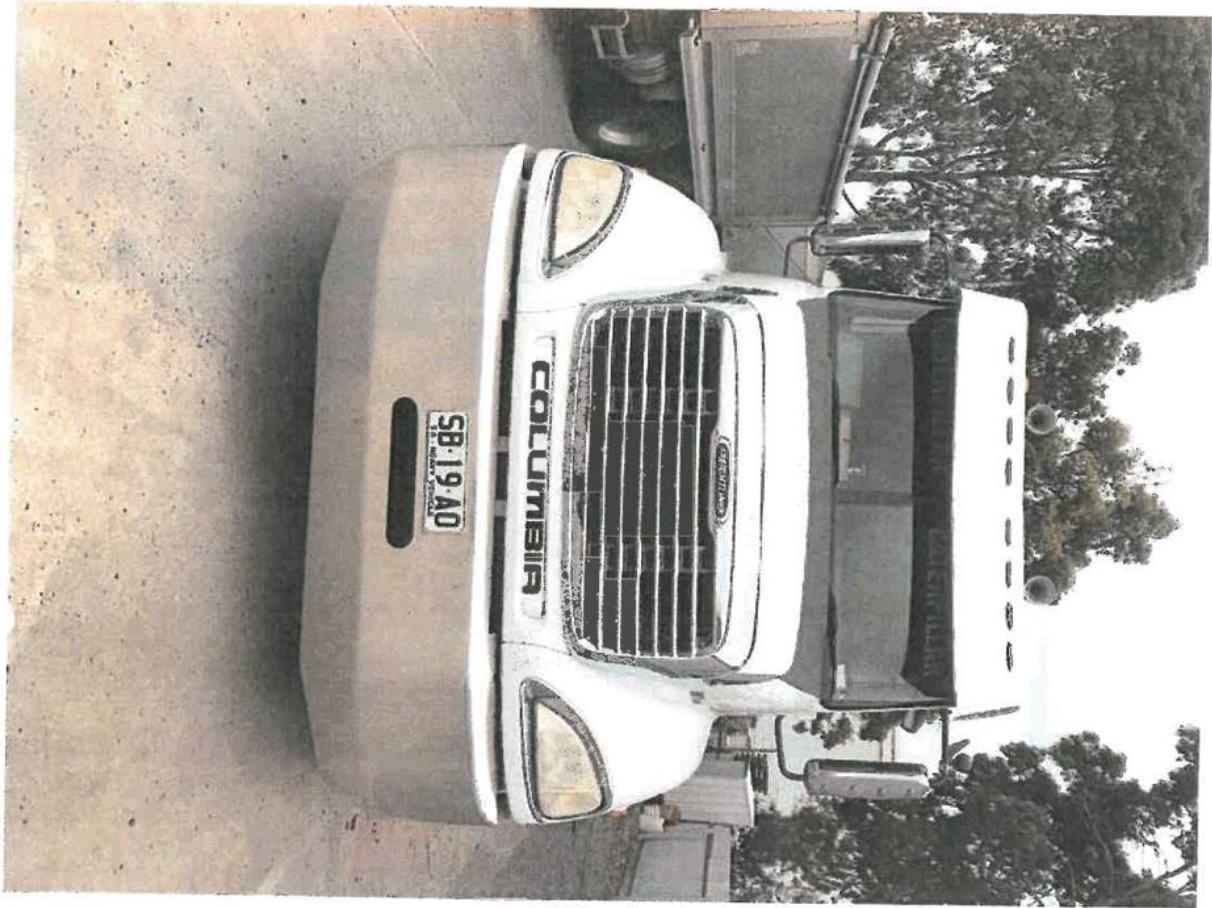
	<u>MAKE</u>	<u>REGISTRATION</u>	<u>GVM</u>	<u>GCM</u>
CHARLIE	ISUZU	SB17BS	24,000	50,000
GINO	ISUZU	SB44GC	10,400	16,000
NICK LONGO	KENWORTH	SB57KG	24,600	50,000
KEVIN PETERSON	VOLVO	VOV123	23,060	42,500
MARK LONGO	FREIGHTLINER	SB19AAO	24,000	50,000
RICK CLARK	VOLVO	WRU242	23,500	42,500
ROSS	SCANIA	SB47BM	26,000	70,000
LAURIE DINHAM	DAF	SB84GL	23,000	50,000





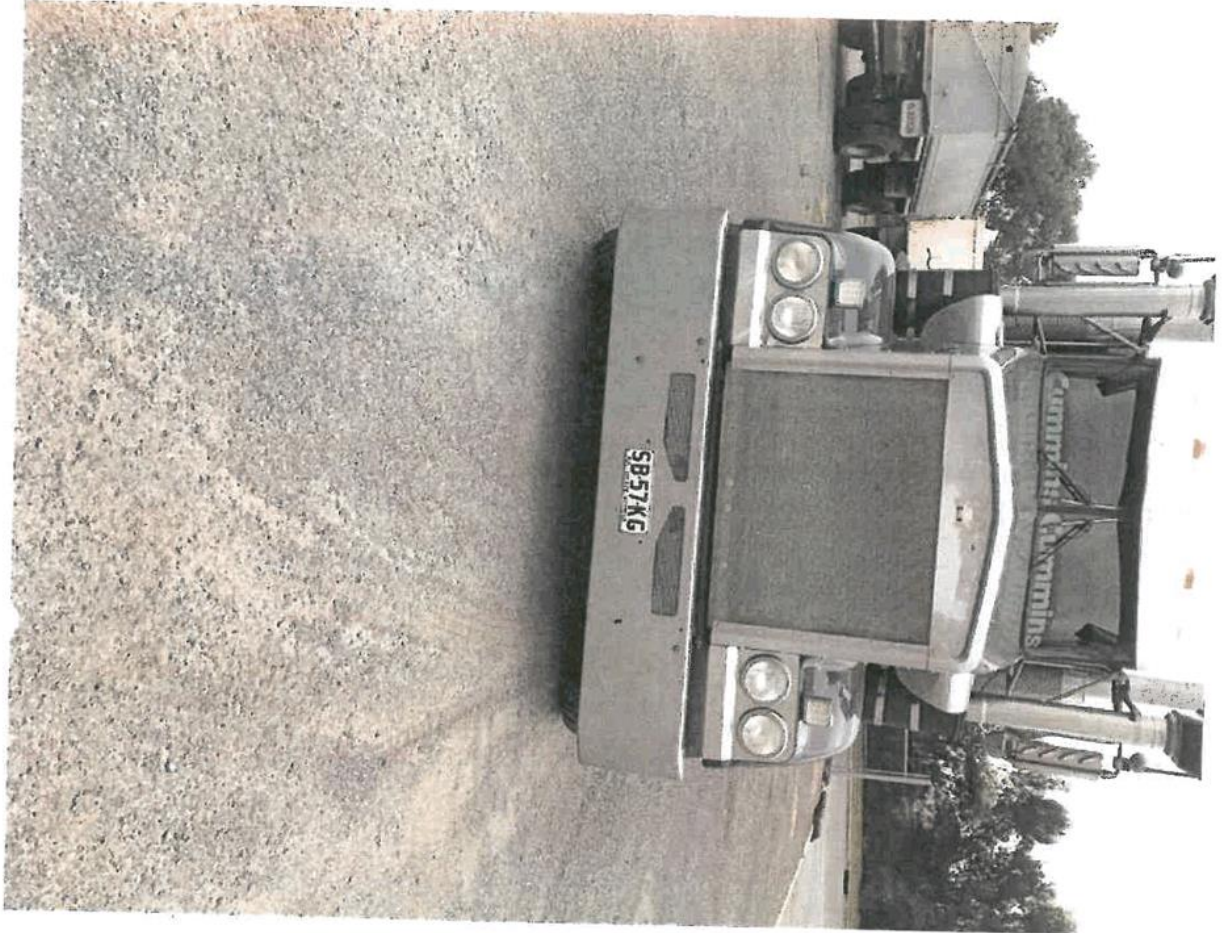










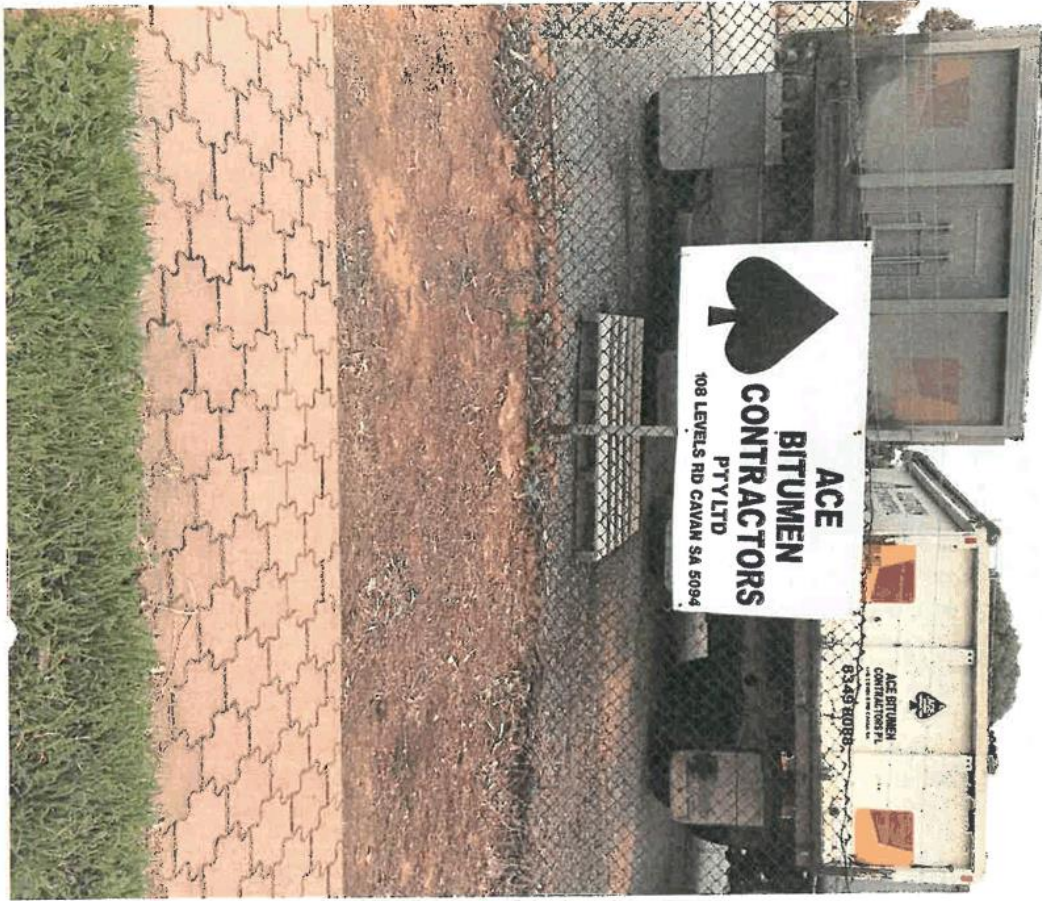


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10 AUG 2017



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10 AUG 2017

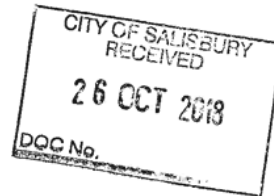






26 October, 2018

Mr Chris Carrey  
Development Officer – Planning  
City of Salisbury  
12 James Street  
SALISBURY SA 5108



Dear Sir,

We attach documents in support of our Development Application No. 361/1223/2017/1B.

Jason Turner of Sonus has advised that his report including Noise Management Plan will be available early next week and it will be forwarded to you as soon as received.

The plan of our premises drawn to scale by Mr David Pound does not show the manoeuvring of our largest vehicle as he does not have a template. We understand that he has spoken to you about this and you indicated that Council would have a template.

Should you require any further information please do not hesitate to contact the writer.

Yours faithfully,  
**Ace Bitumen Contractor Pty Ltd**

**JAN JARRETT**  
**Director**



**ACE BITUMEN  
CONTRACTORS**  
ABN 67 007 944 541 PTY LTD



25 October, 2018



Mrs Julie Frost  
Alltrans Heavy Haulage Pty Ltd  
106 Levels Road  
CAVAN SA 5094

Dear Julie,

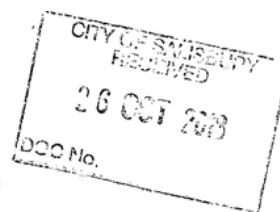
We advise that we are prepared to allow your Company's vehicles access to our property to either enter or exit through front and rear gates as required, especially after hours and at weekends.

This will eliminate the need for your vehicles to back off Levels Road, thus reducing the noise impact on the adjacent residents.

Yours faithfully,  
**Ace Bitumen Contractor Pty Ltd**

**JAN JARRETT**  
**Office Administrator**

## ACE BITUMEN CONTRACTORS DEPOT OPERATIONAL PLAN FOR NOISE MANAGEMENT



### PURPOSE & SCOPE

The purpose of this document is to provide practical guidance on how to minimise, to the fullest extent practicable, the impacts on the community from noise through the application of all feasible and reasonable mitigation measures.

For many infrastructure projects, construction works are required outside normal construction hours because work during daytime periods would be highly disruptive to commuter services and road traffic on major roads.

Living near commercial or industrial premises can mean extra noise in the neighbourhood. Although some noise may be unavoidable, it can often be controlled using improved work practices.

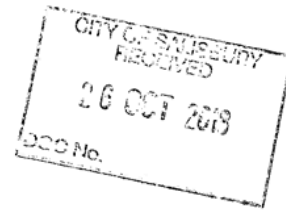
Although some construction activities are inherently noisy, the impact can be minimised by good work practices. We shall endeavour to take all reasonable and practical steps to minimise the impact upon our neighbours.

### PROPOSAL

The proposed mitigation measures that will be employed to minimise the impacts include:

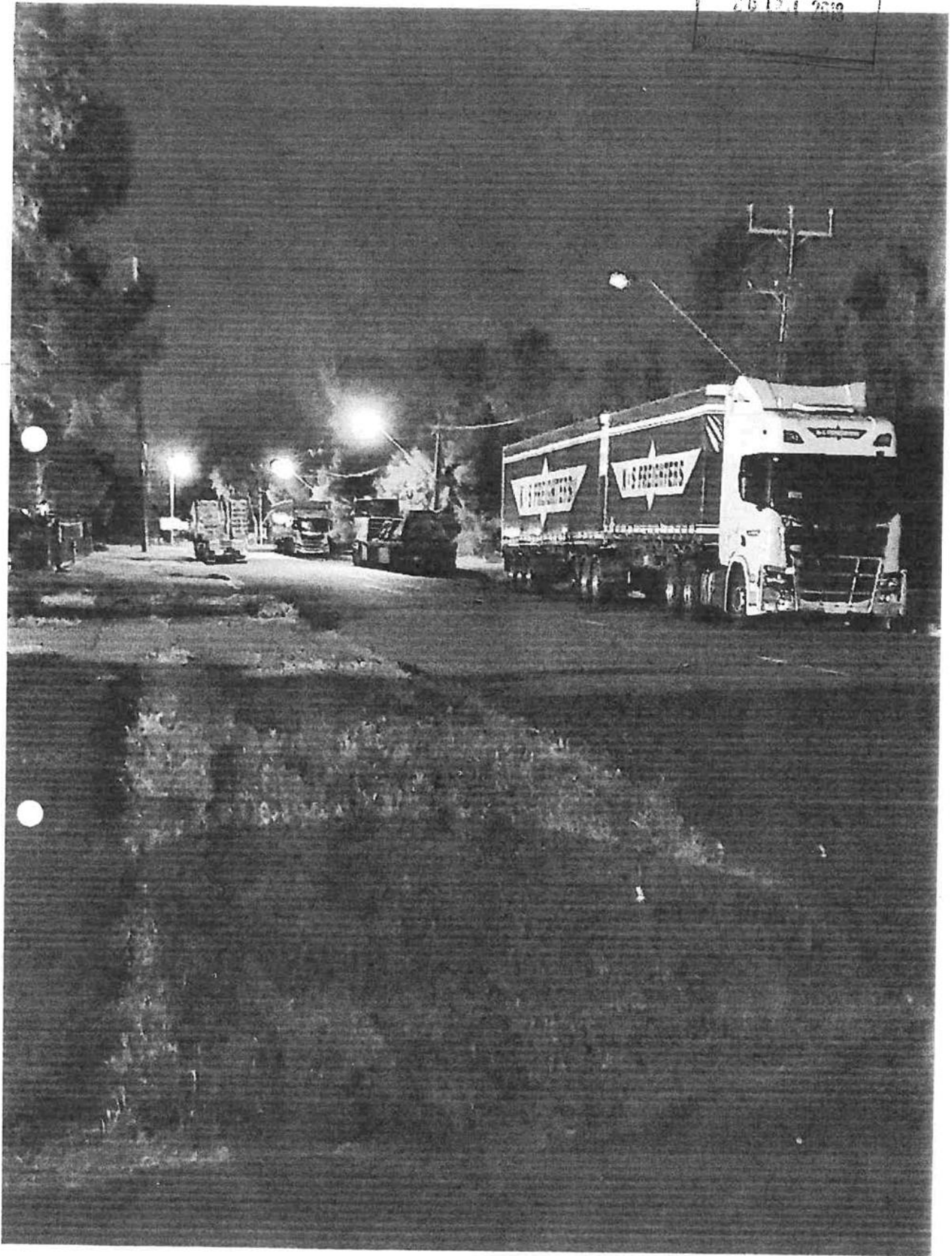
- All employees and sub-contractors are to be trained on the behaviours expected of them in regards to respecting our neighbours and where applicable limit our impact on our surrounding neighbours
- Drivers advised to turn on their truck fans when a few minutes from the depot and travel at around 50 kph
- When opening gates, do so quietly and quickly to avoid truck idling for too long
- Change of the truck parking layout in depot to eliminate the use of reversing beepers outside of normal business hours
- The use of reversing beepers which are a legal safety requirement are only to be used outside normal operating hours when there is no other option
- Truck warmup to take up to 5 minutes
- Truck shutdown to take less than 1 minute

## ACE BITUMEN CONTRACTORS PTY LTD



Other factors the Council should take into consideration

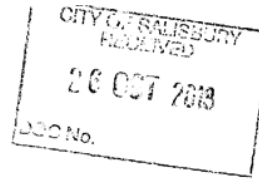
- Trucks parking in front of our premises at night/early morning, some refrigerated trucks with fridges running
- Adjacent properties unloading trucks with forklifts at night
- Hoon driving as evidenced by tyre marks on Levels Road



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26 OCT 2018

1:30AM FRIDAY 19TH OCTOBER, 2018

Attachment 2  
Vehicle Log Details provided by Applicant



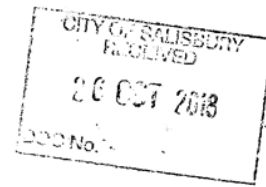
## ACE BITUMEN CONTRACTORS PTY LTD

The following log shows when we have commenced early. The main reasons are -

- Working in the country
- Working at schools where we are required to complete work before students arrive
- Working at shopping centres where we are required to complete work before shoppers arrive
- 7.00 a.m. on site inductions

As indicated we did not work for 30 days between February and August due to Rostered Days Off, Public Holidays, Wet Weather etc. Also, we close down for 5 weeks over the December/January period.

For employee safety in extreme heat we need to start early and finish early to avoid the hottest part of the day.

**ACE BITUMEN****ISUZU - SB17BS**

THURSDAY	1st FEBRUARY	6.30 AM	S/C PARAFIELD GARDENS
WEDNESDAY	7th FEBRUARY	6.15 AM	MINTARO
THURSDAY	8th FEBRUARY	6.15 AM	SCHOOL - PENNINGTON
FRIDAY	9th FEBRUARY	5.45 AM	SCHOOL - CHRISTIES DOWNS
TUESDAY	20th FEBRUARY	6.45 AM	ALDINGA
FRIDAY	23rd FEBRUARY	6.15 AM	NURIOOTPA
WEDNESDAY	28th FEBRUARY	6.15 AM	ALDINGA
<b>[DID NOT WORK 2 DAYS IN FEBRUARY]</b>			
THURSDAY	1st MARCH	6.15 AM	GREENFIELDS
FRIDAY	2nd MARCH	6.45 AM	~
WEDNESDAY	14th MARCH	6.45 AM	WILLASTON
FRIDAY	16th MARCH	6.30 AM	MILE END
MONDAY	19th MARCH	5.45 AM	BLYTH
TUESDAY	20th MARCH	5.45 AM	~
WEDNESDAY	21st MARCH	6.15 AM	EUDUNDA
THURSDAY	22nd MARCH	6.45 AM	~
FRIDAY	23rd MARCH	6.45 AM	SEAFORD MEADOWS
MONDAY	26th MARCH	6.45 AM	NURIOOTPA
TUESDAY	27th MARCH	6.45 AM	~
WEDNESDAY	28th MARCH	6.45 AM	OAKLANDS PARK
<b>[DID NOT WORK 5 DAYS IN MARCH]</b>			
TUESDAY	10th APRIL	6.45 AM	WINDSOR
THURSDAY	19th APRIL	6.00 AM	CAFÉ - FINDON
FRIDAY	20th APRIL	6.45 AM	SCHOOL - HENLEY BEACH
SATURDAY	21st APRIL	5.30 AM - 10.00 AM	REGENCY PARK
MONDAY	23rd APRIL	6.45 AM	SCHOOL - HENLEY BEACH

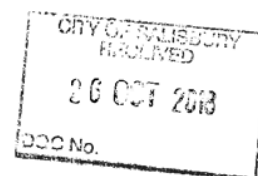
**[DID NOT WORK 8 DAYS IN APRIL]**

TUESDAY	1st MAY	6.15 AM	WHITE SANDS
WEDNESDAY	2nd MAY	6.15 AM	~
MONDAY	14th MAY	6.30 AM	GLENGOWRIE [FULTON HOGAN]
WEDNESDAY	16th MAY	5.30 AM	S/C UNLEY
FRIDAY	18th MAY	6.45 AM	GREENFIELDS
MONDAY	21st MAY	6.45 AM	GREENFIELDS
THURSDAY	24th MAY	5.45 AM	MANOORA
FRIDAY	25th MAY	6.45 AM	MODBURY HOSPITAL
WEDNESDAY	30th MAY	5.45 AM	HARVEY NORMAN - MUNNO PARA
THURSDAY	31st MAY	6.45 AM	HOVE

[DID NOT WORK 3 DAYS IN MAY]

FRIDAY	1st JUNE	6.15 AM	TANUNDA
MONDAY	4th JUNE	6.45 AM	TANUNDA
TUESDAY	5th JUNE	6.45 AM	~
WEDNESDAY	6th JUNE	6.45 AM	~
THURSDAY	7th JUNE	6.45 AM	LIGHTSVIEW
TUESDAY	19th JUNE	6.45 AM	NAIRNE
WEDNESDAY	20th JUNE	6.45 AM	WOOLWORTHS LOADING DOCK - MARDEN
FRIDAY	22nd JUNE	5.45 AM	MURRAY BRIDGE
MONDAY	25th JUNE	5.45 AM	MURRAY BRIDGE
TUESDAY	26th JUNE	6.45 AM	~
WEDNESDAY	27th JUNE	6.00 AM	~
THURSDAY	28th JUNE	6.15 AM	~
FRIDAY	29th JUNE	6.30 AM	PARAFIELD AIRPORT
SATURDAY	30th JUNE	6.15 AM - 11.15 AM	OUTER HARBOR

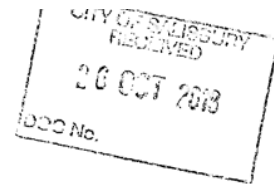
[DID NOT WORK 5 DAYS IN JUNE]





MONDAY	2nd JULY	6.30 AM	PARAFIELD AIRPORT
TUESDAY	3rd JULY	6.00 AM	FLINDERS UNI
WEDNESDAY	4th JULY	6.00 AM	MURRAY BRIDGE
MONDAY	9th JULY	6.00 AM	MURRAY BRIDGE
TUESDAY	10th JULY	6.00 AM	~
WEDNESDAY	11th JULY	6.00 AM	~
THURSDAY	12th JULY	6.00 AM	~
FRIDAY	13th JULY	6.00 AM	FLINDERS UNI
TUESDAY	17th JULY	6.30 AM	TUCKER STREET, ADELAIDE
WEDNESDAY	18th JULY	6.45 AM	SCHOOL - KLEMZIG
FRIDAY	20th JULY	6.00 AM	FLINDERS UNI
MONDAY	23rd JULY	6.00 AM	MURRAY BRIDGE
TUESDAY	24th JULY	5.45 AM	S/C FINDON
WEDNESDAY	25th JULY	6.45 AM	MURRAY BRIDGE
FRIDAY	27th JULY	6.00 AM	~
MONDAY	30th JULY	6.00 AM	SEAFORD HEIGHTS
<b>[DID NOT WORK 1 DAY IN JULY]</b>			
FRIDAY	3rd AUGUST	6.45 AM	INDUCTION - WINGFIELD
THURSDAY	9th AUGUST	6.45 AM	WEST BEACH
THURSDAY	16th AUGUST	6.15 AM	MCLAREN VALE
TUESDAY	21st AUGUST	6.15 AM	TANUNDA
WEDNESDAY	22nd AUGUST	6.45 AM	PLYMPTON PARK
THURSDAY	23rd AUGUST	6.15 AM	GRANGE
FRIDAY	24th AUGUST	6.15 AM	AVENUES COLLEGE - WINDSOR GARDENS
TUESDAY	28th AUGUST	6.45 AM	TUCKER STREET, ADELAIDE
WEDNESDAY	29th AUGUST	6.45 AM	ELIZABETH VALE
<b>[DID NOT WORK 6 DAYS IN AUGUST]</b>			
<b>[FROM FEBRUARY TO AUGUST - DID NOT WORK 30 DAYS]</b>			





5 September, 2018

Jarred Phillips – Logistics, Haulage & Transport, Downer EDI works advised the following dates for Night Work. He preferred not to disclose where they are working and advised that if Salisbury Council required further information they should contact either David Bendo, Business Improvement Manager or Steve Ainscow, SA Surfacing Manager

Thu 6/9/18

Fri 7/9/18

Sat 8/9/18

Sun 9/9/18

Mon 10/9/18

Tue 11/9/18

Wed 12/9/18

Thu 13/9/18

Fri 14/9/18

Mon 17/9/18

Tue 18/9/18

Wed 19/9/18

Thu 20/9/18

Fri 21/9/18

Sat 22/9/18

Sun 23/9/18

Mon 24/9/18

Tue 25/9/18

Wed 26/9/18

Thu 27/9/18

Fri 28/9/18

Sat 6/10/18

Sun 7/10/18

Every night until the end of October but no more weekends

Every week night in November but no weekends

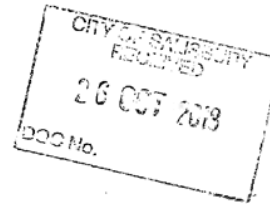
Every week night and weekend in December until 19 December

Closed 20 December – 7 January, 2019

The above is only a guide and is subject to change due to weather etc.

**DOWNER TRUCK DRIVERS:**

<b>ROSS CANDIDO</b>	<b>SCANIA - SB47BM</b>
<b>LAURIE DINHAM</b>	<b>DAF - SB84GL</b>
<b>MARK LONGO</b>	<b>FREIGHTLINER - SB19AO</b>
<b>NICK LONGO</b>	<b>KENWORTH - SB57KG</b>

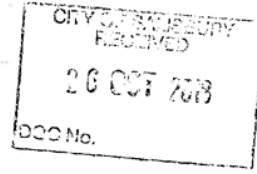


WHEN WORKING NIGHTSHIFT ARRIVE ANYWHERE BETWEEN 6.00 PM TO 9.00 PM & WARM UP TRUCK FOR UP TO 5 MINUTES  
& FINISH TIMES CAN BE ANYWHERE BETWEEN 2.00 AM AND 6.00 AM & TRUCK IS SHUT DOWN WITHIN A MINUTE.

**RICK CLARK**

**VOLVO - WRU242**

FROM APRIL 16TH - JUNE 11TH - TRUCK WAS LEFT PARKED



TUESDAY	12th JUNE	9.30 AM - 3.30 PM
WEDNESDAY	13th JUNE	9.00 AM - 3.15 PM
THURSDAY	14th JUNE	NO WORK
FRIDAY	15th JUNE	NO WORK
MONDAY	18th JUNE	9.00 AM - 3.15 PM
TUESDAY	19th JUNE	9.00 AM - 3.45 PM
WEDNESDAY	20th JUNE	11.45 AM - 11.45 PM
THURSDAY	21st JUNE	12.00 PM - 4.30 AM
FRIDAY	22nd JUNE	2.00 PM - 6.30 AM
MONDAY	25th JUNE	2.00 PM - 2.30 AM
TUESDAY	26th JUNE	2.00 PM - 11.15 AM
WEDNESDAY	27th JUNE	2.00 PM - 11.00 AM
THURSDAY	28th JUNE	10.00 AM - 4.00 PM
FRIDAY	29th JUNE	8.00 AM - 3.30 PM
MONDAY	2nd JULY	10.30 AM - 2.30 PM
TUESDAY	3rd JULY	NO WORK
WEDNESDAY	4th JULY	NO WORK
THURSDAY	5th JULY	10.00 AM - 2.00 PM
FRIDAY	6th JULY	NO WORK
MONDAY	9th JULY	NO WORK
TUESDAY	10th JULY	10.30 AM - 5.30 PM
WEDNESDAY	11th JULY	NO WORK
THURSDAY	12th JULY	NO WORK
FRIDAY	13th JULY	NO WORK
MONDAY	16th JULY	NO WORK
TUESDAY	17th JULY	NO WORK
WEDNESDAY	18th JULY	9.00 AM - 4.00 PM
THURSDAY	19th JULY	NO WORK
FRIDAY	20th JULY	6.00 AM - 5.00 PM
MONDAY	23rd JULY	NO WORK
TUESDAY	24th JULY	NO WORK
WEDNESDAY	25th JULY	NO WORK
THURSDAY	26th JULY	NO WORK
FRIDAY	27th JULY	NO WORK

MONDAY	23rd JULY	NO WORK
TUESDAY	24th JULY	NO WORK
WEDNESDAY	25th JULY	NO WORK
THURSDAY	26th JULY	NO WORK
FRIDAY	27th JULY	NO WORK
MONDAY	30th JULY	NO WORK
TUESDAY	31st JULY	NO WORK
WEDNESDAY	1st AUGUST	NO WORK
THURSDAY	2nd AUGUST	5.00 AM - 7.30 AM
FRIDAY	3rd AUGUST	NO WORK
MONDAY	6th AUGUST	5.15 AM - 10.15 AM
TUESDAY	7th AUGUST	NO WORK
WEDNESDAY	8th AUGUST	6.30 AM - 5.15 PM
THURSDAY	9th AUGUST	8.15 AM - 2.00 PM
FRIDAY	10th AUGUST	NO WORK
MONDAY	13th AUGUST	7.00 AM - 4.00 PM
TUESDAY	14th AUGUST	NO WORK
WEDNESDAY	15th AUGUST	8.00 AM - 2.00 PM
THURSDAY	16th AUGUST	9.00 AM - 2.30 PM
FRIDAY	17th AUGUST	7.00 AM - 4.00 PM
MONDAY	18th AUGUST	NO WORK
TUESDAY	19th AUGUST	NO WORK
WEDNESDAY	20th AUGUST	8.30 AM - 1.00 PM
THURSDAY	21st AUGUST	9.00 AM - 3.30 PM
FRIDAY	22nd AUGUST	9.30 AM - 3.00 PM
SUNDAY	24th AUGUST	7.30 AM - 3.30 PM
MONDAY	25th AUGUST	NO WORK
TUESDAY	26th AUGUST	NO WORK
WEDNESDAY	27th AUGUST	7.00 AM - 4.00 PM
THURSDAY	28th AUGUST	8.30 AM - 2.30 PM

**KEVIN PETERSON**

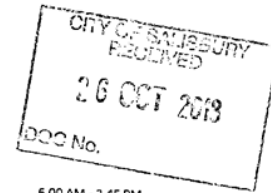
**VOLVO - VOV123**

FROM APRIL 16TH - JUNE 11TH - TRUCK WAS LEFT PARKED

MONDAY	8th JANUARY	7.30 AM - 12.30 PM
TUESDAY	9th JANUARY	6.30 AM - 4.30 PM
WEDNESDAY	10th JANUARY	6.30 AM - 4.00 PM
THURSDAY	11th JANUARY	6.30 AM - 2.45 PM
FRIDAY	12th JANUARY	8.00 AM - 1.00 PM
MONDAY	15th JANUARY	6.00 AM START WORKING AWAY UNTIL 19/01/2018
SATURDAY	20th JANUARY	6.00 AM - 2.00 PM
MONDAY	22nd JANUARY	6.30 AM - 2.00 PM
TUESDAY	23rd JANUARY	6.30 AM - 2.00 PM
WEDNESDAY	24th JANUARY	6.30 AM - 2.00 PM
THURSDAY	25th JANUARY	6.30 AM - 2.00 PM
MONDAY	29th JANUARY	6.00 AM START WORKING AWAY UNTIL 04/02/2018
WEDNESDAY	7th FEBRUARY	6.30 AM - 4.15 PM
THURSDAY	8th FEBRUARY	6.00 AM - 1.30 PM
FRIDAY	9th FEBRUARY	6.00 AM - 4.00 PM
MONDAY	12th FEBRUARY	6.30 AM - 4.30 PM
TUESDAY	13th FEBRUARY	6.30 AM - 3.00 PM
WEDNESDAY	14th FEBRUARY	6.30 AM - 3.30 PM
THURSDAY	15th FEBRUARY	6.30 AM - 2.00 PM
FRIDAY	16th FEBRUARY	6.00 AM - 5.00 PM
SATURDAY	17th FEBRUARY	6.00 AM - 12.30 PM
SUNDAY	18th FEBRUARY	6.30 AM - 12.30 PM
MONDAY	19th FEBRUARY	6.00 AM - 12.30 PM
TUESDAY	20th FEBRUARY	6.00 AM - 12.30 PM
WEDNESDAY	21st FEBRUARY	6.00 AM - 12.30 PM
THURSDAY	22nd FEBRUARY	6.00 AM - 12.30 PM
FRIDAY	23rd FEBRUARY	6.00 AM - 12.30 PM
MONDAY	26th FEBRUARY	7.00 AM - 5.00 PM
TUESDAY	27th FEBRUARY	6.30 AM - 5.30 PM
WEDNESDAY	28th FEBRUARY	6.30 AM - 2.00 PM
THURSDAY	1st MARCH	6.30 AM - 12.30 PM
MONDAY	4th MARCH	6.00 AM - 2.45 PM
TUESDAY	5th MARCH	6.00 AM - 7.00 PM
WEDNESDAY	6th MARCH	6.00 AM - 3.15 PM
FRIDAY	8th MARCH	6.30 AM - 5.30 PM
SATURDAY	9th MARCH	6.30 AM - 11.30 AM

MONDAY	4th MARCH	6.00 AM - 2.45 PM
TUESDAY	5th MARCH	6.00 AM - 7.00 PM
WEDNESDAY	6th MARCH	6.00 AM - 3.15 PM
FRIDAY	8th MARCH	6.30 AM - 5.30 PM
SATURDAY	9th MARCH	6.30 AM - 11.30 AM
WEDNESDAY	13th MARCH	6.00 AM START WORKING AWAY UNTIL 23/03/2018
TUESDAY	26th MARCH	6.00 AM - 5.00 PM
WEDNESDAY	27th MARCH	6.00 AM - 11.00 AM
THURSDAY	28th MARCH	6.00 AM - 2.00 PM
TUESDAY	3rd APRIL	6.00 AM START WORKING AWAY UNTIL 12/04/2018
MONDAY	16th APRIL	6.00 AM START WORKING AWAY UNTIL 19/04/2018
THURSDAY	26th APRIL	7.00 AM - 7.00 PM
FRIDAY	27th APRIL	6.30 AM - 3.00 PM
TUESDAY	1st MAY	7.00 AM - 2.00 PM
WEDNESDAY	2nd MAY	6.00 AM - 5.00 PM
THURSDAY	3rd MAY	6.00 AM - 10.00 AM
SATURDAY	5th MAY	6.30 AM - 3.00 PM
MONDAY	7th MAY	7.00 AM START WORKING AWAY UNTIL 10/05/2018
<b>NO FURTHER WORK UNTIL JUNE 12</b>		
TUESDAY	12th JUNE	6.30 AM - 8.00 PM
WEDNESDAY	13th JUNE	7.00 AM - 5.00 PM
THURSDAY	14th JUNE	6.30 AM - 4.00 PM
FRIDAY	15th JUNE	6.30 AM - 3.00 PM
MONDAY	18th JUNE	7.00 AM START WORKING AWAY UNTIL 20/06/2018
SATURDAY	23rd JUNE	6.00 AM - 1.30 PM
WEDNESDAY	27th JUNE	6.00 AM - 1.30 PM
THURSDAY	28th JUNE	6.00 AM - 1.30 PM
FRIDAY	29th JUNE	6.00 AM - 1.30 PM
SATURDAY	30th JUNE	6.00 AM - 1.30 PM

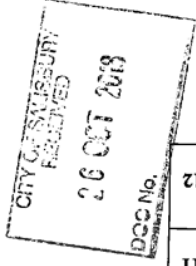
HAVE NOT WORKED SINCE 30/06/2018  
ONLY WARM UP TRUCK UNTIL AIR IS BUILT UP (APPROX. 5 MINS)





**Ace Bitumen Contractors**  
Pre Start-Up Vehicle Inspection Checklist

Vehicle Registration:  
Operator:



	Week 1	Week 2	Week 3	Week 4	Week 5	Week 6	Week 7	Week 8	Week 9	Week 10	Week 11	Week 12
<b>Under Bonnet:</b>												
Engine Oil												
Engine Coolant												
Brake Fluid level												
Belts: Alternator, P/Steering												
Power Steering fluid (if applicable)												
Hydraulic clutch fluid (if applicable)												
Air Intake & Exhaust components OK												
All engine components look serviceable												
<b>Walk Around Inspection:</b>												
All lights operating (inc. trailer)												
Warning signal working (if fitted)												
All tyres in good condition (inc. trailer)												
All tyres correct pressures (inc. trailer)												
Mirrors clean and serviceable												
Seat Belts serviceable / operating OK												
Windscreen wipers & horn operating OK												
Check if vehicle is due for service Y / N												
Steering components serviceable												
<b>After start-up:</b>												
Check no warning indicator lights												
Listen for any odd noises												
Enough fuel												
<b>Comments / any problems (notice any fluid leaks etc)?</b>												
<b>Signature:</b>												

Ace Bitumen Contractors 10.2018

Attachment 3  
Notice of Category 3 Application and Representations  
received

**DEVELOPMENT ACT 1993  
CITY OF SALISBURY**

**NOTICE OF APPLICATION FOR CATEGORY 3 DEVELOPMENT**  
Pursuant to Section 38(5) of the Development Act, 1993

An application for development has been lodged with the Council for assessment. The details are as follows:

<b>DEVELOPMENT NO:</b>	361/1223/2017/3B
<b>APPLICANT:</b>	Ace Bitumen Contractors Pty Ltd 108 Levels Rd CAVAN SA 5094
<b>NATURE OF THE DEVELOPMENT:</b>	CHANGE OF USE TO TRUCK PARKING DEPOT & ASSOCIATED SIGNAGE
<b>LOCATED AT:</b>	108 Levels Road , Cavan SA 5094
<b>CERTIFICATE OF TITLE:</b>	CT-5084/263
<b>ZONE:</b>	Industry

The application may be examined at the Office of the Council located at 12 James Street, Salisbury during business hours (8.30am – 5pm Monday to Friday) and on Council’s web site at [www.salisbury.sa.gov.au](http://www.salisbury.sa.gov.au). Any person or body may make representations in writing, or by email to [development@salisbury.sa.gov.au](mailto:development@salisbury.sa.gov.au), concerning this application and should address their representation to the Chief Executive Officer at PO Box 8, Salisbury or [representations@salisbury.sa.gov.au](mailto:representations@salisbury.sa.gov.au). Representations must be received **no later than Tuesday 5<sup>th</sup> September 2017.**

Each person making a submission should indicate whether they wish to appear personally, or be represented by another party, in support of their submission. Please note that should you nominate to be heard in support of your representation, you will be required to attend a Development Assessment Panel meeting held at the Council offices, scheduled on the fourth Tuesday of each month at 6.00pm (unless otherwise advised).

Please note that, pursuant to Section 38(8) of the Development Act, a copy of each representation received will be forwarded to the applicant to allow them to respond to all representations received.

Signed: Marie Molinaro, Development Officer

Date: 21 August 2017

***THIS IS THE FIRST AND ONLY PUBLICATION OF THIS NOTICE***





**STATEMENT OF REPRESENTATION**  
**Pursuant to Section 38 of the Development Act**

**To:** City of Salisbury  
PO Box 8, SALISBURY SA 5108  
Email: [representations@salisbury.sa.gov.au](mailto:representations@salisbury.sa.gov.au)

THIS SHEET PROVIDES YOU WITH THE OPPORTUNITY TO MAKE COMMENTS IN RELATION TO A PROPOSED DEVELOPMENT. PLEASE FIND ATTACHED DETAILS OF THE PROPOSED DEVELOPMENT.

Development Number:	361/1223/2017/3B
Applicant:	Ace Bitumen Contractors Pty Ltd
Location:	108 Levels Road , Cavan SA 5094
Proposed Development:	CHANGE OF USE TO TRUCK PARKING DEPOT & ASSOCIATED SIGNAGE

**YOUR DETAILS:** (this information must be provided to ensure that this is a valid representation)

NAME(S): Darren Golley  
ADDRESS: 15 Petrel Crescent MANSION LAKES  
PHONE NO: [REDACTED] EMAIL: [REDACTED]

I am: (please tick one of the following boxes as appropriate)

- The owner/occupier of the property located at: .....
- Other (please state): .....

**YOUR COMMENTS:**

I/We: (please tick the most appropriate box below)

- Support the proposed development.
- Oppose the proposed development.

**Whether you support or oppose this proposal you must provide written reasons below to ensure that this is a valid representation.**

Have been living in the location since December 2002. Over the past 4 or 5 year the business behind my property have change hands and seem to change hours of operation which brings unwanted noise to my area. I want to be able to enjoy

PTO

361/1223/2017/3B

some quiet time after work and on weekends and this is current interrupted by trucks entering and exiting these location @ 108 & 106 levels Road. The type of noise is disruptive to sleep and enjoyment even inside my house with the tv going & doors shut. In summer in the evenings I want to enjoy the outside entertainment area I have created but the enjoyment is constantly interrupted by trucks idling for up to 1hr, air brakes, low pitched engine noise, loading & unloading of large heavy equipment. During normal working hrs Mon - Fri 6am - 6pm is OK, but anytime after this is not acceptable due to the close proximity of the residential area. I have been working with the EPA & the Salisbury Council on this directly for sometime & I need for businesses to understand its not OK to do what they like. Therefore I take a great interest in the application point 2 dot point 3 and object to any work outside of the stated business hours. My concerns would be addressed by: (state changes/actions to the proposal sought)

Making it mandatory to not allow any business of any kind (This being trucks, parking, leaving engines idle for long periods of time, entering & exiting) After 6pm - Monday - Friday or before 6am Mon - Friday, Or any activity on Saturday or Sundays & Public holidays.

If this occurs constantly then the council provide an avenue for this to be policed & address with quick & swift response. This company may not be the current business that I am constantly taking issue with, but they recognise that another business in the direct vicinity is doing what they like, this is AU-TRANS Heavy haulage -> next door,

PTO

Regulation 35(e) of the Development Regulations 2008 requires that a representation must indicate a person's desire to be heard. Please note that if you do not indicate that you wish to be heard, it will be taken that you do not wish to be heard by the Panel.

I/We:

Do not wish to be heard in support of my representation.

Wish to be heard in support of my representation, and I will be:

Appearing personally,

**OR**

Represented by the following person: .....

Contact details: .....

*(Please note, matters raised in your written representation will be considered during the assessment and do not need to be repeated at the hearing).*

**Your written representation must be received by Council no later than 11.59pm on Tuesday 5<sup>th</sup> September 2017, to ensure that it is a valid representation and taken into account.**

**Representor's Declaration:**

I am aware that the representation will become a public document as prescribed in the Freedom of Information Act 1991, and will be made available to the applicant, agencies and other bodies pursuant to the Development Act 1993, and may be uploaded to the Council's website as an attachment to the hearing agenda.

Signature:  .....

Date: 5 / 9 / 17.

**Please complete this checklist to ensure your representation is valid:**

- Name and address of person (or persons).
- If more than one person, details of person making the representation.
- Detail of reasons for making the representation.
- Indication whether or not the person (or persons) wishes to be heard.
- Submitted no later than 11.59pm on **Tuesday 5<sup>th</sup> September 2017.**

05 SEP 2017

**STATEMENT OF REPRESENTATION**  
Pursuant to Section 38 of the Development Act

valid



To: City of Salisbury  
PO Box 8, SALISBURY SA 5108  
Email: [representations@salisbury.sa.gov.au](mailto:representations@salisbury.sa.gov.au)

- 5 SEP 2017

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Development Number:	361/1223/2017/3B
Applicant:	Ace Bitumen Contractors Pty Ltd
Location:	108 Levels Road , Cavan SA 5094
Proposed Development:	CHANGE OF USE TO TRUCK PARKING DEPOT & ASSOCIATED SIGNAGE

**YOUR DETAILS:** (this information must be provided to ensure that this is a valid representation)

NAME(S): Amanda Bailey

ADDRESS: 25 PRION CIRCUIT MAWSON LAKES

PHONE NO: [REDACTED] .... EMAIL [REDACTED]

I am: (please tick one of the following boxes as appropriate)

- The owner/occupier of the property located at: 25 PRION CIRCUIT MAWSON LAKES
- Other (please state): .....

**YOUR COMMENTS:**

I/We: (please tick the most appropriate box below)

- Support the proposed development.
- Oppose the proposed development.

**Whether you support or oppose this proposal you must provide written reasons below to ensure that this is a valid representation.**

I oppose the proposed development due to concerns over an increase to noise levels, which is already a problem, especially early mornings. This is a residential area.

PTO

Regulation 35(e) of the Development Regulations 2008 requires that a representation must indicate a person's desire to be heard. Please note that if you do not indicate that you wish to be heard, it will be taken that you do not wish to be heard by the Panel.

I/We:

- Do not wish to be heard in support of my representation.
- Wish to be heard in support of my representation, and I will be:
- Appearing personally,
- OR**
- Represented by the following person: .....
- Contact details: .....

*(Please note, matters raised in your written representation will be considered during the assessment and do not need to be repeated at the hearing).*

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Signature:     cBailey     Date:     3 / 9 / 2017    

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**Please complete this checklist to ensure your representation is valid:**

- Name and address of person (or persons).
- If more than one person, details of person making the representation.
- Detail of reasons for making the representation.
- Indication whether or not the person (or persons) wishes to be heard.
- Submitted no later than 11.59pm on **Tuesday 5<sup>th</sup> September 2017.**

Item 5.1.3 - Attachment 3 - Notice of category 3 Application and Representations Received

V. Kelly

**STATEMENT OF REPRESENTATION**  
Pursuant to Section 38 of the Development Act



**To:** City of Salisbury  
PO Box 8, SALISBURY SA 5108  
Email: [representations@salisbury.sa.gov.au](mailto:representations@salisbury.sa.gov.au)

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Development Number:	361/1223/2017/3B
Applicant:	Ace Bitumen Contractors Pty Ltd
Location:	108 Levels Road , Cavan SA 5094
Proposed Development:	CHANGE OF USE TO TRUCK PARKING DEPOT & ASSOCIATED SIGNAGE

**YOUR DETAILS:** (this information must be provided to ensure that this is a valid representation)

NAME(S): ..... Robin Ford .....

ADDRESS: ..... 108 Levels Road Cavan .....

PHONE NO: ..... [REDACTED] ..... EMAIL: ..... [REDACTED] .....

I am: (please tick one of the following boxes as appropriate)

- The owner/occupier of the property located at: ..... 108 Levels Road Cavan SA .....
- Other (please state): .....

**YOUR COMMENTS:**

I/We: (please tick the most appropriate box below)

- Support the proposed development.
- Oppose the proposed development.

**Whether you support or oppose this proposal you must provide written reasons below to ensure that this is a valid representation.**

We Support this business and have no issue  
with traffic or noise during the week or on Saturdays  
when we are at the depot

PTO

361/1223/2017/3B

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**My concerns would be addressed by:** *(state changes/actions to the proposal sought)*

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PTO

Item 5.1.3 - Attachment 3 - Notice of category 3 Application and Representations Received

Regulation 35(e) of the Development Regulations 2008 requires that a representation must indicate a person's desire to be heard. Please note that if you do not indicate that you wish to be heard, it will be taken that you do not wish to be heard by the Panel.

I/We:

Do not wish to be heard in support of my representation.

Wish to be heard in support of my representation, and I will be:

Appearing personally,

**OR**

Represented by the following person: .....

Contact details: .....

*(Please note, matters raised in your written representation will be considered during the assessment and do not need to be repeated at the hearing).*

**Your written representation must be received by Council no later than 11.59pm on Tuesday 5<sup>th</sup> September 2017, to ensure that it is a valid representation and taken into account.**

**Representor's Declaration:**

I am aware that the representation will become a public document as prescribed in the Freedom of Information Act 1991, and will be made available to the applicant, agencies and other bodies pursuant to the Development Act 1993, and may be uploaded to the Council's website as an attachment to the hearing agenda.

Signature:  .....

Date: 30/8/17

**Please complete this checklist to ensure your representation is valid:**

- Name and address of person (or persons).
- If more than one person, details of person making the representation.
- Detail of reasons for making the representation.
- Indication whether or not the person (or persons) wishes to be heard.
- Submitted no later than 11.59pm on **Tuesday 5<sup>th</sup> September 2017.**



valid

**STATEMENT OF REPRESENTATION**  
**Pursuant to Section 38 of the Development Act**



**To:** City of Salisbury  
PO Box 8, SALISBURY SA 5108  
Email: [representations@salisbury.sa.gov.au](mailto:representations@salisbury.sa.gov.au)

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Development Number:	361/1223/2017/3B
Applicant:	Ace Bitumen Contractors Pty Ltd
Location:	108 Levels Road , Cavan SA 5094
Proposed Development:	CHANGE OF USE TO TRUCK PARKING DEPOT & ASSOCIATED SIGNAGE

**YOUR DETAILS:** (this information must be provided to ensure that this is a valid representation)

NAME(S): KORA BAY PTY LTD  
ADDRESS: H WYFIELD ST. WATTLE PARK S.A  
PHONE NO: ..... EMAIL: .....

I am: (please tick one of the following boxes as appropriate)

- The owner/occupier of the property located at: 110 Levels Rd. Cavan SA ✓
- Other (please state): .....

**YOUR COMMENTS:**

I/We: (please tick the most appropriate box below)

- Support the proposed development.
- Oppose the proposed development.

**Whether you support or oppose this proposal you must provide written reasons below to ensure that this is a valid representation.**

We believe that Ace Bitumen has been operating in a proper manner. They have never caused inconvenience to us,

PTO

Item 5.1.3 - Attachment 3 - Notice of category 3 Application and Representations Received

361/1223/2017/3B

and on using the property  
for industrial use.  
We recommend their application

**My concerns would be addressed by:** *(state changes/actions to the proposal sought)*

PTO

Regulation 35(e) of the Development Regulations 2008 requires that a representation must indicate a person's desire to be heard. Please note that if you do not indicate that you wish to be heard, it will be taken that you do not wish to be heard by the Panel.

I/We:

- Do not wish to be heard in support of my representation.
- Wish to be heard in support of my representation, and I will be:
  - Appearing personally,
  - OR**
  - Represented by the following person: .....
  - Contact details: .....

*(Please note, matters raised in your written representation will be considered during the assessment and do not need to be repeated at the hearing).*

**Your written representation must be received by Council no later than 11.59pm on Tuesday 5<sup>th</sup> September 2017, to ensure that it is a valid representation and taken into account.**

**Representor's Declaration:**

I am aware that the representation will become a public document as prescribed in the Freedom of Information Act 1991, and will be made available to the applicant, agencies and other bodies pursuant to the Development Act 1993, and may be uploaded to the Council's website as an attachment to the hearing agenda.

Signature:  .....

Date: 23/8/17

**Please complete this checklist to ensure your representation is valid:**

- Name and address of person (or persons).
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- Detail of reasons for making the representation.
- Indication whether or not the person (or persons) wishes to be heard.
- Submitted no later than 11.59pm on **Tuesday 5<sup>th</sup> September 2017.**

Valid.

**STATEMENT OF REPRESENTATION**  
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Location:	108 Levels Road , Cavan SA 5094
Proposed Development:	CHANGE OF USE TO TRUCK PARKING DEPOT & ASSOCIATED SIGNAGE

**YOUR DETAILS:** (this information must be provided to ensure that this is a valid representation)

NAME(S): THE GIVIANI NOMINEES NO 2  
ADDRESS: 4 WYFIELD ST MATTHE PARK S.A  
PHONE NO: ..... EMAIL: .....

I am: (please tick one of the following boxes as appropriate)

- The owner/occupier of the property located at: 112 LEVELS RD CAVAN ✓
- Other (please state): .....

**YOUR COMMENTS:**

I/We: (please tick the most appropriate box below)

- Support the proposed development.
- Oppose the proposed development.

**Whether you support or oppose this proposal you must provide written reasons below to ensure that this is a valid representation.**

THE PRESENT TENANTS OF 108 LEVELS  
RD. HAVE BEEN OCCUPANTS FOR QUITE  
SOME TIME. AT NO TIME HAVE  
THEY EVER CAUSED US ANY INCONVENIENCE

PTO

361/1223/2017/3B

on Reason To Complain.  
We Agree With The Approval.

**My concerns would be addressed by:** *(state changes/actions to the proposal sought)*

.....

PTO

Regulation 35(e) of the Development Regulations 2008 requires that a representation must indicate a person's desire to be heard. Please note that if you do not indicate that you wish to be heard, it will be taken that you do not wish to be heard by the Panel.

I/We:

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  - Wish to be heard in support of my representation, and I will be:
    - Appearing personally,
    - OR**
    - Represented by the following person: .....
- Contact details: .....

*(Please note, matters raised in your written representation will be considered during the assessment and do not need to be repeated at the hearing).*

**Your written representation must be received by Council no later than 11.59pm on Tuesday 5<sup>th</sup> September 2017, to ensure that it is a valid representation and taken into account.**

**Representor's Declaration:**

I am aware that the representation will become a public document as prescribed in the Freedom of Information Act 1991, and will be made available to the applicant, agencies and other bodies pursuant to the Development Act 1993, and may be uploaded to the Council's website as an attachment to the hearing agenda.

Signature: 

Date: 23/8/17

**Please complete this checklist to ensure your representation is valid:**

- Name and address of person (or persons).
- If more than one person, details of person making the representation.
- Detail of reasons for making the representation.
- Indication whether or not the person (or persons) wishes to be heard.
- Submitted no later than 11.59pm on **Tuesday 5<sup>th</sup> September 2017.**

Attachment 4  
Applicant Response to Representations



10 October 2017

Marie Molinaro – Development Officer  
Email: [mmolinaro@salisbury.sa.gov.au](mailto:mmolinaro@salisbury.sa.gov.au)

Dear Ms. Molinaro,

RE: Response to Representations for DA 361/1223/2017/3B at  
108 Levels Road, Cavan

MasterPlan SA have been engaged by Giulia Properties Pty Ltd ('our client') to review and respond to representations made during the public notification period for Development Application 361/1223/2017/3B located at 108 Levels Road, Cavan.

Having reviewed the five submissions forwarded by Council following the Category 3 notification of the proposed development, we note that only two are opposed and three offer their support (objectors are in **bold**):

- **D L Golley at 15 Petrel Crescent, Mawson Lakes (object);**
- **A E Bailey at 25 Prion Circuit, Mawson Lakes (object);**
- R Frost at 106 Levels Road, Mawson Lakes (support);
- Kora Bay Pty Ltd at 4 Wyfield Street, Wattle Park (support); and
- Giuliani Nominees Pty Ltd at 4 Wyfield Street, Wattle Park (support).

The matters raised in the representations opposing the development can be summarised as follows:

1. Type and level of noise after work and weekends.
2. Operating hours.

Please find our response to the matters of concern as expressed in the representations summarised below:

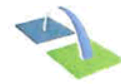
33 Caundington Street  
Adelaide, 5000  
P (08) 8199 5600  
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ISO 9001:2015 Certified  
ABN 30 007 755 277  
plan@masterplan.com.au



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The subject site is located within a long standing industrial area, that pre-dates the Residential (Mawson Lakes – Policy Area 22) Zoned land to the north and east. Located within this industrial zone are land uses including freight/transport distribution, warehousing and industrial/manufacturing, all of which are permitted land uses within the Industry Zone.

The nature of our client's business requires trucks to occasionally enter and exit the site prior to normal business hours, depending on the weather, as bitumen cannot be laid during the extreme heat of summer or periods of heavy rain. However, this is not the norm and would only occur when necessary. Works on Sundays and Public Holidays would also be avoided where possible, but cannot be ruled out completely, during the same weather conditions as described above.

Having regard to the above, the proposed development is considered to have appropriate regard to the relevant Objectives and Principles of Development Control (PDC) associated with 'Industrial Development' in the following manner:

- Objective 1 – the development is located within an industrial area; where heavy vehicle movement is both expected and is common;
- Objective 4 – the proposed development will not have adverse impacts on the health and amenity of occupiers in adjoining zones given the small scale of the operation in comparison to other industrial land uses within the immediate locality;
- PDC 6 – impacts on adjoining industrial land uses is minimal as the proposed land use does not involve any processing or manufacturing which would generate high levels of noise, fumes, dust, paint or chemical over-spray;
- PDC 7 – the subject site is located approximately 40 metres from the closest residential allotment, and is separated from these residential uses by Levels Road and a 15 metre wide landscaping strip which is densely vegetated;
- PDC 7(d) - no fixed hours of operation are proposed as our client's business is susceptible to extreme weather events, and therefore requires flexibility when they occur. It should be noted that our client predominantly operates within the hours of 7.00 am and 6.00 pm and avoids work on Sundays and Public Holidays unless required to do so under certain weather conditions; and
- PDC 8 – a 15 metre landscaping strip is located on the opposing side of Levels Road, which is heavily vegetated screening the industrial area from the adjacent residential properties.

Furthermore, the proposed development is considered to have appropriate regard to the relevant Objectives and Principles of Development Control (PDC) associated with the 'Interface between Land Uses' provisions of the Development Plan in the following manner:



- PDC 1 – the proposed development will not detrimentally affect the amenity of the locality which comprises a wide variety of large scale industrial land uses, including freight, manufacturing and warehousing;
- PDC 2 – the proposed development does not negatively impact on existing and potential land uses desired within the Industry Zone;
- PDC 3 – the proposed development does not comprise any built form and therefore will not result in overlooking or shadowing;
- PDC 6 – noise impacts on the adjacent residential area are considered inconsequential given the subject site is located within a long standing industrial area comprising multiple industrial complexes which utilise heavy vehicles and trucks, such as the subject site; and
- PDC 7 – the only noise generating activity occurring on the subject site is the entry and egress of heavy vehicles which are associated with almost every industrial premise within the immediate locality.

It is also important to note that the subject site is smaller than other industrial tenancies within the immediate locality, with other much larger freight transport and logistics companies and sites within the immediate vicinity which undoubtedly significantly contribute to the assertions made about heavy vehicle noise raised in the two representations submitted to Council.

In conclusion, we recognise that two residentially zoned properties have raised concerns relating to noise and hours of operation, however these concerns must be balanced against the fact that the subject site is one of many industrial properties within the immediate locality, which is located within a long standing industrial area and Zone. Many of these businesses, and the Industry Zone itself, pre-date residential development in this area, and therefore it is incumbent upon prospective purchasers to be aware of their surrounds, including adjacent commercial and industrial operations, when purchasing a residential property.

We respectfully request that Council proceed with the assessment of this development application to Council's Assessment Panel (CAP).

Please note that our client or its representative reserves the right to appear at the CAP meeting to elaborate on this response and to answer any questions that may arise.

Yours sincerely,

**Grant Croft**  
MasterPlan SA Pty Ltd

cc: Pat Giuliani.

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Attachment 5  
Relevant Development Plan Extracts and Location  
Maps consolidated 15 December 2016

## Advertisements

### OBJECTIVES

- 1 Urban and rural landscapes that are not disfigured by advertisements and/or advertising hoardings.
- 2 Advertisements and/or advertising hoardings that do not create a hazard.
- 3 Advertisements and/or advertising hoardings designed to enhance the appearance of the building and locality.

### PRINCIPLES OF DEVELOPMENT CONTROL

- 1 The location, siting, design, materials, size, and shape of advertisements and/or advertising hoardings should be:
  - (a) consistent with the predominant character of the urban or rural landscape
  - (b) in harmony with any buildings or sites of historic significance or heritage value in the area
  - (c) co-ordinated with and complement the architectural form and design of the building they are to be located on.
- 2 The number of advertisements and/or advertising hoardings associated with a development should be minimised to avoid:
  - (a) clutter
  - (b) disorder
  - (c) untidiness of buildings and their surrounds
  - (d) driver distraction.
- 3 Buildings occupied by a number of tenants should exhibit co-ordinated and complementary advertisements and/or advertising hoardings to identify the tenants and their type of business.
- 4 The content of advertisements should be limited to information relating to the legitimate use of the associated land.
- 5 Advertisements and/or advertising hoardings should:
  - (a) be completely contained within the boundaries of the subject allotment
  - (b) be sited to avoid damage to, or pruning or lopping of, on-site landscaping or street trees
  - (c) not obscure views to vistas or objects of high amenity value.
- 6 Advertisements and/or advertising hoardings should not be erected on:
  - (a) a public footpath or veranda post
  - (b) a road, median strip or traffic island
  - (c) a vehicle adapted and exhibited primarily as an advertisement

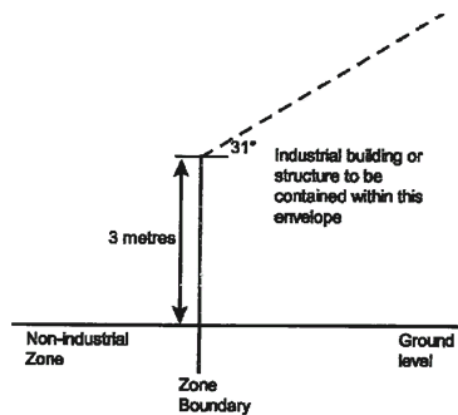
## Industrial Development

### OBJECTIVES

- 1 Industrial, warehouse, storage and transport distribution development on appropriately located land, integrated with transport networks and designed to minimise potential impact on these networks.
- 2 The development of small scale agricultural industries, wineries, mineral water extraction and processing plants, and home based industries in rural areas.
- 3 Industrially zoned allotments and uses protected from encroachment by adjoining uses that would reduce industrial development or expansion.
- 4 Industrial development occurring without adverse effects on the health and amenity of occupiers of land in adjoining zones.
- 5 Compatibility between industrial uses within industrial zones.
- 6 The improved amenity of industrial areas.
- 7 Co-location of industries in townships to enable promotion and implementation of innovative waste recovery practices, methods of power generation and reuse of by-products.

### PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Offices and showrooms associated with industrial, warehouse, storage and transport development should be sited at the front of the building with direct and convenient pedestrian access from the main visitor parking area.
- 2 Any building or structure on, or abutting the boundary of, a non-industrial zone should be restricted to a height of 3 metres above ground level at the boundary and a plane projected at 31 degrees above the horizontal into the development site from that 3 metre height, as shown in the following diagram:



- 3 Industrial development should enable all vehicles to enter and exit the site in a forward direction.
- 4 Industrial development abutting an arterial road, a non-industrial zone boundary, or significant open space should be developed in a manner that does not create adverse visual impacts on the locality.

**Salisbury Council  
General Section  
Industrial Development**

5 Building facades facing a non-industrial zone, public road, or public open space should:

- (a) use a variety of building finishes
- (b) not consist solely of metal cladding
- (c) contain materials of low reflectivity
- (d) incorporate design elements to add visual interest
- (e) avoid large expanses of blank walls.

**6 Industrial development should minimise significant adverse impact on adjoining uses due to hours of operation, traffic, noise, fumes, smell, dust, paint or other chemical over-spray, vibration, glare or light spill, electronic interference, ash or other harmful or nuisance-creating impacts.**

7 Development within 50 metres of the Residential Zone boundary should:

- (a) demonstrate appropriate acoustic performance
- (b) ensure that all noise sources including machinery, outside loading, unloading and other service areas are located away from the Residential Zone boundary
- (c) comprise buildings of masonry or equivalent construction to minimise the transmission of noise with openings located away from residential properties
- (d) limit operating hours to between 7am and 6 pm
- (e) where there is a railway on the boundary development should:
  - (i) ensure the rear walls of the industrial premises are sited on the rear boundary of the allotments
  - (ii) incorporate building materials that will minimise the reflection of railway traffic noise towards the residential area opposite
  - (iii) where a wall is not located on the boundary, landscaping, including mounding, land sculpting and/or thick planting, is to be established between the rear walls of the industrial premises and the railway in order to minimise the reflection of railway traffic noise.

8 Landscaping should be incorporated as an integral element of industrial development along non-industrial zone boundaries.

9 Fencing (including colour-coated wire mesh fencing) adjacent to public roads should be set back in one of the following ways:

- (a) in line with the building facade
- (b) behind the building line
- (c) behind a landscaped area that softens its visual impact.

10 Marine aquaculture onshore storage, cooling and processing facilities should not impair the coastline and its visual amenity and should:

- (a) be sited, designed, landscaped and developed at a scale and using external materials that minimise any adverse visual impact on the coastal landscape
- (b) be sited and designed with appropriate vehicular access arrangement
- (c) include appropriate waste treatment and disposal.

## Infrastructure

### OBJECTIVES

- 1 Infrastructure provided in an economical and environmentally sensitive manner.
- 2 Infrastructure, including social infrastructure, provided in advance of need.
- 3 Suitable land for infrastructure identified and set aside in advance of need.
- 4 The visual impact of infrastructure facilities minimised.
- 5 The efficient and cost-effective use of existing infrastructure.

### PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development should not occur without the provision of adequate utilities and services, including:
  - (a) electricity supply
  - (b) water supply
  - (c) drainage and stormwater systems
  - (d) waste disposal
  - (e) effluent disposal systems
  - (f) formed all-weather public roads
  - (g) telecommunications services
  - (h) social infrastructure, community services and facilities
  - (i) gas services.
- 2 Development should only occur where it provides, or has access to, relevant easements for the supply of infrastructure.
- 3 Development should incorporate provision for the supply of infrastructure services to be located within common service trenches where practicable.
- 4 Development should not take place until adequate and co-ordinated drainage of the land is assured.
- 5 Development in urban areas should not occur without provision of an adequate reticulated domestic quality mains water supply and an appropriate waste treatment system.
- 6 In areas where no reticulated water supply is available, buildings whose usage is reliant on a water supply should be equipped with an adequate and reliable on-site water storage system.
- 7 Electricity infrastructure should be designed and located to minimise its visual and environmental impacts.
- 8 Development and landscaping within 25 metres of the 275 kV overhead electricity lines should ensure that all clearances and safety restrictions are met.

## Interface between Land Uses

### OBJECTIVES

- 1 Development located and designed to minimise adverse impact and conflict between land uses.
- 2 Protect community health and amenity from adverse impacts of development.
- 3 Protect desired land uses from the encroachment of incompatible development.

### PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development should not detrimentally affect the amenity of the locality or cause unreasonable interference through any of the following:
  - (a) the emission of effluent, odour, smoke, fumes, dust or other airborne pollutants
  - (b) noise
  - (c) vibration
  - (d) electrical interference
  - (e) light spill
  - (f) glare
  - (g) hours of operation
  - (h) traffic impacts.
- 2 Development should be sited and designed to minimise negative impacts on existing and potential future land uses desired in the locality.
- 3 Development adjacent to a **Residential Zone** should be designed to minimise overlooking and overshadowing of adjacent dwellings and private open space.
- 4 Residential development adjacent to non-residential zones and land uses should be located, designed and/or sited to protect residents from potential adverse impacts from non-residential activities.
- 5 Sensitive uses likely to conflict with the continuation of lawfully existing developments and land uses desired for the zone should be designed to minimise negative impacts.
- 6 Non-residential development on land abutting a residential zone should be designed to minimise noise impacts to achieve adequate levels of compatibility between existing and proposed uses.

### Noise Generating Activities

- 7 Development that emits noise (other than music noise) should include noise attenuation measures that achieve the relevant *Environment Protection (Noise) Policy* criteria when assessed at the nearest existing noise sensitive premises.
- 8 Development with the potential to emit significant noise (e.g. industry) should incorporate noise attenuation measures that prevent noise from causing unreasonable interference with the amenity of noise sensitive premises.



## Landscaping, Fences and Walls

### OBJECTIVES

- 1 The amenity of land and development enhanced with appropriate planting and other landscaping works, using locally indigenous plant species where possible.
- 2 Functional fences and walls that enhance the attractiveness of development.

### PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development should incorporate open space and landscaping and minimise hard paved surfaces in order to:
  - (a) complement built form and reduce the visual impact of larger buildings (eg taller and broader plantings against taller and bulkier building components)
  - (b) enhance the appearance of road frontages
  - (c) screen service yards, loading areas and outdoor storage areas
  - (d) minimise maintenance and watering requirements
  - (e) enhance and define outdoor spaces, including car parking areas
  - (f) maximise shade and shelter
  - (g) assist in climate control within and around buildings
  - (h) minimise heat absorption and reflection
  - (i) maintain privacy
  - (j) maximise stormwater re-use
  - (k) complement existing vegetation, including native vegetation
  - (l) contribute to the viability of ecosystems and species
  - (m) promote water and biodiversity conservation.
- 2 Landscaping should:
  - (a) include the planting of locally indigenous species where appropriate
  - (b) be oriented towards the street frontage
  - (c) result in the appropriate clearance from powerlines and other infrastructure being maintained.
- 3 Landscaping should not:
  - (a) unreasonably restrict solar access to adjoining development
  - (b) cause damage to buildings, paths and other landscaping from root invasion, soil disturbance or plant overcrowding

## Orderly and Sustainable Development

### OBJECTIVES

- 1 Orderly and economical development that creates a safe, convenient and pleasant environment in which to live.
- 2 Development occurring in an orderly sequence and in a compact form to enable the efficient provision of public services and facilities.
- 3 Development that does not jeopardise the continuance of adjoining authorised land uses.
- 4 Development that does not prejudice the achievement of the provisions of the Development Plan.
- 5 Development abutting adjoining Council areas having regard to the policies of that Council's Development Plan.
- 6 Urban development contained within existing townships and settlements and located only in zones designated for such development.

### PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development should not prejudice the development of a zone for its intended purpose.
- 2 Land outside of townships and settlements should primarily be used for primary production and conservation purposes.
- 3 The economic base of the region should be expanded in a sustainable manner.
- 4 Urban development should form a compact extension to an existing built-up area.
- 5 Ribbon development should not occur along the coast, water frontages or arterial roads shown in *Overlay Maps - Transport*.
- 6 Development should be located and staged to achieve the economical provision of public services and infrastructure, and to maximise the use of existing services and infrastructure.
- 7 Where development is expected to impact upon the existing infrastructure network (including the transport network), development should demonstrate how the undue effect will be addressed.
- 8 Vacant or underutilised land should be developed in an efficient and co-ordinated manner to not prejudice the orderly development of adjacent land.

**Salisbury Council**  
**General Section**  
**Transportation and Access**

- 5 Land uses that generate large numbers of visitors such as shopping centres and areas, places of employment, schools, hospitals and medium to high density residential uses should be located so that they can be serviced by existing transport networks and encourage walking and cycling.
- 6 Development generating high levels of traffic, such as schools, shopping centres and other retail areas, entertainment and sporting facilities, should incorporate passenger pick-up and set down areas. The design of such areas should ensure interference to existing traffic is minimised and give priority to pedestrians, cyclists and public and community transport users.
- 7 The location and design of public and community transport set-down and pick-up points should maximise safety and minimise the isolation and vulnerability of users.
- 8 Development should provide safe and convenient access for all anticipated modes of transport including cycling, walking, public and community transport, and motor vehicles.
- 9 Development at intersections, pedestrian and cycle crossings, and crossovers to allotments should maintain or enhance sightlines for motorists, cyclists and pedestrians to ensure safety for all road users and pedestrians.
- 10 Driveway crossovers affecting pedestrian footpaths should maintain the level of the footpath.
- 11 Development should discourage commercial and industrial vehicle movements through residential streets and adjacent other sensitive land uses such as schools.
- 12 Industrial/commercial vehicle movements should be separated from passenger vehicle car-parking areas.
- 13 Development should make sufficient provision on site for the loading, unloading and turning of all traffic likely to be generated.

**Cycling and Walking**

- 14 Development should ensure that a permeable street and path network is established that encourages walking and cycling through the provision of safe, convenient and attractive routes with connections to adjoining streets, paths, open spaces, schools, public and community transport stops and activity centres.
- 15 Development should provide access, and accommodate multiple route options, for cyclists by enhancing and integrating with:
  - (a) open space networks, recreational trails, parks, reserves and recreation areas
  - (b) Adelaide's Metropolitan Open Space System.
- 16 Cycling and pedestrian networks should be designed to be permeable and facilitate direct and efficient passage to neighbouring networks and facilities.
- 17 New developments should give priority to and not compromise existing designated bicycle routes.
- 18 Where development coincides with, intersects or divides a proposed bicycle route or corridor, development should incorporate through-access for cyclists.
- 19 Developments should encourage and facilitate cycling as a mode of transport by incorporating end-of-journey facilities including:
  - (a) showers, changing facilities, and secure lockers
  - (b) signage indicating the location of bicycle facilities

- (c) secure bicycle parking facilities provided at the rate set out in [Table Sall3 - Off Street Bicycle Parking Requirements](#).
- 20 Pedestrian facilities and networks should be designed and provided in accordance with relevant provisions of the *Australian Standards and Austroads Guide to Traffic Engineering Practice Part 13*.
- 21 Cycling facilities and networks should be designed and provided in accordance with the relevant provisions of the *Australian Standards and Austroads Guide to Traffic Engineering Practice Part 14*.

#### Access

- 22 Development should have direct access from an all weather public road.
- 23 Development should be provided with safe and convenient access which:
  - (a) avoids unreasonable interference with the flow of traffic on adjoining roads
  - (b) provides appropriate separation: distances from existing roads or level crossings
  - (c) accommodates the type and volume of traffic likely to be generated by the development or land use and minimises induced traffic through over-provision
  - (d) is sited and designed to minimise any adverse impacts on the occupants of and visitors to neighbouring properties.
- 24 Development should not restrict access to publicly owned land such as recreation areas.
- 25 The number of vehicle access points onto arterial roads shown on *Overlay Maps - Transport* should be minimised, and where possible access points should be:
  - (a) limited to local roads
  - (b) shared between developments.
- 26 The number of access points for cyclists and pedestrians onto all adjoining roads should be maximised.
- 27 Development with access from roads with existing or projected traffic volumes exceeding 6000 vehicles per day should be sited to avoid the need for vehicles to reverse on to or from the road.
- 28 Development with access from arterial roads or roads as shown on *Overlay Maps - Transport* should be sited to avoid the need for vehicles to reverse on to or from the road.
- 29 Driveways, access tracks and parking areas should be designed and constructed to:
  - (a) follow the natural contours of the land
  - (b) minimise excavation and/or fill
  - (c) minimise the potential for erosion from run-off
  - (d) avoid the removal of existing vegetation
  - (e) be consistent with *Australian Standard AS 2890 Parking facilities*.

#### Access for People with Disabilities

- 30 Development should be sited and designed to provide convenient access for people with a disability.
- 31 Where appropriate and practical, development should provide for safe and convenient access to the coast and beaches for disabled persons.

**Salisbury Council**  
**General Section**  
**Transportation and Access**

**Vehicle Parking**

- 32 Development should provide off-street vehicle parking and specifically marked disabled car parking places to meet anticipated demand in accordance with Table Sall2 - Off Street Vehicle Parking Requirements or Table Sall2A - Off Street Vehicle Parking Requirements for Designated Areas (whichever applies) unless an agreement is reached between the Council and the applicant for a reduced number of parking spaces where one of the following applies:
- a financial contribution is paid into the Council Car Parking Funds specified by the Council, in accordance with the gazetted rate per car park associated with the 'Car Park Fund Areas' identified on Concept Plan Map Sall27 - Salisbury District Centre Car Park Fund Area, Concept Plan Map Sall29 - Ingle Farm District Centre Car Park Fund Area and Concept Plan Map Sall32 - Mawson Lakes Town Centre Car Parking Fund Area
  - it can be demonstrated that fewer car parks would be required to meet the car parking needs associated with the development.
- 33 Development should be consistent with *Australian Standard AS 2890 Parking facilities*.
- 34 Vehicle parking areas should be sited and designed in a manner that will:
- facilitate safe and convenient pedestrian linkages to the development and areas of significant activity or interest in the vicinity of the development
  - include safe pedestrian and bicycle linkages that complement the overall pedestrian and cycling network
  - not inhibit safe and convenient traffic circulation
  - result in minimal conflict between customer and service vehicles
  - avoid the necessity to use public roads when moving from one part of a parking area to another
  - minimise the number of vehicle access points to public roads
  - avoid the necessity for backing onto public roads
  - where reasonably possible, provide the opportunity for shared use of car parking and integration of car parking areas with adjoining development to reduce the total extent of vehicle parking areas and the requirement for access points
  - not dominate the character and appearance of a site when viewed from public roads and spaces
  - provide landscaping that will shade and enhance the appearance of the vehicle parking areas.
- 35 Vehicle parking areas should be designed to reduce opportunities for crime by:
- maximising the potential for passive surveillance by ensuring they can be overlooked from nearby buildings and roads
  - incorporating walls and landscaping that do not obscure vehicles or provide potential hiding places
  - being appropriately lit
  - having clearly visible walkways.
- 36 Where parking areas are not obviously visible or navigated, signs indicating the location and availability of vehicle parking spaces associated with businesses should be displayed at locations readily visible to customers.

## Industry Zone

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this zone.

### OBJECTIVES

- 1 A zone primarily accommodating a wide range of industrial, warehouse, storage and transport land uses.
- 2 Development that is compatible with existing and forecast noise nuisance from aircraft operations based at RAAF Edinburgh or Parafield Airport.
- 3 Provision of landscaped buffers adjacent to main roads and residential areas.
- 4 Water sensitive urban design and landscaping incorporated as an integral elements of development within the zone.

### PRINCIPLES OF DEVELOPMENT CONTROL

#### Land Use

- 1 The following forms of development are envisaged in the zone:
  - Industry
  - office in association with and ancillary to industry
  - transport distribution
  - warehouse.
- 2 Centre facilities such as shops, offices and consulting rooms should not occur unless it can be demonstrated that they primarily serve businesses in the zone and do not detract from the function of any centre zone or centres generally.
- 3 Development listed as non-complying is generally inappropriate.

#### Form and Character

- 4 Development should be set back from any road frontage in accordance with [Table Sall1- Building Setbacks from Road Boundaries](#).
- 5 Industrial buildings should not occupy more than 50 per cent of the total area of the site upon which they are located.
- 6 In areas where a uniform street setback pattern has not been established, buildings should be set back in accordance with the following criteria (subject to adequate provision of car parking spaces and landscaping between buildings and the road):
  - (a) buildings up to a height of 6 metres should be sited at least 8 metres from the primary street alignment
  - (b) buildings exceeding a height of 6 metres should be sited at least 10 metres from the primary street alignment
  - (c) where an allotment has two street frontages, no building should be erected within 4 metres of the secondary street alignment.

**Salisbury Council**  
**Zone Section**  
**Industry Zone**

- 7 Development involving the outdoor storage of goods or materials should:
  - (a) not be located adjacent to arterial roads and major roads, or facing residential properties unless it can be demonstrated that the amenity of the locality will be enhanced
  - (b) ensure that storage and service areas are effectively screened from public view.
- 8 Industrial buildings should present an attractive façade by incorporating offices of masonry or similar construction at the front of the building, and through the use of architectural elements that will enhance the appearance of the locality, such as surface treatments, form or decoration
- 9 Building facades facing land zoned for residential purposes should not contain openings or entrance ways that would result in the transmission of noise that would adversely affect the residential amenity.
- 10 Access points which are required to cross an open swale stormwater drain should:
  - (a) be minimised by limiting the number of allotments with frontage to swales
  - (b) serve 2 or more allotments where possible
  - (c) be designed to facilitate efficient stormwater management and drainage.
- 11 Open swale stormwater drainage should:
  - (a) be used in conjunction with roadways to cater for major stormwater flows and where practicable, for minor (2 to 10 year) stormwater flows
  - (b) be designed in an attractive form with grass-lined sides of no more than 1-in-5 gradient and a concrete base
  - (c) allow for the planting of trees and shrubs at either side of the channel.
- 12 Any plant or equipment with potential to cause an environmental nuisance (including a chimney stack or air-conditioning plant) should be sited as far as possible from adjoining non-industrially zoned allotments, and should be designed to minimise its effect on the amenity of the locality.
- 13 Development should ensure that the following is achieved:
  - (a) at least 10 per cent of the site is landscaped
  - (b) landscaping along allotment boundaries that adjoin roads or public reserve and at least one side boundary, for a width of at least 3 metres
  - (c) landscaping within parking areas to break-up extensive areas of paving.
- 14 Freestanding structures should not exceed 6 metres in height and should be restricted to one such structure per 6 tenancies.
- 15 Advertisements and advertising hoardings should not include any of the following:
  - (a) flashing or animated signs
  - (b) bunting, streamers, flags, or wind vanes
  - (c) roof-mounted advertisements projected above the roofline
  - (d) parapet-mounted advertisements projecting above the top of the parapet.
- 16 Advertisements and advertising hoardings that are internally illuminated should be unobtrusive and not conspicuous when viewed from adjacent residential properties.

## Greater Levels Policy Area 8

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this policy area.

### OBJECTIVES

- 1 A policy area primarily accommodating a range of industrial activities.
- 2 The provision of unique entrance treatment identifying each industrial area at the junction of the major access points with arterial roads or major collector roads.

### PRINCIPLES OF DEVELOPMENT CONTROL

#### Form and Character

- 1 Development should be designed and sited to incorporate unique entrance treatments that incorporate co-ordinated area identification signage and pavement treatment to denote the entrance to the industrial area.
- 2 Land division should be designed to allow for the establishment of unique entrance treatments that identify each industrial area at the junction of the major access points with arterial roads or major collector roads.
- 3 Development on sites adjacent to Diagonal Road should:
  - (a) incorporate a 10 metre wide landscaped and mounded buffer area along the boundary of the site contiguous with the respective road boundary
  - (b) provide a building setback distance of 25 metres from the road to any building erected on the site.

#### Land Division

- 4 Land division should ensure that:
  - (a) all open space is located in the area marked 'Landscape Buffer' as shown on [Concept Plan Map Sall8 - Mawson Lakes](#)
  - (b) allotments do not have direct access to arterial roads
  - (c) construction of spur-lines linking with the Port Augusta to Adelaide railway and adjacent marshalling yards do not unduly detract from the adjacent Open Space Zone, and is adequately screened from the view of the general public.



**PROCEDURAL MATTERS****Complying Development**

Complying developments are prescribed in Schedule 4 of the *Development Regulations 2008*.

**Non-complying Development**

Development (including building work, a change in the use of land, or division of an allotment) for the following is non-complying:

<b>Form of development</b>	<b>Exceptions</b>
Advertisement and/or Advertising hoarding	Except where it complies with all of the following: <ul style="list-style-type: none"> <li>(a) it does not move, rotate or incorporate flashing lights</li> <li>(b) it does not wholly or partly consist of bunting, streamers, flags, wind vanes and the like</li> <li>(c) if attached to a building, it does not wholly or partly extend above the top of the wall or the fascia.</li> </ul>
Amusement machine centre	
Animal keeping	
Builder's yard	Except where it achieves all of the following: <ul style="list-style-type: none"> <li>(a) it is located on an allotment which does not adjoin an arterial, controlled-access arterial or proposed controlled-access arterial road</li> <li>(b) it is located at least 20 metres from an arterial, controlled-access arterial or proposed controlled-access arterial road.</li> </ul>
Bus depot where it is located within the <b>Infrastructure Policy Area 9</b>	
Caravan park	
Community centre	
Consulting room	
Dairy	
Dwelling	Except where: <ul style="list-style-type: none"> <li>(a) ancillary to and in association with industrial development</li> <li>(b) located on the same allotment.</li> </ul>
Educational establishment	Except where: <ul style="list-style-type: none"> <li>(a) ancillary to and in association with industrial development</li> <li>(b) located on the same allotment.</li> </ul>
General industry where it is located within the <b>Pooraka Policy Area 11</b>	
Horse keeping	
Horticulture	
Hospital	
Hotel	
Intensive animal keeping where it is located in the <b>Pooraka Policy Area 11</b>	

**Salisbury Council  
Zone Section  
Industry Zone**

<b>Form of development</b>	<b>Exceptions</b>
Motel	
Nursing home	
Office	Except where it achieves all of the following: (a) ancillary to and in association with industrial development (b) located on the same allotment (c) it achieves one of the following: (i) it is located outside of the <b>Pooraka Policy Area 11</b> (ii) it is located within the <b>Pooraka Policy Area 11</b> and it has a maximum floor area of no more than 250 square metres.
Place of worship	Except where it has a floor area greater than 500 square metres and is less than 80 metres from the boundary of the Industry Zone.
Pre-school	
Prescribed mining operations	
Primary school	
Residential flat building	
Road transport terminal where it is located in the <b>Infrastructure Policy Area 9</b> at Walkley Heights	
Shop or group of shops	Except where the gross leasable area is less than 250 square metres and it is located in one of the following policy areas or suburb: (a) <b>Greater Levels Policy Area 8</b> (b) <b>Pooraka Policy Area 11</b> (c) the suburb of Greenfields.
Special industry	
Stock sales yard	Except where it is located outside of the <b>Pooraka Policy Area 11</b> .
Stock slaughter works	Except where it is a poultry slaughter works and it is located within the <b>Burton Poultry Processing Policy Area 5</b> .
Telecommunication facility where it is located within 100 metres of a State Heritage Place	
Tourist accommodation	
Waste reception, storage, treatment or disposal in <b>Infrastructure Policy Area 9</b>	Except where it is located outside of the <b>Pooraka Policy Area 11</b> and/or the <b>Infrastructure Policy Area 9</b> .
Wrecking yard	Except where it achieves all of the following: (a) it is located on an allotment which does not adjoin an arterial, controlled-access arterial or proposed controlled-access arterial road (b) it is located at least 20 metres from an arterial, controlled-access arterial or proposed controlled-access arterial road.

**Public Notification**

Categories of public notification are prescribed in Schedule 9 of the *Development Regulations 2008*.

Salisbury Council  
Table Section  
**Table Sal/2 - Off Street Vehicle Parking Requirements**

### Table Sal/2 - Off Street Vehicle Parking Requirements

The following vehicle parking requirements do not apply:

- (a) to the Mixed Use (Bulky Goods, Entertainment, Leisure) Zone except where the form of development is light industry whereby the rates for industry, warehouse, stores are applicable
- (b) to development that is subject to the requirements in [Table Sal/2A - Off Street Vehicle Parking Requirements for Designated Areas](#).

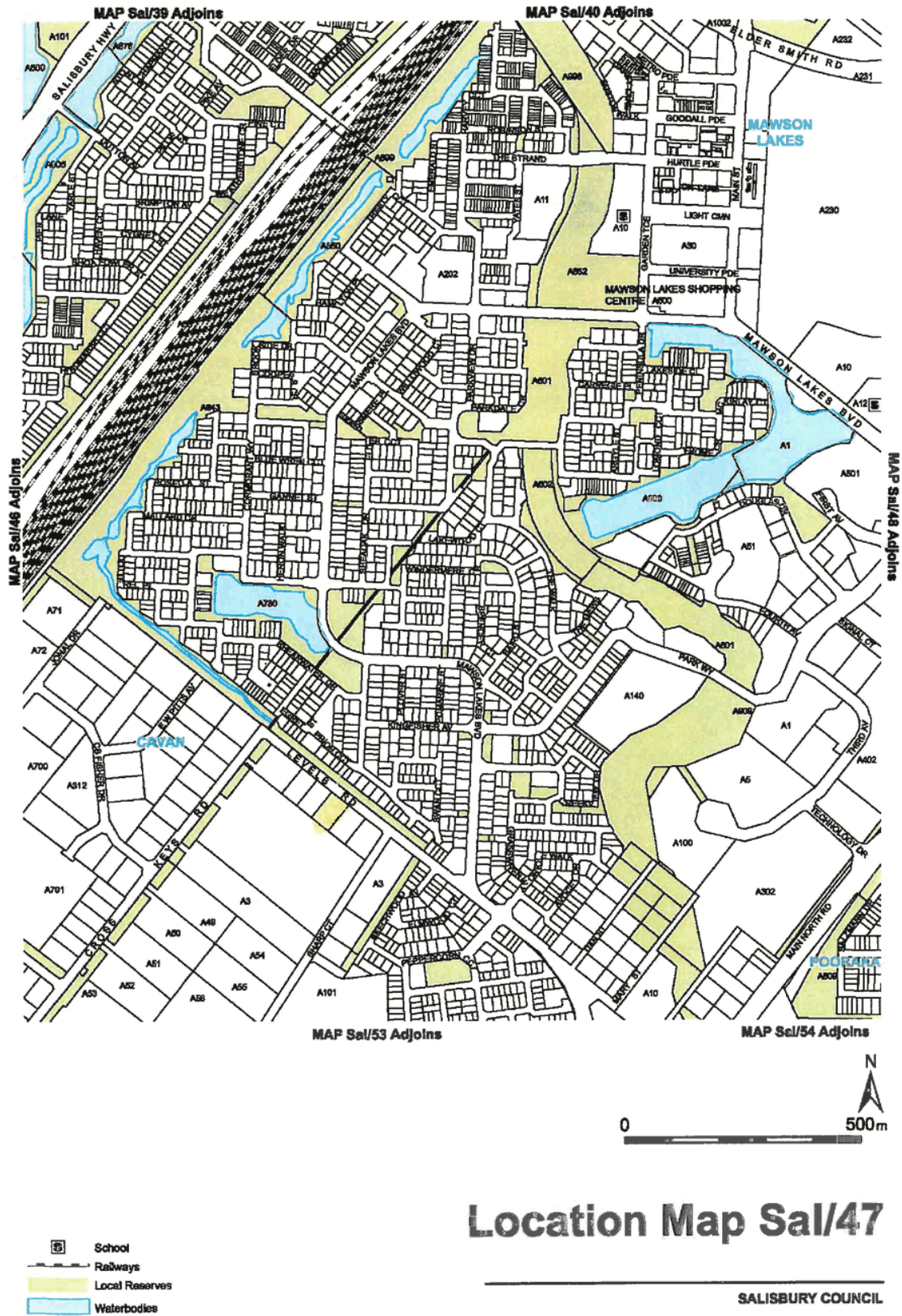
Form of Development	Number of Required Car Parking Spaces
<b>Accommodation</b>	
Aged Care / retirement home	1 space per unit
Serviced apartment	1 space per unit plus 1 space per employee
Motel	1 space per unit
<b>Commercial</b>	
Bulky goods outlet	3 spaces per 100 square metres of gross leasable floor area
Cinema	1 space per 4 cinema seats
Hotel Public bar	1 space per 2 square meters of floor area available to the public
Lounge or beer garden	1 space per 6 square metres of floor area available to the public
Gaming room	1 space per 2 machines
Office	1 space per 25 square metres, with a minimum of 4 spaces per office
Restaurant	Greater of 1 space for every 3 seats or 1 space for every 15 square metres of dining area
Service trade premises	3 spaces per 100 square metres
Shop	7 spaces per 100 square metres of gross leasable area for shops outside of centre zones 5 spaces per 100 square metres of gross leasable area for shops within centre zones
<b>Community/civic</b>	
Child care centre	1 space per 4 children
Community centre	10 spaces per 100 square metres of total floor area
Library	4 spaces per 100 square metres
Place of worship	Greater of 1 space for every 3 seats or every 3 attendees
<b>Dwellings</b>	

**Salisbury Council**  
**Table Section**  
**Table Sal/2 - Off Street Vehicle Parking Requirements**

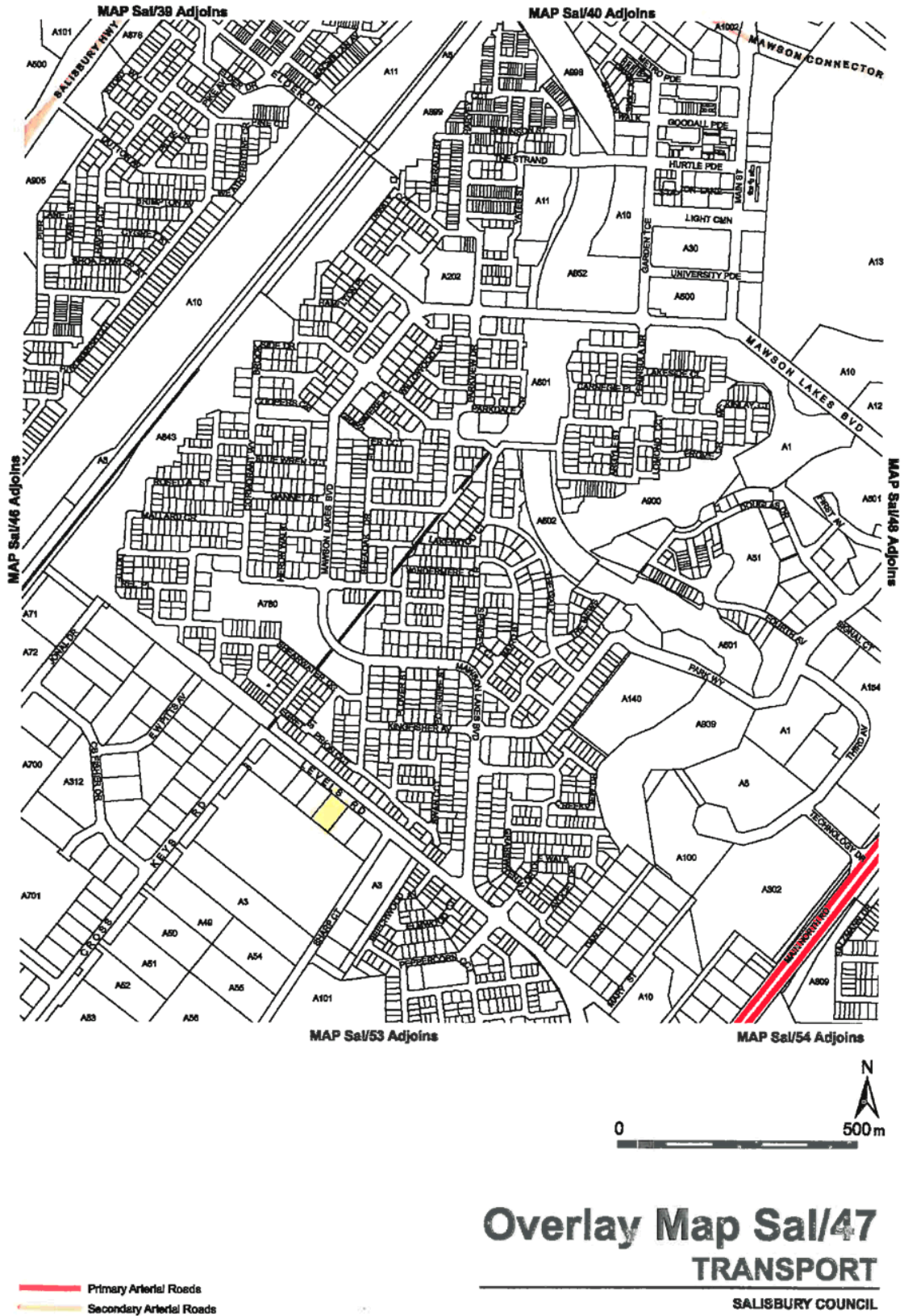
<b>Form of Development</b>	<b>Number of Required Car Parking Spaces</b>
Detached dwelling Semi Detached Dwelling Row Dwelling	2 spaces per dwelling, one of which is to be covered
Residential flat building Multiple dwelling Group dwelling	1 space per dwelling, plus 0.5 on-site visitor car parking spaces per dwelling
<b>Industry, warehouses, stores</b>	
Office component	1 space per 30 square metres
Plus	Plus
Non-office component	
Up to 200 square metres	1 space per 50 square metres
Plus 200-2000 square metres	1 additional space for every 75 square metres
Plus greater than 2000 square metres	1 additional space for every 150 square metres
Or	Or
For labour intensive industries, inclusive of office component (whichever ever is greater)	0.75 car parking spaces per employee
<b>Medical</b>	
Consulting room	10 per 100 square metres of total floor area, with a minimum of 3 spaces per tenancy
Hospital	2.5 spaces per bed
Nursing home	1 space for every 4 beds

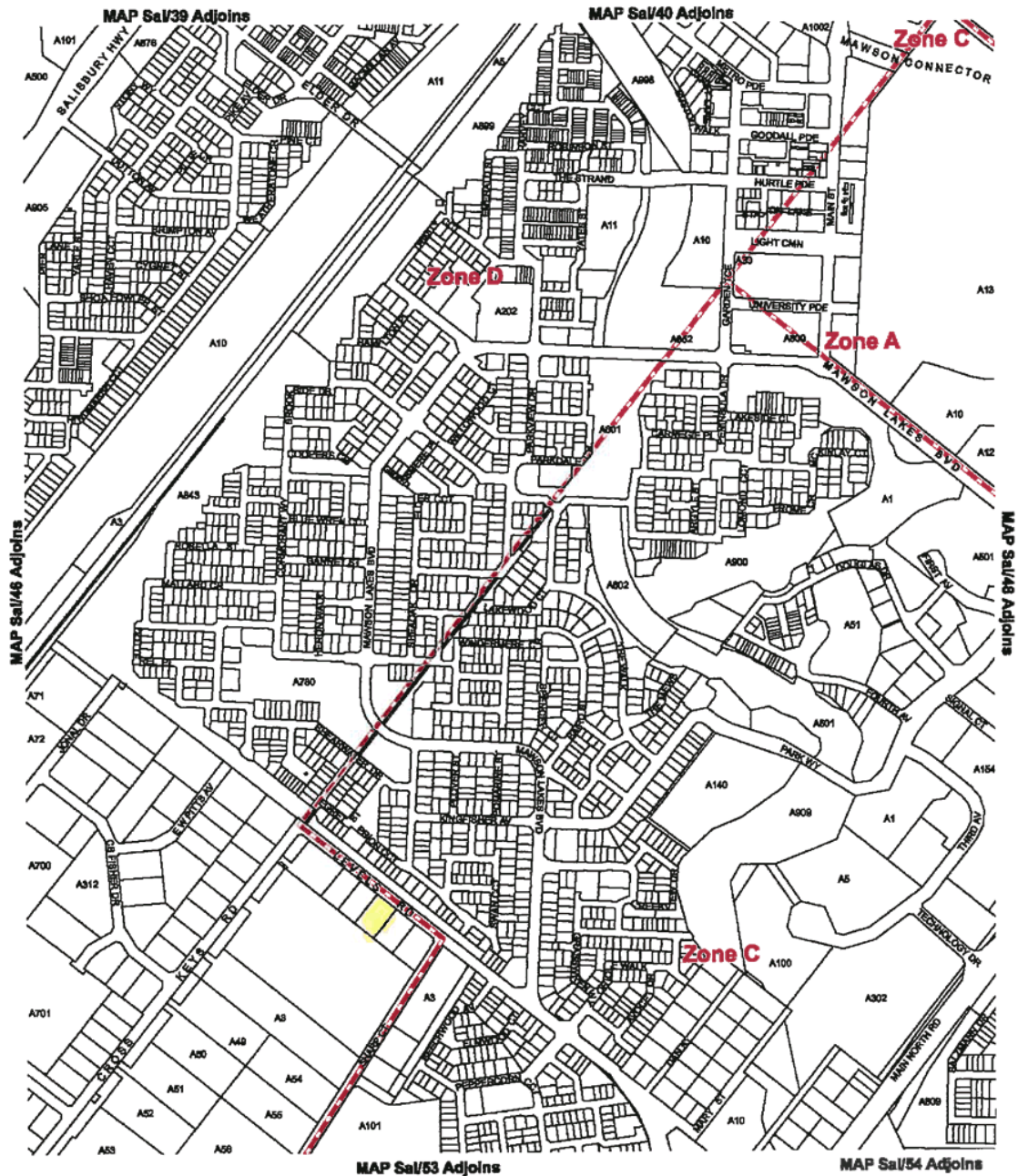
The following vehicle parking requirements apply to development specifically within the **Mixed Use (Bulky Goods, Entertainment and Leisure) Zone**:

<b>Form of Development</b>	<b>Minimum number of required vehicle parking spaces</b>
All forms of development (except Light Industry)	3 spaces per 100 square metres of gross leaseable floor area



Item 5.1.3 - Attachment 5 - Relevant Development Plan Extracts and Location Maps consolidated 15 December 2016





NOTE: Airport Building Heights should be read in conjunction with concept plan map showing heights for Edinburgh Defence Airfield

Airport Building Heights  
Referral to Commonwealth Secretary  
for Dept. of Transport and Regional Services

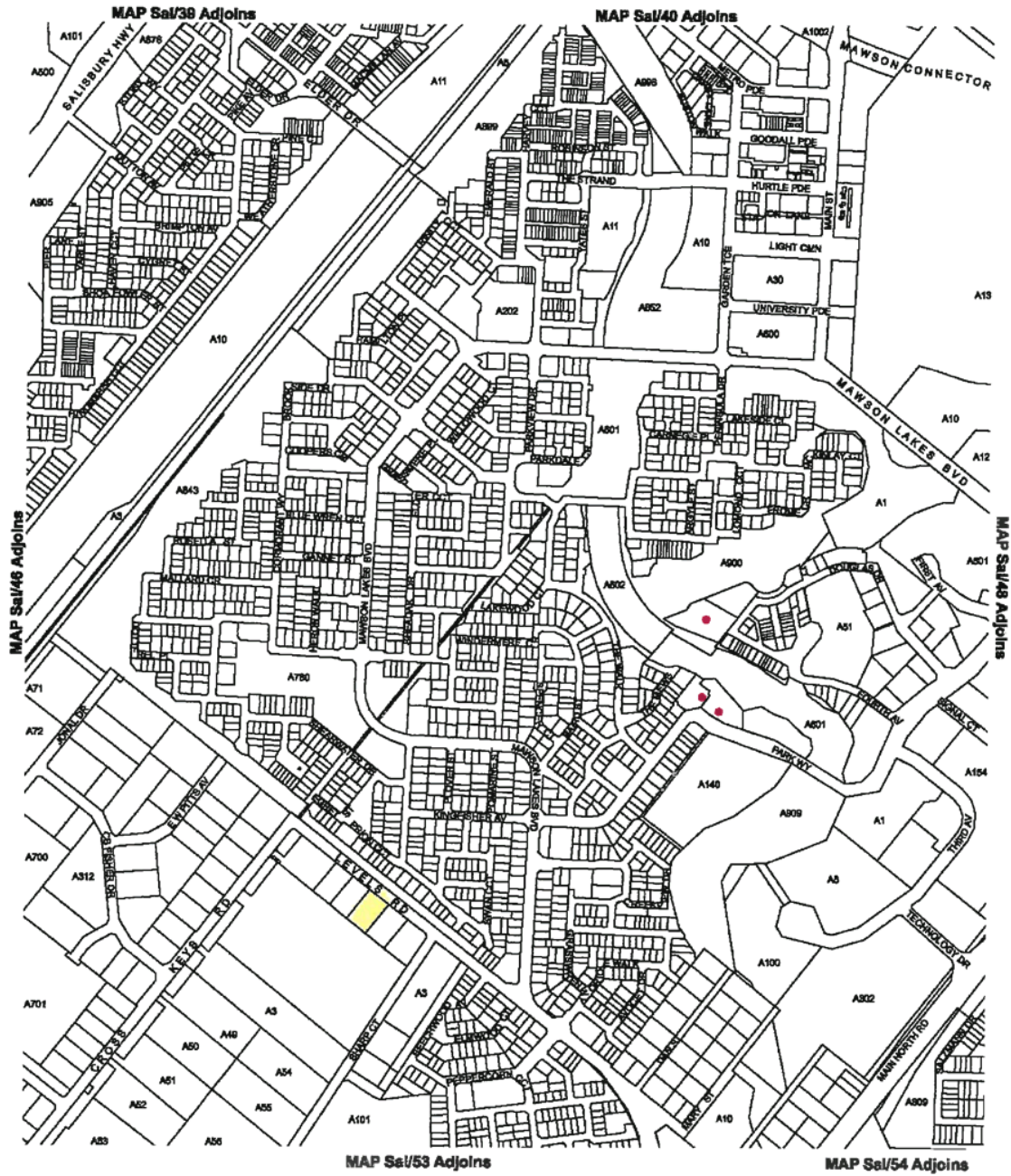
- Zone A All Structures
- Zone B All Structures Exceeding 15 metres above existing ground level
- Zone C All Structures Exceeding 45 metres above existing ground level

Airport Building Heights

## Overlay Map Sal/47 DEVELOPMENT CONSTRAINTS

SALISBURY COUNCIL

Consolidated - 15 December 2016



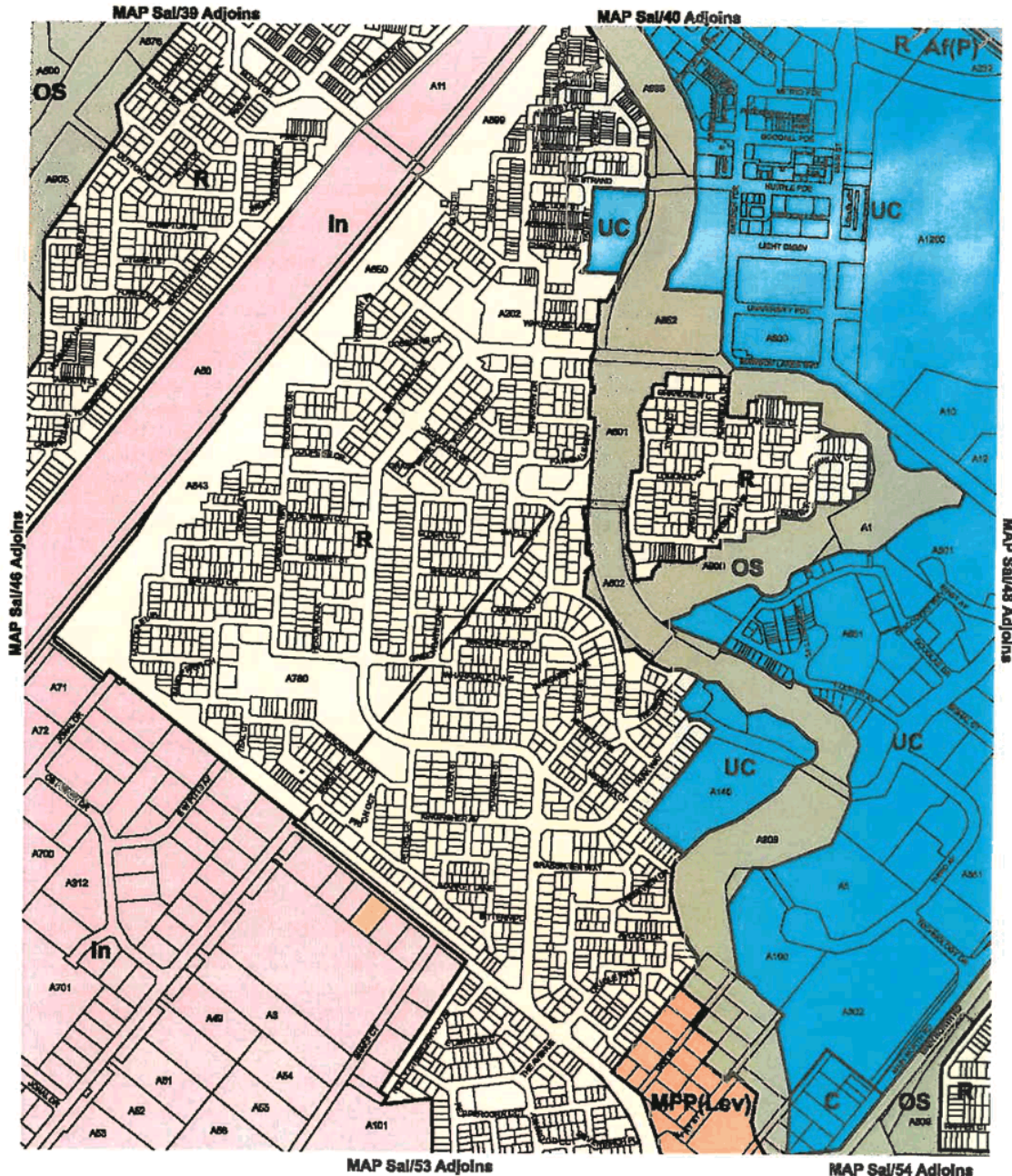
Heritage points are indicative only.  
 For further information on State and Local Heritage Places and Contributory items please refer to the relevant tables within this document.

● State heritage place

# Overlay Map Sal/47 HERITAGE

SALISBURY COUNCIL  
 Consolidated - 15 December 2016





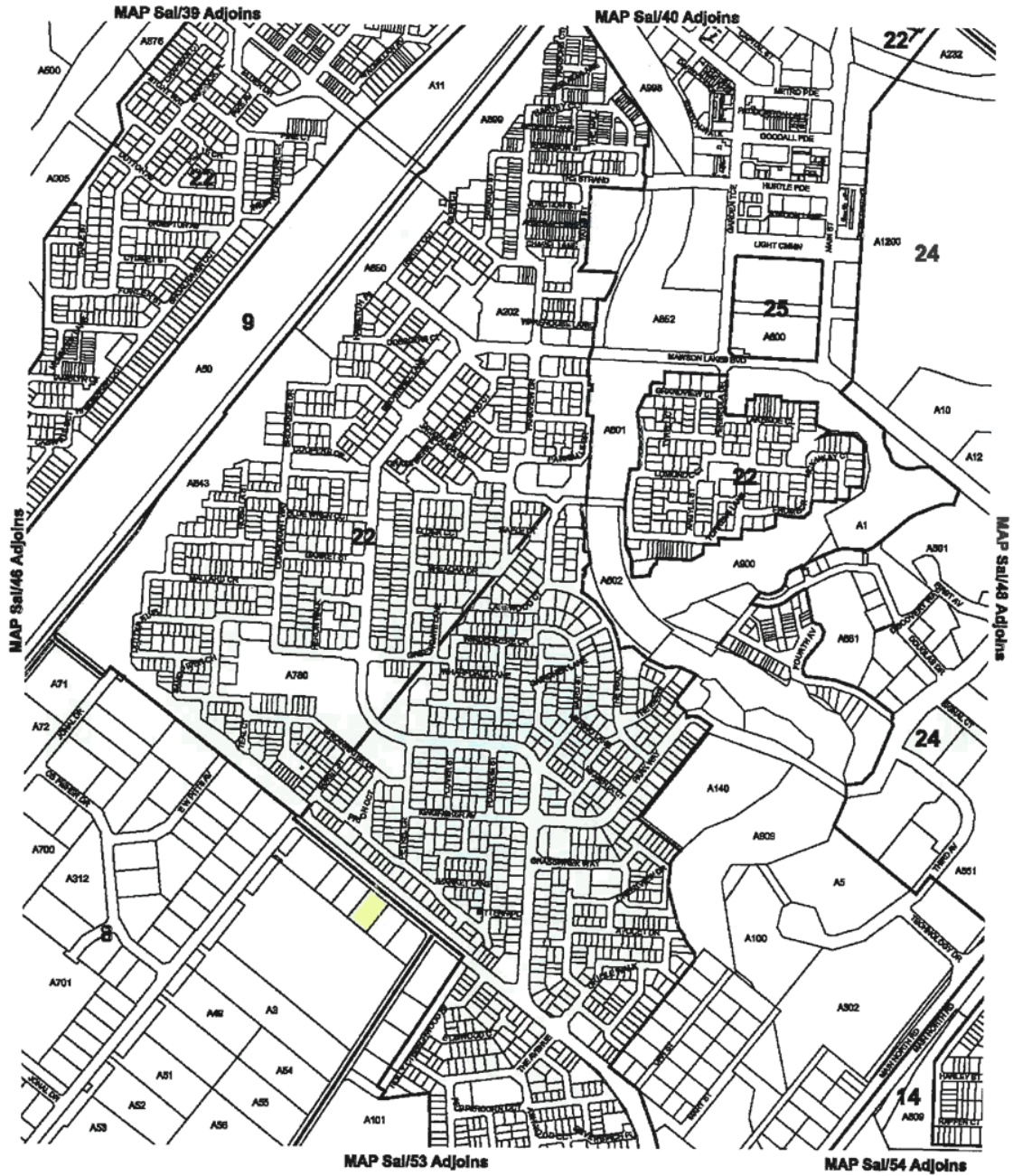
Lambert's Conformal Conic Projection, GDA94

- Zones
- Airfield (Parafield)
  - Commercial
  - Industry
  - Multi Function Polis (Levels)
  - Open Space
  - Residential
  - Urban Core
  - Zone Boundary



## Zone Map Sal/47

SALISBURY COUNCIL  
Consolidated - 15 December 2016



- Lamberts Conformal Conic Projection, GDA84
- Policy Area**
- 14 Landscape Buffer
  - 22 Mawson Lakes
  - 24 Mawson Innovation
  - 25 Main Shopping
  - 8 Greater Levels
  - 9 Infrastructure



## Policy Area Map Sal/47

 Policy Area Boundary

**SALISBURY COUNCIL**  
Consolidated - 15 December 2016

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<b>ITEM</b>	5.1.4
	<b>COUNCIL ASSESSMENT PANEL</b>
<b>DATE</b>	4 December 2018
<b>APPLICATION NO.</b>	361/1246/2018/3B
<b>APPLICANT</b>	Medalstone Pty Ltd - M & J Beattie
<b>PROPOSAL</b>	Change of Use to Truck Parking Depot
<b>LOCATION</b>	106 and 108 Levels Road, Cavan SA 5094
<b>CERTIFICATE OF TITLE</b>	CT-5061/130; CT-5084/263
<b>AUTHOR</b>	Chris Carrey, Development Officer, City Development

### 1. DEVELOPMENT APPLICATION DETAILS

<b>Zone/Policy Area</b>	Industry Zone Greater Levels Policy Area 8
<b>Application Type</b>	Category 3 Merit
<b>Public Notification</b>	Representations received: Six Representations to be heard: One
<b>Referrals - Statutory</b>	Nil
<b>Referrals – Internal</b>	Development Engineering
<b>Development Plan Version</b>	Salisbury Council Development Plan Consolidated 15 December 2016
<b>Assessing Officer</b>	Chris Carrey – Development Officer - Planning
<b>Recommendation</b>	Grant Development Plan Consent subject to conditions
<b>Meeting Date</b>	4 December 2018

### 2. REPORT CONTENTS

Assessment Report

Attachment 1:	Proposal Plans and Supporting Information (including Acoustic Report)
Attachment 2:	Vehicle Log Details provided by Applicant
Attachment 3:	Notice of Category 3 Application and Representations
Attachment 4:	Relevant Development Plan Extracts and Location Maps consolidated 15 December 2016

### 3. EXECUTIVE SUMMARY

The applicant seeks Development Plan Consent for a change in the use of the land to a truck parking depot at 106 and 108 Levels Road, Cavan.

The site is located within the Industry Zone, adjacent to the Residential Zone. The application was assessed 'on-merit' and was subject to Category 3 public notification. Six (6) representations were received during the advertising period, with one in opposition wishing to be heard.

This report has provided a detailed assessment of the application against the relevant provisions of the Salisbury Council Development Plan (consolidated 15 December 2016). The assessment found that:

- a) The proposed land use is an appropriate form of development within the Industry Zone.
- b) Appropriate noise mitigation measures have been included to minimise noise impacts at the nearest residential dwelling and the proposal should comply with the *Environment Protection (Noise) Policy 2007*.
- c) There are no anticipated traffic impacts and the site can accommodate vehicle entry and exit in a forward direction with ample room available onsite for vehicle maneuvering.
- d) There are no anticipated impacts associated with dust or fumes.
- e) The visual appearance of the site is considered to be acceptable having regard to the proposed landscaping treatments and orderly parking of the vehicles.

Given the above, it is recommended that Development Plan Consent be granted, subject to conditions.

### 4. BACKGROUND

This is a retrospective application and the applicant has been operating from the subject site since around 2009.

During 2017, Council received a complaint from a nearby residential property within Mawson Lakes regarding hours of operation and associated truck noise emanating from the subject site and from the neighbouring property at 108 Levels Road. The complainant raised concern that the activities and associated noise were impacting upon their residential amenity. The complaints regarding the current use of the site have been ongoing since 2017.

Following discussions with Council staff, this application and a separate development application (361/1223/2017/3B) for 108 Levels Road were lodged with Council for assessment.

Development Application 361/1223/2017/3B is also to be considered within this Council Assessment Panel Agenda. While development application 361/1223/2017/3B applies to 108 Levels Road only - this development application seeks to regularise the current land use of 106 Levels Road and utilise 108 Levels Road for the purposes of truck access and egress.

The operators of 106 and 108 Levels Road have sought to work together to try and resolve the issues raised by the complainant and as a consequence of the development assessment process. In particular, the operators of 108 Levels Road have agreed that the operator from 106 Levels Road can make use of their site for entry and/or exit purposes. This will mean there is no need for trucks to reverse into 106 Levels Road which currently occurs.

The current layout of 106 Levels Road appears to be largely consistent with an earlier development approval, granted during the 1990s for a 'Workshop and Office' (DA 361/754/1996 refers). That approval allowed for construction of the existing buildings, provision of access from Levels Road and the general site layout. Therefore, Council understands the existing approved use of 106 Levels Road to be as a 'Workshop and Office'.

The existing approval does not preclude 24 hour operation.

The subject site is located within an established industrial area which pre-dates the construction of the Mawson Lakes residential area to the north.

## **5. SUBJECT SITE**

### 106 Levels Road

This site is 3150sqm in area and has a primary frontage to Levels Road of 45 metres. There is an existing off-centre concrete driveway which provides access to/from Levels Road which is approximately eight (8) metres in width.

The site is relatively level and is occupied by an existing office and workshop which are located on the north west side.

Surfaces are fully sealed with bitumen and/or concrete pavement and there are eleven (11) standard car parking spaces available to the front and rear of the office.

Truck parking can occur to the side and rear of the existing workshop.

There is an existing storm water spoon drain which allows for discharge to the Levels Road storm water network.

The front, rear and north west (side) property boundaries comprise open chain mesh fencing. The majority of the south east (side) boundary is occupied by solid concrete wall associated with the building on 104 Levels Road. The front of the site is already landscaped.

There are no easements registered to the Certificate of Title.

The existing approved land use is for 'workshop and office'.

### 108 Levels Road

## ITEM 5.1.4

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### Item 5.1.4

108 Levels Road is also 3150sqm in area with a primary frontage to Levels Road of 45 metres. There is an existing six (6) metre wide central concrete driveway which provides access to/from Levels Road.

The site is relatively level and is occupied by an existing office and workshop which are located on the south east side.

The surfaces are fully sealed with bitumen pavement and there are seven (7) standard car parking spaces to the side and front of the office. There is an existing storm water spoon drain which allows for discharge to the Levels Road storm water network.

There is a small landscaping strip located along the front property boundary and the site is fenced with a combination of solid metal and open chain mesh fencing.

There is one easement registered to the Certificate of Title, however it is not affected by the proposal.

The existing approved land use is for 'workshop and office'.

Access between both sites will be provided via an internal driveway/gate which will be located towards the rear of the sites, behind the existing workshops.

Site photos are provided below.

**Photo 1.**

*Looking south west into 106 Levels Road with the workshop on the right*



**Photo 2.**

*Looking at the workshop within 106 Levels Road with the office on the right*



ITEM 5.1.4

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**Photo 3.**  
*Looking to the rear of the workshop on 106 Levels Road – where ‘night time’ parking of trucks will occur*



**Photo 4.**  
*Looking south east along Levels Road, with the subject site*



**Photo 5.**  
*Looking north west along Levels Road from in front of the subject site*



**Photo 6.**  
*Looking south east along Levels Road from in front of the subject site*



**Photo 7.**  
*Looking north towards the residential area across Levels Road Reserve*



## 6. LOCALITY

The subject site is located within the Industry Zone and Greater Levels Policy Area 8 of Council's Development Plan and is directly adjacent to the Residential Zone boundary which aligns with Levels Road.

The site is bounded by established industrial properties to the north west, south west and south east.

The Mawson Lakes residential area is located to the north, across Levels Road and Levels Road Reserve. Several residential properties on Petrel Crescent and Prion Circuit have their rear yards and back fence abutting this Reserve. The Levels Road Reserve is approximately 15 metres wide and consists of established trees and shrubs. The fence to the rear of the residential properties exceeds 2.1 metres height and extends above eave height of those adjacent single storey dwellings.

Levels Road is gazetted for B-Double heavy vehicle access and predominantly services surrounding industrial sites on Levels Road, Sharpe Court, Cross Keys Road, CB Fisher Drive and Jonal Drive. However; it also allows for standard passenger car access to/from the residential areas to the south east and east via Beechwood Avenue.



Sharpe Court does not allow for a right turn (west) onto Montague Road. Therefore, vehicles on Sharpe Court and Levels Road seeking to access Port Wakefield Road (or heading west more generally) are likely to make use of Levels Road, to access Cross Keys Road which then facilitates a right turn onto Montague Road.

Aerial and contextual plans are provided below.

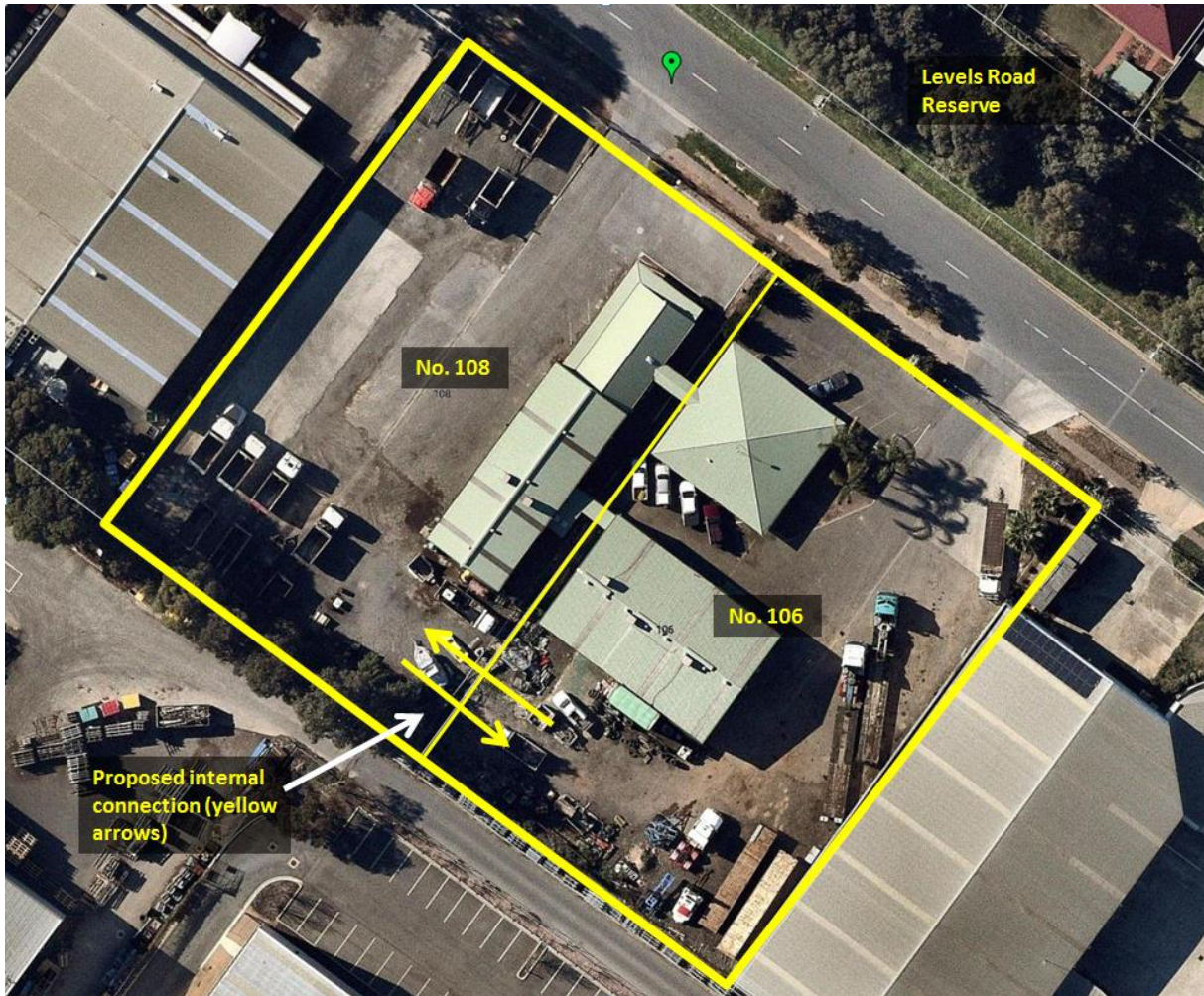
Aerial View



Legend (Source: NearMap)

	Subject site
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Close Up Aerial View



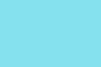



Source: NearMap

Locality Plan – Cadastre



Note – 522 Cross Keys Road was not notified during the public notification period (see Section 9 below)

Legend (Source: Geocortex)	
	Subject site
	Locality boundary
	Properties notified during Category 3 public notification
	Representations Received during Category 3 public notification

**7. DESCRIPTION OF THE PROPOSED DEVELOPMENT**

The applicant seeks Development Plan Consent for a change in the use from ‘Workshop and Office’ to a truck parking depot which can be accessed 24 hours a day, seven days a week. The existing office and workshop located on 106 Levels Road are to be used in association with the truck parking depot.

The operator of the site (AllTrans) is a heavy haulage transport company who move earthmoving and mining equipment on a contract basis. They operate five heavy vehicles out of the subject site (with associated trailers) with a maximum vehicle length of 19 metres.

There is no loading, unloading or material processing and the use extends to the parking and associated movement of trucks only.

Since commencing operations at the site, the operator has made use of 106 Levels Road only. Given the size and layout of the site and the truck lengths involved this has meant that trucks have been reversed into the site. The applicant proposes to alter this arrangement with access or egress proposed to occur via the adjoining site at 108 Levels Road (Ace Bitumen), and manoeuvring to occur to the rear of the existing buildings (to the rear of the site). This will mean that all vehicles can enter and exit the site in a forward direction.

The applicant has advised that 24 hour truck access is required to satisfy operational demands and is dependent upon project type, location of work etc. The applicant has also advised that contracts often change at short notice and therefore a high degree of flexibility is required.

The office is occupied by three (3) staff and operates from 7.30am to 5.30pm.

Only minor mechanical repairs and tyre changing is to occur on site.

Waste disposal is managed by private contractors, Signal Waste or Mulherns Disposal.

A copy of the proposal plans and supporting documentation are contained in Attachment 1.

The applicant has also provided extensive vehicle activity logs which highlight varying workloads and starting times associated with the activity. These are contained in Attachment 2.

## **8. CLASSIFICATION**

The subject site is located within the Industry Zone, as identified within Council's Development Plan (consolidated 15 December 2016). In this Zone, a truck parking depot is not listed as a complying or non-complying form of development. As such, the application must be assessed as a merit form of development.

## **9. PUBLIC NOTIFICATION**

The proposed development is neither listed as a Category 1 or 2 form of development within the Industry Zone or under Schedule 9 of the *Development Regulations 2008*. Accordingly, the application was advertised as a Category 3 form of development under Section 38 of the *Development Act 1993*.

Public notification took place between 16 January and 6 February 2018. The Council received six (6) representations during the notification period. One (1) representation was in opposition and five (5) were in support of the proposal. The representors are listed in the table below.

<b>Representations received</b>			
<b>Representations received</b>		<b>Support / Oppose</b>	<b>Wish to be Heard</b>
1	D & E Golley 15 Petrel Crescent Mawson Lakes SA 5095	<b>Oppose</b>	✓
2	A Qn 17 Prion Circuit Mawson Lakes SA 5095	<b>Support</b> *subject to conditions	✓
3	Giulia Property Trust 108 Levels Road Cavan SA 5094	<b>Support</b>	
4	Jan Jarrett 108 Levels Road Cavan SA 5094	<b>Support</b>	
5	Kora Bay Pty Ltd 110 Levels Road Cavan SA 5094	<b>Support</b>	
6	The Giuliani Nominees No 2 112 Levels Road Cavan SA 5094	<b>Support</b>	

A copy of the Category 3 public notice and submissions received are contained in Attachment 3. The applicant did not provide a response to the representations.

The content of the representation are summarised in the table below:

<b>Summary of Representations</b>	
<b>Representation</b>	<b>Applicant's Response</b>
The type and level of noise associated with the activity, outside normal business hours and at weekends is not appropriate next to a residential area.	No response provided
Smoke from trucks can spread to the residential area and have a negative impact on the health of residents.	No response provided

*Note: Public notification was undertaken on the basis that the proposal was to be confined to 106 Levels Road with the applicant evidencing this via hand drawn plans only. As a consequence of the planning assessment process and issues raised by the representors, the applicant negotiated shared access with 108 Levels Road, engaged a project designer to provide more detailed plans, and engaged Sonus Acoustic Engineers to undertake an acoustic assessment of the proposal (this will be discussed later in the report). Therefore, the public notification process did not include the current set of plans, Acoustic Report or reference the use of both 106 and 108 Levels Road.*

*As a result, 522 Cross Keys Road was not notified of the proposal – however, it is considered that the proposed inclusion of 108 Levels Road will not impact their operations and they have not been notified (note - they were also notified under DA 361/1223/2017/3B and did not raise any objection).*

*The representors in opposition to the proposal have been advised of the intended changes and have been made aware of the updated documentation. If any additional comments or concerns are raised by the representors, these will be provided to members of Council's Assessment Panel prior to the meeting.*

*When notified, the proposal also included reference to a freestanding advertising sign (located close to the front property boundary). However, the applicant has since clarified that the sign was existing and they simply re-badged the sign. As a result, it is considered that the sign does not require approval.*

## **10. REFERRALS – STATUTORY**

The application was not subject to any referrals under Schedule 8 of the Regulations.

## **11. REFERRALS – INTERNAL**

<b>DIVISION</b>	<b>COMMENT</b>
<i>Development Engineering</i>	<i>There is sufficient space onsite to allow for entry and exit in a forward direction.</i>  <i>As an existing, developed site no additional upgrades are required.</i>

## **12. ASSESSMENT**

Pursuant to Section 35(2) of the *Development Act 1993*, it is recommended that the Panel determine that the proposed development is not seriously at variance with the Salisbury Council Development Plan, consolidated 15 December 2016. Truck parking and associated truck movements are an established activity within the locality and can be an appropriate form of development within an Industry Zone subject to amenity based considerations.

### Assessment

Detailed assessment of the application has taken place against the relevant provisions of the Salisbury Council Development Plan and is described below under headings.

An extract of the relevant Development Plan, Consolidated 15 December 2016 is contained in Attachment 4. The relevant provisions are also highlighted in the Attachment.

Zoning and Land Use

Objective 1 of the Industry Zone states:

*1 A zone primarily accommodating a wide range of industrial, warehouse, storage and transport land uses.*

Objective 1 of the Greater Levels Policy Area 8 states:

*1 A policy area primarily accommodating a range of industrial activities.*

It is therefore considered that development in the form of truck parking (with associated truck movements) which supports an industrial activity can be an appropriate form of land use within the Industry Zone, subject to appropriate amenity based considerations

Interface Between Land Uses

While the use may be acceptable within the Industry Zone, consideration must be given to the various impacts of the proposal to ensure the health and amenity of surrounding land occupiers and the locality are protected. The interface issues are addressed below.

*Noise and Hours of Operation*

Principle of Development Control 1(b) & (g) of the General Section: Interface Between Land Uses module states:

*1 Development should not detrimentally affect the amenity of the locality or cause unreasonable interference through any of the following:*

*(b) noise*

*(g) hours of operation*

Principle of Development Control 7 (a), (b) and (d) of the General Section: Industrial Development module states:

*7 Development within 50 metres of the **Residential Zone** boundary should:*

*(a) demonstrate appropriate acoustic performance*

*(b) ensure that all noise sources including machinery, outside loading, unloading and other service areas are located away from the **Residential Zone** boundary*

*(d) limit operating hours to between 7am and 6 pm*

The subject site is located within 50 metres of the Residential Zone boundary and proposes to cater for access 24 hours a day across 7 days. The applicant has advised that 24 hour operation is required for operational reasons. The vehicle logs provided by the applicant in Attachment 2 demonstrate the varied start and finish times associated with the use.

The applicant provided an Environmental Noise Assessment Report prepared by Sonus Acoustic Engineers. This considers the noise impact of the proposed development against the noise goals of the *Environment Protection (Noise) Policy 2007* (“Noise Policy”) and details the required acoustic treatments to achieve these targets.

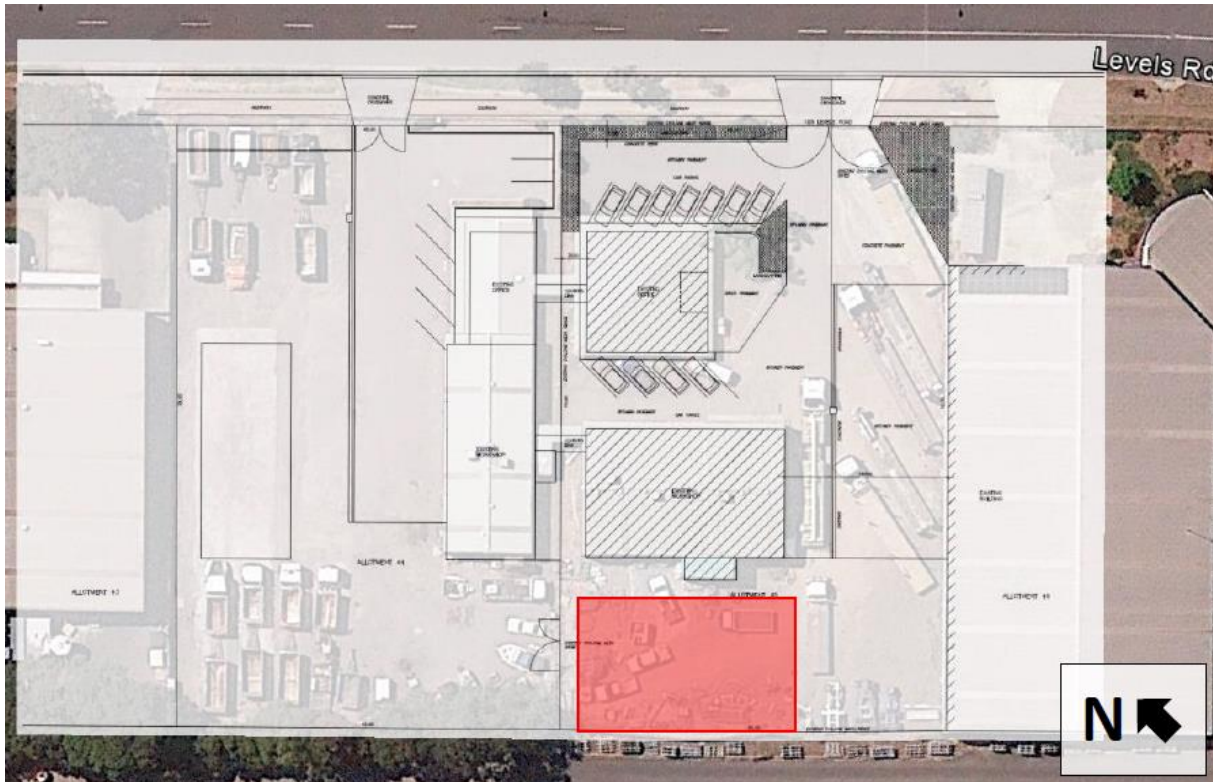
## ITEM 5.1.4

### Item 5.1.4

In particular, the report considered the impact of the development on the Residential Zone to the north.

The noise treatment measures recommended by Sonus are:

- As per the current site plan, ensure that trucks are not required to reverse while on site during the night time hours (10pm to 7am);
- Ensure no loading or unloading occurs on the site, with the exception of occasional hitching and unhitching of trailers during the day time hours (7am to 10pm) only;
- Restrict the amount of time a truck can idle after entering or before leaving the site to be no more than 5 minutes;
- Restrict the area where trucks can park on the site to that shown in the following figure, when entering or exiting during the night time hours of the Policy (10pm to 7am).



The report concludes that incorporation of these measures will ensure that the maximum predicted noise levels at the nearest residential dwelling will be no more than:

- 42 dB(A) during the daytime (7.00am to 10.00pm). This satisfies the noise goal levels of the Policy which stipulate a maximum of 54 dB(A) during such times; and
- 42 dB(A) during the night time (10.00pm to 7.00am). This satisfies the noise goal levels of the Policy which stipulate a maximum of 45 dB(A) during such times.

The report also considered the maximum noise level from the activity such as truck brake compressed air release, people talking, or the opening and closing doors during the night time



hours. The highest predicted noise level ( $L_{max}$ ) at the residences is 50 dB(A), which satisfies the 60 dB(A) goal noise level of the Policy.

On this basis, it is considered that while the development proposes 24 hours activity adjacent the Residential Zone, it has demonstrated appropriate acoustic performance to support the activity.

These measures should improve the existing situation and address previous complaints from nearby residential occupiers and address issues relating to noise and hours of operation raised during the public notification period. The applicant has committed to implementing these changes.

In addition, a development plan condition has been recommended that all waste collection occur should occur during appropriate day time hours to limit the impact of noise on nearby residential properties.

Over and above these measures, the applicant has sought to further improve the existing situation. They have advised that where possible, when they have early morning or late night work scheduled or activity booked on a Sunday, they will commence or conclude operations from a nearby depot at 79 Cavan Road (see aerial photograph below). This depot is operated by K & L Profiling who the operator (All Trans) works with on a contract basis. This has not been considered as a formal aspect of the proposal, rather, as an alternative arrangement that the operator will seek to utilise where possible.



*Traffic Impacts*

Principle of Development Control 3 of the General Section; Industrial Development module states:

*3 Industrial development should enable all vehicles to enter and exit the site in a forward direction.*

Principles of Development Control 13 and 22 of the General Section: Transportation and Access module state:

*13 Development should make sufficient provision on site for the loading, unloading and turning of all traffic likely to be generated.*

*22 Development should have direct access from an all-weather public road.*

Levels Road is gazetted for the use of B-Double heavy vehicles and is of appropriate construction and design to support the size of trucks using the site.

All trucks are able to enter and exit the site in a forward direction with no negative impact on the road network or adjoining industrial sites.

On this basis, it is considered that the proposal accords with the above provisions and will not have a detrimental impact on the amenity of the locality.

*Dust and Fumes*

Principle of Development Control 1(a) of the General Section: Interface Between Land Uses module states:

*1 Development should not detrimentally affect the amenity of the locality or cause unreasonable interference through any of the following:*

*(a) the emission of effluent, odour, smoke, fumes, dust or other airborne pollutants*

The site is fully sealed and there is unlikely to be significant amounts of dust generated in association with vehicle movements and maneuvering.

The only fumes likely to result as a consequence of the proposal are related to truck movements and truck idling. The applicant has confirmed that truck idling is restricted to five (5) minutes and this will occur on the subject site more than 50 metres from the nearest residential dwelling.

On this basis, and having regard to the established industrial character of this locality, there is no anticipated dust or fume impacts associated with the proposal.

### *Lighting*

PDC 1 (e) of the *General Section: Interface Between Land Uses* module states:

*1 Development should not detrimentally affect the amenity of the locality or cause unreasonable interference through any of the following:*

*(e) light spill.*

No outdoor lighting has been proposed. If outdoor lighting were to be installed (e.g. for security purposes) it is unlikely to have a negative impact on the surrounding locality. Nearby industrial properties are likely to adopt similar arrangements and are not considered to be a 'sensitive' receptor.

Landscaping within the Levels Road Reserve and the existing solid fence provide an appropriate screen to prevent vehicle headlight glare affecting residential properties within the Mawson Lakes residential area.

To reinforce this position, a Development Plan condition has been recommended to ensure that any outdoor lighting use is restricted to avoid driver distraction or nuisance to adjoining sites.

### Visual Impact

As this is a retrospective proposal, the visual impact of the proposal is already apparent. The site is maintained in a neat and tidy condition, with parking occurring in an orderly manner and no outdoor storage of wastes or other materials taking place. The front of the site is already landscaped and no additional upgrades are proposed or considered to be necessary.

## **13. CONCLUSION**

The applicant seeks Development Plan Consent for a change in the use of the land to a truck parking depot at 106 and 108 Levels Road, Cavan.

This report has provided a detailed assessment of the application against the relevant provisions of the Salisbury Council Development Plan (consolidated 15 December 2016).

The assessment found that:

- a) The proposed land use is an appropriate form of development within the Industry Zone.
- b) Appropriate noise mitigation measures have been included to minimise noise impacts at the nearest residential dwelling and the proposal should comply with the *Environment Protection (Noise) Policy 2007*.
- c) There are no anticipated traffic impacts and the site can accommodate vehicle entry and exit in a forward direction with ample room available onsite for vehicle manoeuvring.
- d) There are no anticipated impacts associated with dust or fumes.
- e) The visual appearance of the site is considered to be acceptable

Given the above, it is recommended that Development Plan Consent be granted, subject to conditions.

#### 14. STAFF RECOMMENDATION

That the Council Assessment Panel resolve that:

- A. The proposed development is not considered to be seriously at variance with the Salisbury Council Development Plan – Consolidated 15 December 2016.
- B. Pursuant to Section 33 of the *Development Act 1993*, Development Plan Consent is **GRANTED** to application number 361/1246/2018/3B for Change of Use to Truck Parking Depot in accordance with the plans and details submitted with the application and subject to the following conditions:

#### DEVELOPMENT PLAN CONSENT CONDITIONS

1. The development shall be carried out in accordance with the details submitted with the application and the following stamped approved plans and documents, except where otherwise varied by the conditions herein:

Drawing No.	Plan Type	Date	Prepared By
18/08-A02 Revision B	Site Plan	Date Received Stamped by Council 26 October 2018	David W. Pound Project Design
18/08-A03 Revision B	Site Plan	Date Received Stamped by Council 26 October 2018	David W. Pound Project Design
S5789C2	Environmental Noise Assessment	Date Received Stamped by Council 2 November 2018	Sonus Acoustic Engineers
N/A	Supporting cover letter	Date Received by Council Stamped 8 January 2018	Medalstone Pty Ltd
N/A	Supporting Cover Letter	Date Received by Council Stamped 26 October 2018	AllTrans

N/A	Letter agreeing to use of 108 Levels Road	Date Received by Council Stamped 26 October 2018	Ace Bitumen
N/A	Letter agreeing to use of 79 Cavan Road	Date Received by Council Stamped 26 October 2018	K & L Profiling

*Reason: To ensure the proposal is established in accordance with the submitted plans.*

2. The development shall be carried out in accordance with the Environmental Noise Assessment (“Acoustic Report”) approved by Council under Development Plan Condition 1, and shall be adhered to at all times to the reasonable satisfaction of Council. In particular:
  - a. Trucks shall not reverse while on site during night time hours (10.00pm to 7.00am)
  - b. No loading or unloading shall occur onsite, with the exception of occasional hitching and unhitching of trailers during day time hours only (7.00am to 10.00pm)
  - c. Truck idling shall be restricted to no more than five (5) minutes after entering or before leaving the site
  - d. Trucks shall only park to the rear (south west) of the site, on that area of the site identified on page 13 of the Acoustic Report when entering or exiting during night time hours (10.00pm to 7.00am)

*Reason: To ensure that noise does not cause nuisance to adjoining residential occupiers or owners.*

3. All vehicles shall enter and exit the site in a forward direction.

*Reason: To ensure that vehicles associated with the land use do not cause disruption or danger to vehicles on adjoining public roads.*

4. Noise from the subject site when measured at the nearest residential property boundary shall remain within the requirements of the Environment Protection Authority (EPA) Guidelines for development adjacent a residential area.

*Reason: To ensure that noise does not cause nuisance to adjoining residential occupiers or owners.*

5. No materials, goods or containers shall be stored in the designated parking areas, driveways or manoeuvring areas.

*Reason: To ensure the car parking areas are always available for the purpose they are designed. Further that the site be maintained in a clean and tidy state.*

6. All driveways and car parking areas shall be constructed with brick paving, concrete or bitumen to a standard appropriate for the intended traffic volumes and vehicle types. Individual car parking bays shall be clearly line marked. Driveways and car parking areas shall be maintained at all times to the satisfaction of Council.

*Reason: To ensure access and car parking is provided on the site in a manner that maintains and enhances the amenity of the locality.*

7. All loading and unloading of vehicles and manoeuvring of vehicles in connection with the now approved land use shall be carried out entirely within the subject land.

*Reason: To ensure that vehicles associated with the land use does not cause disruption or danger to vehicles on adjoining public roads.*

8. Outside lighting shall be restricted to that necessary for security purposes only and shall be directed and shaded to prevent light overspill and/or nuisance to adjacent occupiers or distraction to drivers on adjacent public roads.

*Reason: To ensure that floodlighting does not cause nuisance or danger to adjoining occupiers or road users thereby reducing the amenity of the locality and/or making road use unsafe.*

9. All waste and other rubbish shall be contained and stored pending removal in covered containers and waste containers and must not be located within designated car parks or maneuvering areas.

*Reason: To maintain the amenity of the locality.*

10. All waste shall be collected between the hours of 7.00am and 7.00pm Monday to Saturday. Waste collection times shall be limited to between 9.00am and 7.00pm on Sundays and Public Holidays, in accordance with the *Environment Protection (Noise) Policy 2007*.

*Reason: To maintain the amenity of the locality*

#### **CO-ORDINATION**

Officer:	GMCiD	MDS
Date:	19.11.18	14.11.18

#### **ATTACHMENTS**

This document should be read in conjunction with the following attachments:

1. Proposal Plans and Supporting Information (including Acoustic Report)
2. Vehicle Log Details provided by Applicant
3. Notice of Category 3 Application and Representations
4. Relevant Development Plan Extracts and Location Maps consolidated 15 December 2016

## Attachment 1

Proposal Plans and supporting information (including Acoustic Report)



**CITY OF SALISBURY  
DEVELOPMENT APPLICATION FORM**

361/ 1246 /2017/ 1B

Please use BLOCK LETTERS and Black or Blue Ink

APPLICANT: COMPANY / FIRST NAME		SURNAME	
Meckinstone Pty Ltd - Malcolm + Josh		Beattie	
POSTAL ADDRESS:			
PO Box 311 Dry Creek SA 5094			
OWNER NAME: (This must be completed)			<input checked="" type="checkbox"/> as above
OWNER POSTAL ADDRESS:			<input checked="" type="checkbox"/> as above
OWNER PHONE NO:	OWNER FAX NO:	OWNER EMAIL:	
0881691300	0881691399	mal@beattietransport.com.au	
CONTACT PERSON FOR FURTHER INFORMATION			<input checked="" type="checkbox"/> as above
NAME:		TELEPHONE (W):	
Deb Hobbs		0881691300	
		(M):	
EMAIL:		FAX:	
deb@beattietransport.com.au		0881691399	
BUILDER NAME:		BUILDERS EMAIL:	
N/A		N/A	
BUILDER POSTAL ADDRESS:			CONTACT NO.:
N/A			LICENCE NO.:
			N/A
CURRENT USE OF PROPERTY:			
Transport			
DESCRIPTION OF PROPOSAL:			DEVELOPMENT COST
			\$
LOCATION OF PROPOSAL			
Street No:	Street:	Suburb:	
106 ✓	Levels Road	Cavan	
Lot No:	Section:	Plan:	Volume: Folio:
45 ✓	S2243 ✓	28994	5061 ✓ 130 ✓
OFFICE USE ONLY			
Registration Date:	Zone:	Ward:	
7 / 7 /2017	IN	levels	
BUILDING RULES CLASSIFICATION			
Classification sought:			
If Class 6, 6, 7, or 9 classification is sought, state the proposed number of employees			Male:
			Female:
If Class 9a classification is sought, state the number of persons for whom accommodation is provided:			
If Class 9b classification is sought, state the proposed number of occupants of the various spaces at the premises:			

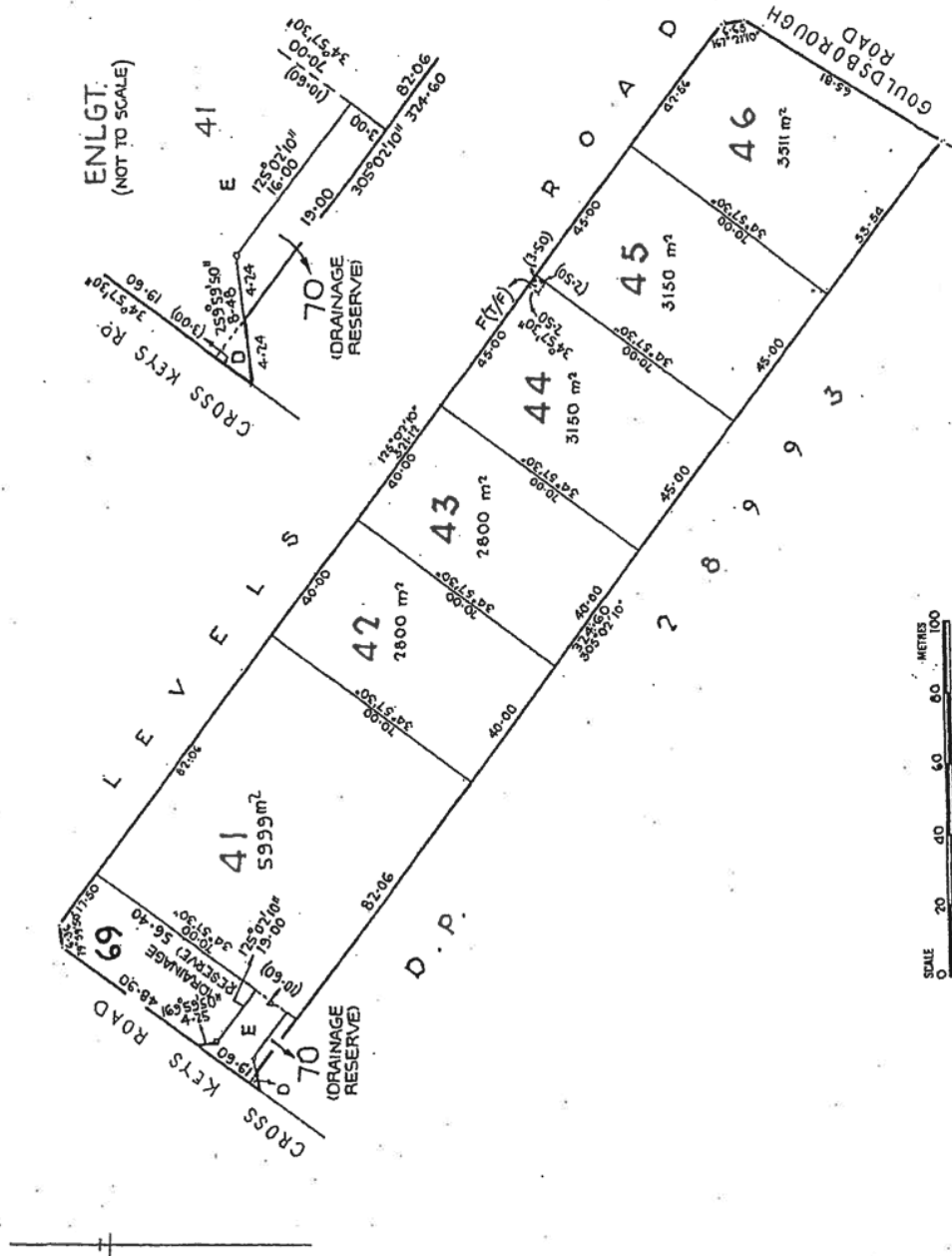
I acknowledge that copies of this application and supporting documentation may be provided to interested persons in accordance with the Development Regulations, 1993. Developments requiring public notification will be made available to the public for comment via Council's web site at [www.salisbury.sa.gov.au](http://www.salisbury.sa.gov.au)

SIGNATURE: \_\_\_\_\_

DATE: 6 / 7 / 17



LANDS TITLES OFFICE ADELAIDE SOUTH AUSTRALIA  
DIAGRAM FOR CERTIFICATE OF TITLE VOLUME 5061 FOLIO 130  
SEARCH DATE : 26/09/2008 TIME: 14:11:46



### Medalstone Pty Ltd

ABN: 18 502 812 449

25 Sharp Court  
CAVAN S.A. 5094  
Phone: 08 8169 1300



PO Box 211  
DRY CREEK S.A. 5094  
Fax: 08 8169 1399

www.beattietransport.com.au

Dated ...20<sup>th</sup> Dec 2017

Marie Molinaro

Our Application Ref 361/1246

We have had a meeting with Alltrans Heavy Haulage to discuss the ongoing application for 106 Levels Road Cavan  
We feel they have done all the necessary requirements to avoid as much noise as possible,  
advise their drivers not to use their reversing beepers, to park only on the side of the road where the premises are and not  
along side of where the housing is.

They may have on rare occasions been probably out of their operating hours but this is an industrial area and not an  
everyday occurrence.

With only running 5 Trucks out of this depot, and a majority of time some of the Trucks are Interstate if would defiantly only  
be on a rare occasion.

Alltrans have also said that there are other Transport Companies running along Levels Road, and some of the Trucks that may  
park along this road have nothing to do with Alltrans.

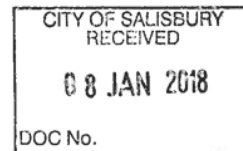
Alltrans have run out of this depot since 2009 and this is the first time there has been any complaints about noise, I do have to  
stipulate that it is a Commercial/Industrial Area and there is a lot of thorough fare at all times of the day and night.

Alltrans is also working with a company who do road works and on occasion start earlier and later in the evening (as do the  
company next door) they are negotiating with the company to be able to leave their truck at their depot on these occasions.

Regards

Debra Hobbs

General Finance Manager





106 Levels Road, Cavan SA, PO Box 1028 Salisbury SA 5120  
Phone: (08) 8359 7770 Mobile: 0419 134725 Fax: (08) 8162 5170  
Email: alltranshh@bigpond.com ABN 31 126 365 680

Alltrans is a Heavy Haulage company and has been working from this depot since 2009 in which the company has not increased its company size. Alltrans is not a general delivery service, the majority of machinery is loaded and delivered to customers premises direct. As we travel all over Australia drivers can be away for long periods of time, and we would have only a couple of trucks working out of the Cavan depot.

The office is a shared office with Hardies, with a shed on site which ½ is used for office storage and other ½ is used for truck parts. No major mechanical repairs are done on site. The trucks are fitted with reversing beepers which is a legal requirement.

Truck Numbers: 5

Staff Numbers - 3 office staff, 4 drivers

Operating hours - Office - 7.30 am to 5.30pm  
Drivers - 7.30am to 6.30pm (depending on work load)

Signage - Color – white, red, black  
Size - 1.2mt x 1.5mt

**Marie Molinaro**

**From:** Debra Hobbs <debra.hobbs@bttransportlogistics.com.au>  
**Sent:** Thursday, 17 August 2017 11:08 AM  
**To:** Marie Molinaro  
**Subject:** RE: Request for additional information - truck parking application - 106 Levels Road, Cavan - Ref. 361/1246

Hi Marie

Alltrans Operate Mon-Friday  
Saturday Morning –Mainly for Maintenance Trucks are not going in and out of the Yard all day  
On the rare occasion on a Sunday which would be just to load or Unload with a max of about ½ Hour  
Or again just to do Maintenance  
Alltrans are not a Transport Company that are loading continually as most of their deliveries are Interstate  
So most of the drivers come into Load and they may not see that same driver for a week

I will arrange payment for the application today

With Thanks Deb

Debra Hobbs | General Finance Manager  
BT TRANSPORT & LOGISTICS  
T 08 8169 1300 | F 08 8169 1399 | E [deb@bttransportlogistics.com.au](mailto:deb@bttransportlogistics.com.au) | W [www.bttransportlogistics.com.au](http://www.bttransportlogistics.com.au)  
Total Logistics Solutions | Military & Defence Capability | Project Design Solutions  
Hot Shots Nationally | Heavy Haulage & Over Size Specialists | 3PL  
Load Master & On Wharf Solutions | Extensive Modern Fleet



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INNOVATIVE DISTRIBUTION SOLUTIONS

**From:** Marie Molinaro [mailto:MMolinaro@salisbury.sa.gov.au]  
**Sent:** Thursday, 17 August 2017 9:04 AM  
**To:** Debra Hobbs  
**Subject:** RE: Request for additional information - truck parking application - 106 Levels Road, Cavan - Ref. 361/1246

Hi Deb

Thank you for the additional information.

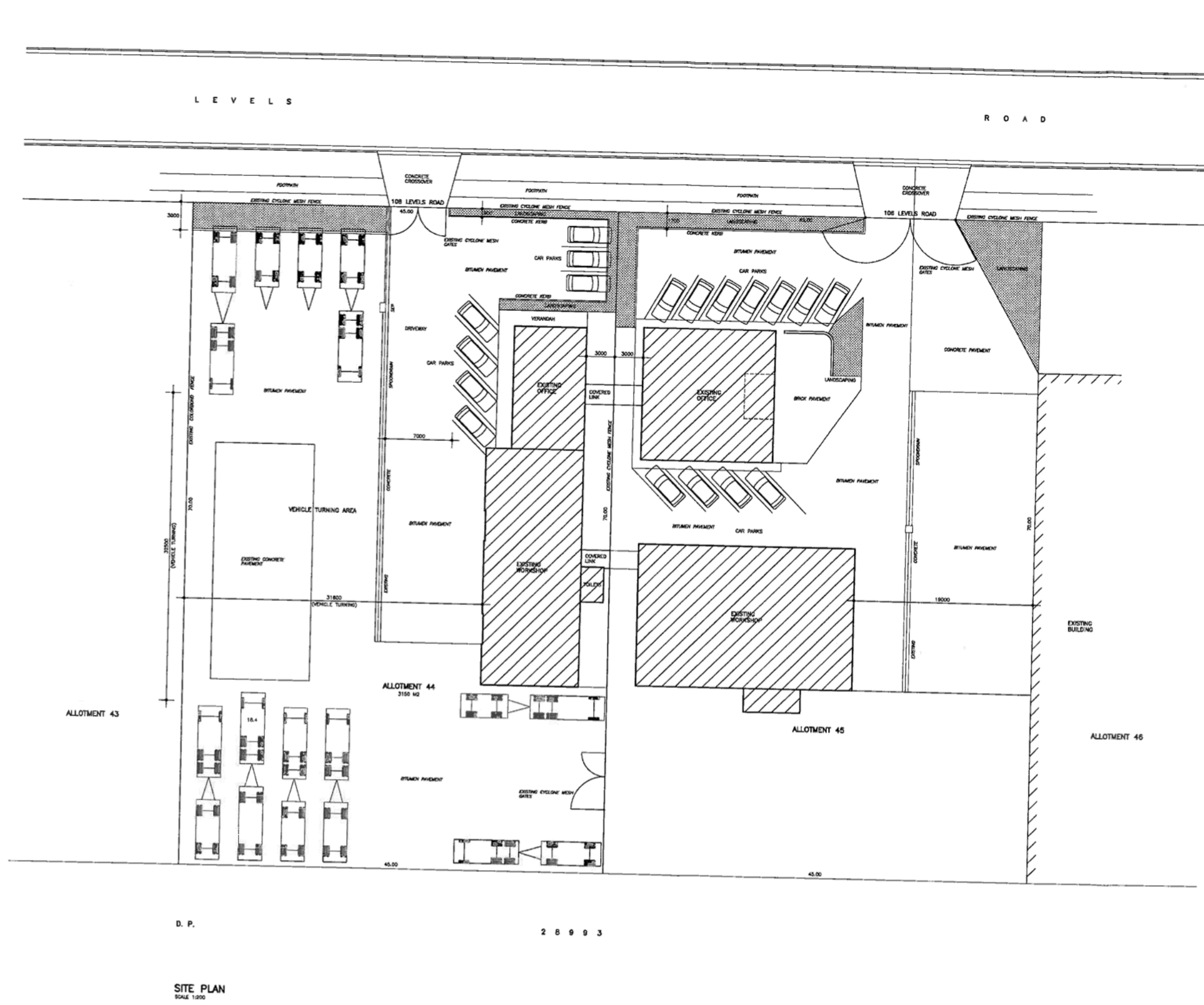
Can you please also clarify though if Alltrans operates 7 days per week (e.g. are there truck movements on the weekend)?

Finally, application fees are still outstanding. In my request for additional information I quoted that the application fees total \$746.00. Apologies, but this amount is actually \$808.50, I incorrectly thought the lodgement fee of \$62.50 had been paid.

1. Location of sign and surface treatment as per enclosed site plan  
All surfaces are bitumen
  
2. 5 cab over prime movers approx. 10t ea  
Maximum trucks parked at any one time 4  
Usually 2 trucks are away interstate at any one time  
As Alltrans is heavy haulage the majority of time the trucks leave at sun up or 9am and return to depot late afternoon  
Alltrans moves earthmoving and mining equipment from one site to another and does not load in depot.  
Trucks are fitted with reversing beeper no exhaust brakes are used  
Only minor mechanical repairs and tyre changing is carried out onsite  
Any waste disposal is picked up by Signal Waste or Mulherns Waste Disposal







CITY OF SALISBURY  
RESOLVED  
26 OCT 2018  
DOC No.

Note:  
All dimensions shall be checked on site prior to commencing shop drawings or fabrication of any building elements.  
Figured dimensions shall take precedence over scale.  
Any discrepancies shall be reported and clarification sought prior to commencing with the building works.

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Rev	Issue	Date	By
B	ISSUED FOR COUNCIL APPLICATION	26.10.2018	DWP
A	PRELIMINARY ISSUE TO CLIENTS FOR APPROVAL	23.10.2018	DWP
	RESPONSE TO COUNCIL REGULATIONS		

'ACE BITUMEN / ALLTRANS'

**D** DAVID W POUND  
PROJECT DESIGN  
Tel: (08447) 787 586  
Email: dwp@dwpprojectdesign.com  
27 - 29 Mansfield Road, FLORENCE, SA 5006

Project: DA 361/1223/2017/3B DA 361/1246/2017  
AT ALLOTMENTS 44 & 45  
LEVELS ROAD CAVAN SA

Title: EXISTING FACILITIES  
AMALGAMATED SITE PLAN

Issue	Scale	Drawing Number	Issue
0001	1:200		
0002	1:200	18/08-A02	B
0003	DWP		

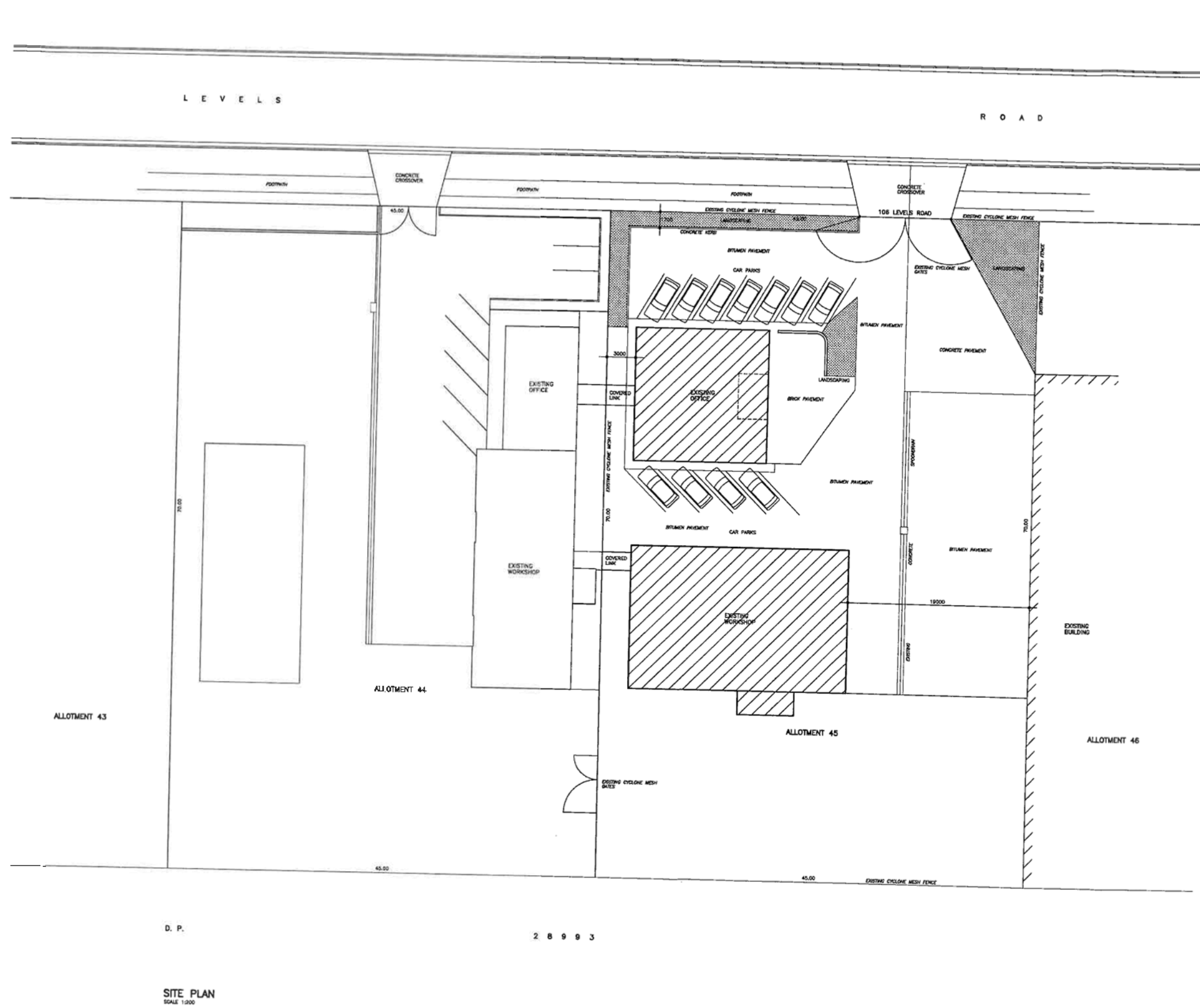
25/10/2018 1:28 PM FILE NAME:183-ALLIEN

D. P. 2 8 9 9 3

SITE PLAN  
SCALE 1:200







CT REFERENCE:  
 CT --- FOLIO ---  
 VOL ---  
 ALLOTMENT 45 DEPOSITED PLAN  
 IN THE AREA NAMED CAVAN  
 HUNDRED OF YATALA  
 106 LEVELS ROAD CAVAN SA

Note:  
 All dimensions shall be checked on site prior to commencing shop drawings or fabrication of any building elements.  
 Fixed dimensions shall take precedence over scale.  
 Any discrepancies shall be reported and clarification sought prior to continuing with the building works.

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Rev	Issue	Date	By
B	ISSUED FOR COUNCIL APPLICATION	25.10.2018	DWP
A	PRELIMINARY ISSUE TO CLIENT FOR APPROVAL (RESPONSE TO COUNCIL REQUEST)	25.10.2018	DWP

'ALLTRANS HEAVY HAULAGE PTY LTD'

**DAVID W POLINO PROJECT DESIGN**  
 Tel: (0844) 757 686  
 Email: dwpolino@bigpond.com  
 27 - 29 Mansell Road POORANNA SA 5006

Project: DA 381/1246/2017 AT ALLOTMENT 45 LEVELS ROAD CAVAN SA

Title: EXISTING FACILITY SITE PLAN

scale	1:200	drawing number		sheet	
date	18/08/18		18/08-A03		B
drawn	DWP				

15/10/2018 10:14 FILE NAME: 188-45206

D. P. 2 8 9 9 3

SITE PLAN  
 SCALE 1:200



# Alltrans Heavy Haulage

106 Levels Road

Environmental Noise Assessment

S5789C2

November 2018

# sonus.

Jason Turner  
Senior Associate  
Phone: +61 (0) 410 920 122  
Email: [jturner@sonus.com.au](mailto:jturner@sonus.com.au)  
[www.sonus.com.au](http://www.sonus.com.au)

Prepared by  
Jason Turner (MAAS)

Reviewed by  
Chris Turnbull (MAAS)



**Document Title** : Alltrans Heavy Haulage  
Environmental Noise Assessment

**Document Reference** : S5789C2

**Date** : November 2018

**Author** : Jason Turner, MAAS

**Reviewer** : Chris Turnbull, MAAS



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Alltrans Heavy Haulage  
Environmental Noise Assessment  
S5789C2  
November 2018

sonus.

### **INTRODUCTION**

An environmental noise assessment has been made of activity at Alltrans Heavy Haulage (Alltrans), 106 Levels Road, Cavan (the site).

The site is proposed to be used as a truck depot which can be accessed at any time of the day and night. The adjacent truck depot at 108 Levels Road is also proposed to be use by the Alltrans trucks as a thoroughfare to enable a forward in and forward out maneuver through 106 Levels Road.

The closest residences to the site are on Petrel Crescent. The interface between Levels Road, which carries a range of heavy commercial traffic, and the residential area including Petrel Crescent, is addressed by a fence which extends above the eaves height of the dwellings (which is a good acoustic feature). A site layout and the closest residences to the proposal are shown in Appendix A.

The assessment determines appropriate noise criteria in accordance with the Development Plan and ensures that noise from the site achieves the requirements, such that the proposal does not unreasonably impact on the amenity of the nearby residences.

The assessment has been based on:

- A site inspection and measurements of the noise from trucks which will access the site on 18 October 2018;
- The potential for trucks to access the facility at any time of the day or night;
- Discussions with the operator regarding practical noise reduction measures; and,
- No loading or unloading activity occurring at the depot other than the occasional hitching or unhitching of trailers.



**CRITERIA**

**Development Plan**

The site is located within the Industry Zone of the Salisbury Council Development Plan and the existing residences are located within the Residential Zone. The Development Plan has been reviewed and particular consideration has been given to the following relevant provisions.

**General Section:**

**Industrial Development**

**Objective 4:** *Industrial development occurring without adverse effects on the health and amenity of occupiers of land in adjoining zones.*

**PRINCIPLES OF DEVELOPMENT CONTROL**

**6.** *Industrial development should minimise significant adverse impact on adjoining uses due to... noise...*

**Interface between Land Uses**

**Objective 1:** *Development located and designed to minimise adverse impact and conflict between land uses.*

**Objective 2:** *Protect community health and amenity from adverse impacts of development.*

**Objective 3:** *Protect desired land uses from the encroachment of incompatible development.*

**PRINCIPLES OF DEVELOPMENT CONTROL**

**1** *Development should not detrimentally affect the amenity of the locality or cause unreasonable interference through any of the following:*

...

*(b) noise*

...

**2** *Development should be sited and designed to minimise negative impacts on existing and potential future land uses desired in the locality.*

**6** *Non-residential development on land abutting a residential zone should be designed to minimise noise impacts to achieve adequate levels of compatibility between existing and proposed uses.*

Alltrans Heavy Haulage  
Environmental Noise Assessment  
S5789C2  
November 2018

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**Noise Generating Activities**

- 7 *Development that emits noise (other than music noise) should include noise attenuation measures that achieve the relevant Environment Protection (Noise) Policy criteria when assessed at the nearest existing noise sensitive premises.*
- 8 *Development with the potential to emit significant noise (e.g. industry) should incorporate noise attenuation measures that prevent noise from causing unreasonable interference with the amenity of noise sensitive premises.*

**Environment Protection (Noise) Policy 2007**

The Development Plan provisions make specific reference to the *Environment Protection (Noise) Policy 2007* (the Policy). The Policy is based on the World Health Organisation Guidelines to prevent annoyance, sleep disturbance and unreasonable interference with the amenity of a locality. Therefore, compliance with the Policy is considered to satisfy the relevant provisions of the Development Plan related to environmental noise.

The Policy provides goal noise levels based on the principally promoted land use in the Development Plan Zones of the noise source (ACE Bitumen) and noise receivers (residences). Based on the Industry and Residential zoning of the development site and residences respectively, the policy applies the following goal noise levels;

- An average ( $L_{eq}$ ) noise level of 54 dB(A) during the day (7am to 10pm);
- An average ( $L_{eq}$ ) noise level of 45 dB(A) at night (before 7am or after 10pm); and,
- An instantaneous maximum ( $L_{max}$ ) noise level of 60 dB(A) at night (10pm to 7am).

When measuring or predicting noise levels for comparison with the average ( $L_{eq}$ ) goal noise levels of the Policy, penalties may be applied to the goal noise levels for each characteristic of tone, impulse, low frequency and modulation of the noise source. To apply a penalty, these characteristics of the noise source must be considered dominant in the context of the existing acoustic environment.

The existing acoustic environment at the site includes noise from trucks and passenger vehicles on Levels Road. The road separates the subject site from the residences (as shown in Appendix A). The residences are therefore subject to trucks and vehicles on Levels Road which are closer in proximity, travelling at greater speed and occurring at a greater frequency of occurrence in comparison to the low speed truck movements on the site.



In such a circumstance, the noise from the forward movement of trucks and passenger vehicle activity on the site cannot adversely impact on the amenity of the locality other than in the following circumstances:

- The trucks on the site have to reverse and in so doing, introduce a noise source with a different characteristic (a reversing tone) into the environment;
- The trucks need to idle for long periods of time, which introduces a noise source with a different characteristic into the environment;
- Passenger vehicles opening and closing doors, which is a different characteristic to that which already regularly occurs in the environment.

Based on the above observations and influence of the existing public road network, the site layout has been designed such that trucks will only reverse into the parking bays during the day time period on their return to the site, and will enter and/or exit the site with forward-only movement during the night time hours (refer site plan in Appendix A).



Alltrans Heavy Haulage  
Environmental Noise Assessment  
S5789C2  
November 2018

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#### **ASSESSMENT**

The assessment of the proposal has been based on a site inspection and noise measurements of the following activities on 18 October 2018;

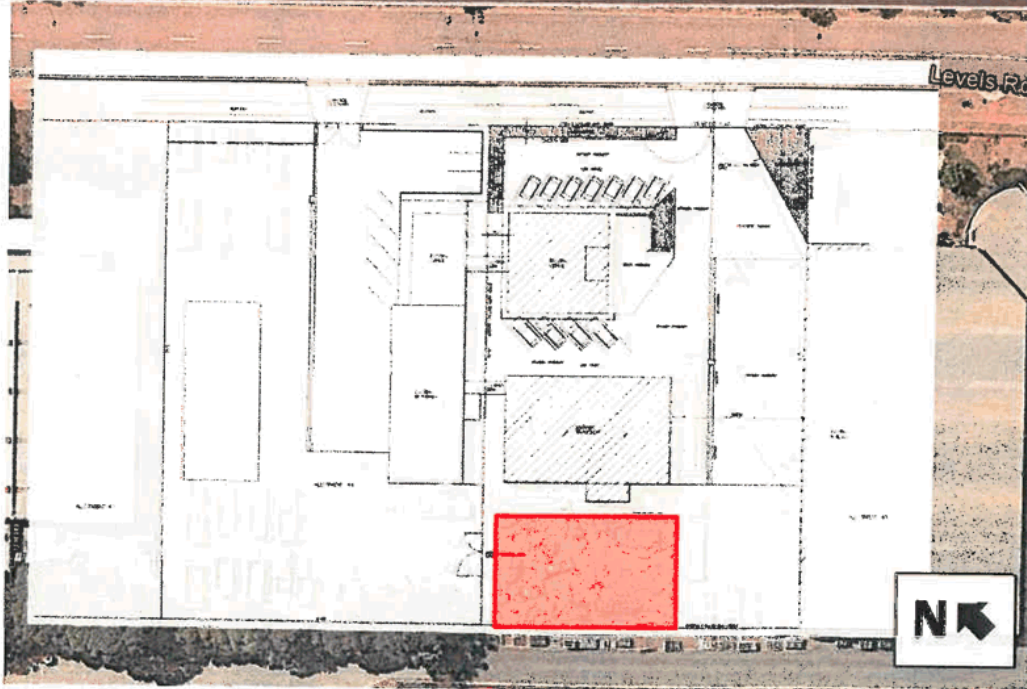
- Trucks entering and exiting the site; and,
- Trucks starting up and idling.

It is understood that the site will be used only by Alltrans trucks which were measured during the site inspection. The measured noise level has been used for predicting the noise levels from the site with the following level of activity within any 15 minute period (the default assessment period of the Policy) at any time of the day or night;

- 2 Trucks either;
  - Enter the site and park at the rear of the site; or,
  - Drive from their parked position out of the site through the 108 Levels Road Site.
- 2 trucks idle for up to 5 minutes each
- passenger vehicle movements into or out of the site

In order to achieve the goal noise levels of the Policy, the following acoustic treatments have been developed for the site;

- Ensure that trucks do not reverse while on site during the night time hours (10pm to 7am);
- Ensure no loading or unloading occurs on the site, with the exception of occasional hitching and unhitching of trailers during the day time hours (7am to 10pm) only;
- Restrict the amount of time a truck can idle after entering or before leaving the site to be no more than 5 minutes;
- Restrict the area where trucks can park on the site to that shown in the following figure, when entering or existing during the night time hours of the Policy (10pm to 7am).



- Implement a Noise Management Plan which requires truck activity to be in accordance with the restrictions in this report.

With the above acoustic treatments incorporated, the predicted noise level at the existing residences is predicted to be no more than 42 dB(A), therefore achieving both the day time and night time goal noise levels of the Policy (being 54 dB(A) and 45 dB(A) respectively).

The maximum noise level from activity such as truck brake compressed air release, people talking or opening and closing doors during the night time hours has also been predicted based on previous noise measurements at other similar sites. The highest predicted ( $L_{max}$ ) noise level at the residences is 50 dB(A), therefore easily achieving the 60 dB(A) goal noise level of the Policy.

Alltrans Heavy Haulage  
Environmental Noise Assessment  
S5789C2  
November 2018

sonus.

#### **CONCLUSION**

An environmental noise assessment has been made of the proposed Alltrans Heavy Haulage truck depot at 108 Levels Road, Cavan.

The assessment considers noise levels at the existing residences in the vicinity of the proposal based on specific noise measurements of the trucks which will access the site.

Appropriate noise criteria have been derived for the site based on the Development Plan, the *Environment Protection (Noise) Policy 2007*.

Predictions of the noise at residences will achieve the noise criteria where the following measures are implemented;

- Limiting the number of trucks using the site is limited;
- Restricting the time of truck idling;
- no loading or unloading occurring on site, with the exception of hitching and unhitching trailers during the day time; and,
- Restricting the area where trucks may park and leave the site from during the night time hours.

Based on the above, the development is considered to have been designed to prevent adverse impact, detrimental effects, and to not unreasonably interfere with the existing acoustic amenity of the community, thereby achieving the relevant provisions of the Development Plan.

Alltrans Heavy Haulage  
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55789C2  
November 2018

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APPENDIX A: Site and Residence Locality





**APPENDIX B: NOISE MANAGEMENT PLAN**

**ALLTRANS HEAVY HAULAGE- NOISE MANAGEMENT PLAN**

**1. INTRODUCTION**

*This Noise Management Plan (the Plan) specifies an action plan and procedure for the management of environmental noise from the Alltrans Heavy Haulage truck depot at 106 Levels Road, Cavan.*

**2. SCOPE**

*The Plan includes the appropriate design and management of the site, and the requirements of all staff to minimise environmental noise emissions.*

**3. OBJECTIVE**

*The objective of the Plan is to satisfy the "Part 4 Clause 25 – General Environmental Duty" of the Environment Protection Act 1993 (the EP Act).*

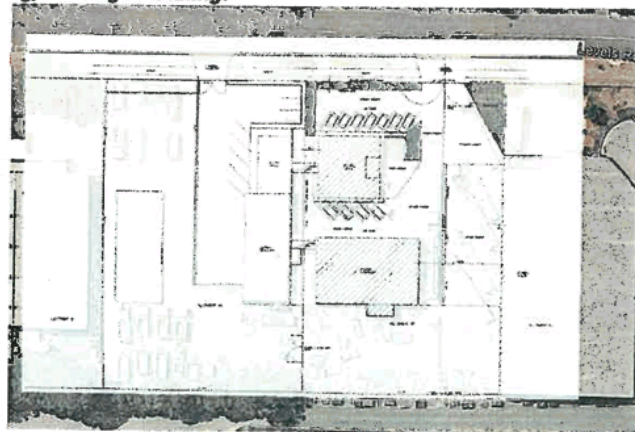
*Compliance with the policies and guidelines made under the EP Act, such as the Environment Protection (Noise) Policy 2007, is one method of ensuring compliance with the Clause 25 provisions, in circumstances where these objective standards cannot be achieved, then the Section 25 provisions can still be satisfied by taking all "reasonable and practicable" noise reduction measures. The extent of "reasonable and practicable" is subject to consideration of factors relevant to the individual circumstance.*

*In order to achieve the Environment Protection (Noise) Policy 2007 a noise level ( $L_{Aeq,10min}$ ) of no more than 45 dB(A) would need to be achieved during the night time hours (10pm to 7am) and no more than 54 dB(A) during the day time hours (7am to 10pm).*

**4. RISK ASSESSMENT**

**4.1. Site Layout and Associated Noise Sources**

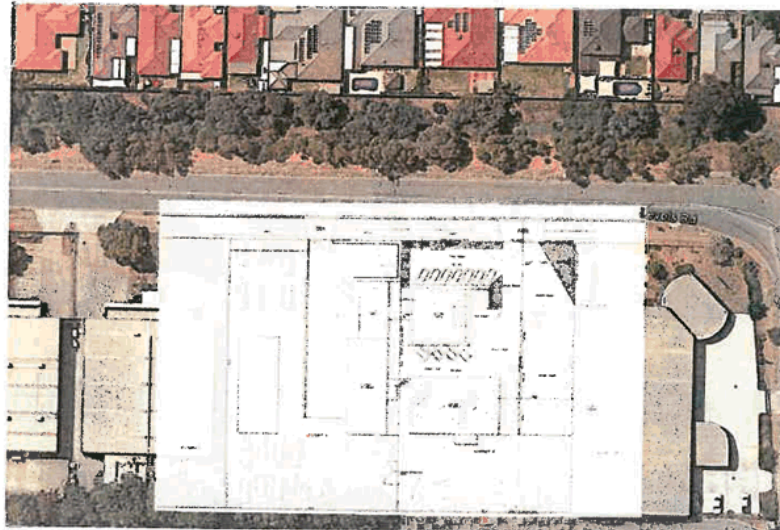
*The site layout is shown in the following Figure, with the main noise sources at the site being trucks entering/existing, starting and idling:*





**4.2. Noise Sensitive Receivers**

*The closest noise sensitive receivers to the site are the existing residences opposite Levels Road, as shown in the following Figure;*



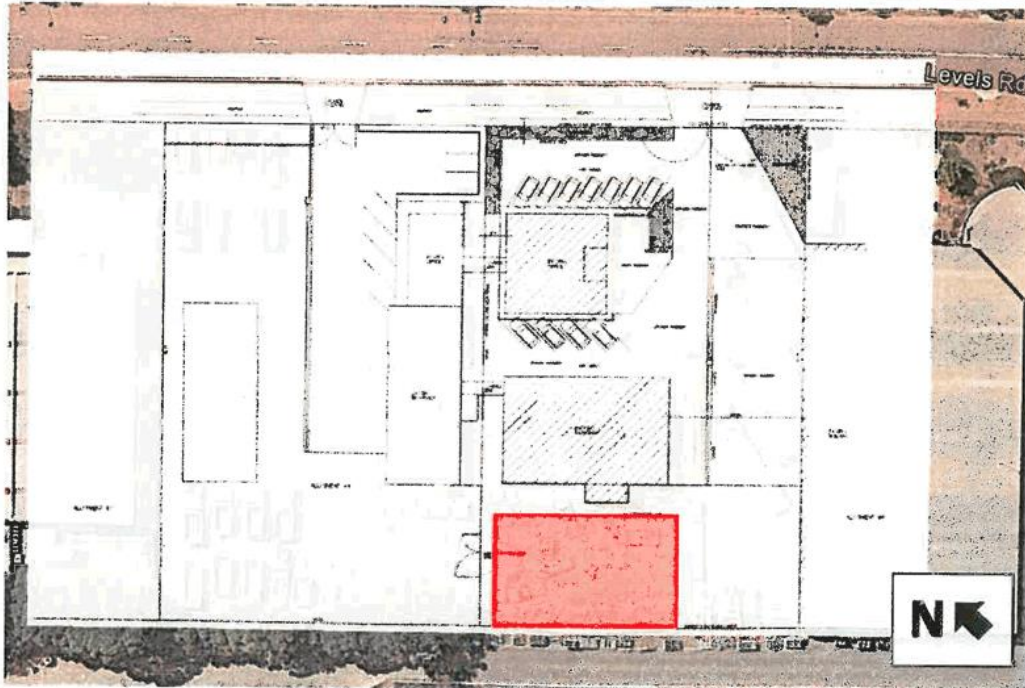
**4.3. Mitigation**

*In order to satisfy the Act, all reasonable and practicable measures must be taken to minimise the noise impact at the surrounding residences through the implementation of this Plan. As a result of implementing this plan, noise from the site will also be reduced such that compliance with the Environment Protection (Noise) Policy 2007 will be achieved, thereby ensuring the General Environmental Duty is satisfied.*

**5. NOISE MANAGEMENT MEASURES**

*The noise management measures that will be implemented in order to minimise environmental noise impact on the surrounding residences are provided below.*

- As per the current site plan, ensure that trucks are not required to reverse while on site during the night time hours (10pm to 7am);
- Ensure no loading or unloading occurs on the site, with the exception of occasional hitching and unhitching of trailers during the day time hours (7am to 10pm) only;
- Restrict the amount of time a truck can idle after entering or before leaving the site to be no more than 5 minutes;
- Restrict the area where trucks can park on the site to that shown in the following figure, when entering or existing during the night time hours of the Policy (10pm to 7am).



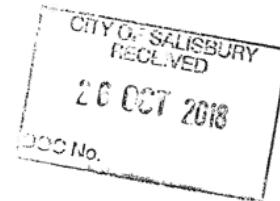






106 Levels Road, Cavan SA, PO Box 1028 Salisbury SA 5120  
Phone: (08) 8359 7770 Mobile: 0419 134725 Fax: (08) 8162 5170  
Email: alltranshh@bigpond.com ABN 31 126 365 580

1<sup>st</sup> October, 2018



Alltrans is a Heavy Haulage company and has been working from this depot since 2009 in which the company has not increased its company size.  
Alltrans is not a general delivery service, the majority of machinery is loaded out on site and delivered to customers premises direct. As we travel all over Australia drivers can be away for long periods of time, and we would have only a couple of trucks working out of the Cavan depot.

Alltrans operates out of the office with a shed on site which ½ is used for office storage and other ½ is used for truck parts. No major mechanical repairs are done on site.  
Alltrans has 24hr access to the rear gates that adjoin the properties and will leave or enter this way.

As Alltrans has on going work with a profiling company that's work requires at times early and late starts Alltrans has now arranged with K&L Profiling so Alltrans drivers start and leave from their depot when required.

Truck Numbers: 4

Staff Numbers - 4 office staff, 4 drivers

Operating hours - Office - 6.30 am to 5.30pm  
Drivers - 7.00 am to 7.00pm (depending on work load)



25 October, 2018

Mrs Julie Frost  
 Alltrans Heavy Haulage Pty Ltd  
 106 Levels Road  
 CAVAN SA 5094



Dear Julie,

We advise that we are prepared to allow your Company's vehicles access to our property to either enter or exit through front and rear gates as required, especially after hours and at weekends.

This will eliminate the need for your vehicles to back off Levels Road, thus reducing the noise impact on the adjacent residents.

Yours faithfully,  
**Ace Bitumen Contractor Pty Ltd**

A handwritten signature in black ink that reads "Jan Jarrett".

JAN JARRETT  
**Office Administrator**





ABN 11 500 996 781

79 Cavan Road  
GEPPS CROSS SA 5094  
PO Box 2528  
REGENCY PARK SA 5942  
Telephone: 8360 2308  
Facsimile: 8268 9367  
Website: [www.klprofiling.com.au](http://www.klprofiling.com.au)



**TO WHOM IT MAY CONCERN**

K & L Profiling have agreed to allow Alltrans Heavy Haulage drivers access to their depot as follows:

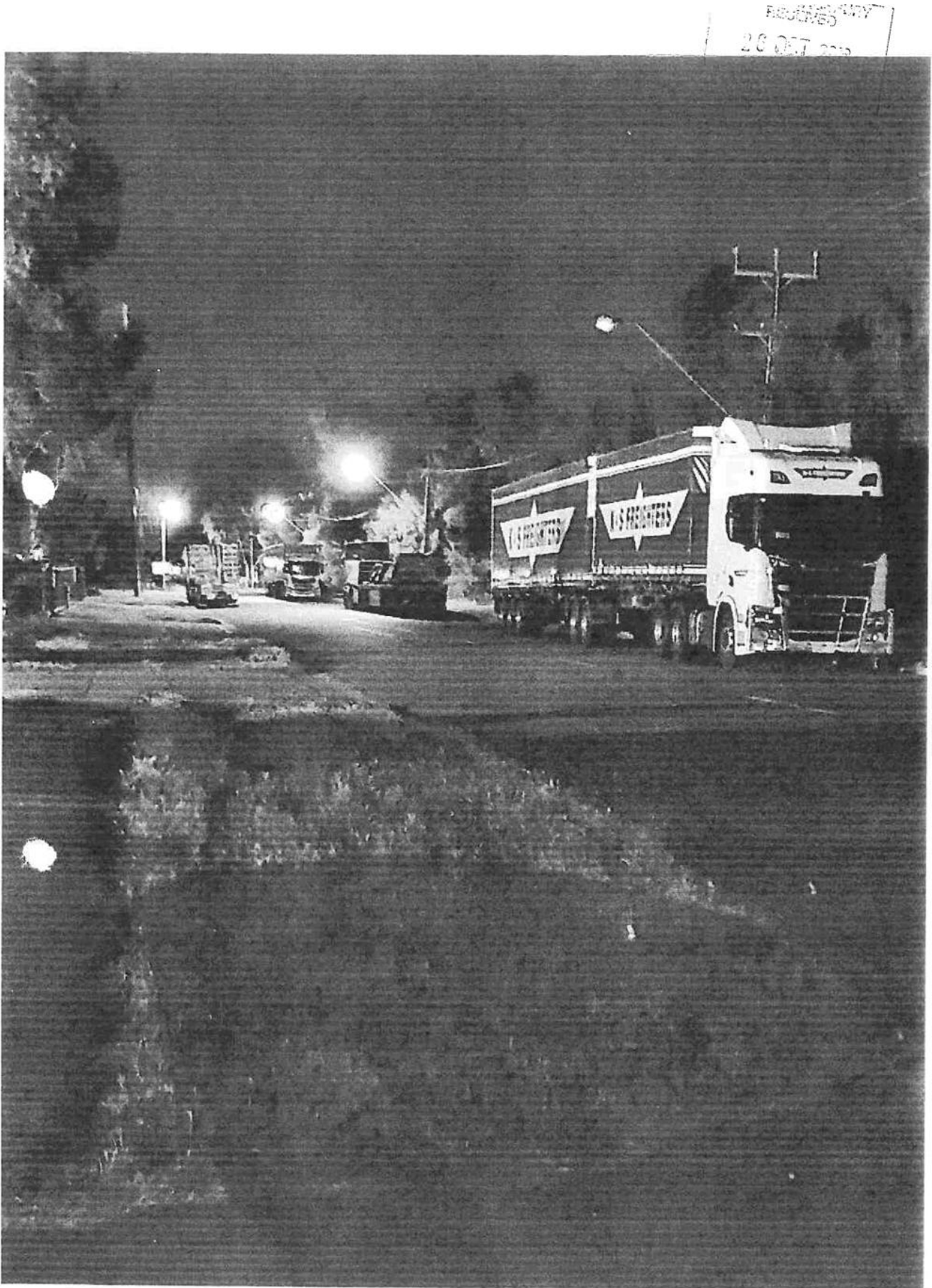
During the weekends and at times when early morning or late afternoon/evening float shifts have been booked.

Alltrans drivers can then drive their own personal vehicles to the K & L Profiling Depot to start and finish with their vehicles

Any further queries, please contact me at your convenience

Regards

Lawrence Parry  
Director  
K & L Profiling



1:30 AM FRIDAY 19<sup>TH</sup> OCTOBER, 2018

# ALLTRANS HEAVY HAULAGE PTY LTD



**"Innovative Distribution Solutions, Driven by Service"**

## INDUCTION MANUAL

Alltrans Heavy Haulage – Induction Manual (version 1SR)

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Alltrans Heavy Haulage - Induction Manual (version 1SR)

# SECTION 1

## GENERAL EMPLOYEE INFORMATION

**"Innovative Distribution Solutions, Driven by Service"**

### 1. INTRODUCTION

Alltrans Heavy Haulage is one of the most innovative national freight companies in Australia and there are many reasons why Alltrans Heavy Haulage are successful in the transport industry; the main undoubtedly, is our genuine commitment to customer service.

At Alltrans Heavy Haulage our customers are our number one priority. This induction handout is designed to give employees and contractors an overview of our policies, to provide general information in terms to conditions of employment and further highlight the importance of occupational health and safety for all employees.

It is also designed to emphasise our service objective - **"To deliver all freight on time, in full and damage free"**. Alltrans Heavy Haulage believe in total quality performance and are committed to the principles of Total Quality Management (TQM) in everything we do.

### 2. CUSTOMER SERVICE COMMITMENT

Customers remember excellent service, and they also remember a bright and enthusiastic response to their needs. As employees in a competitive service industry, we must consistently strive for service excellence.

Service excellence can be reflected in our appearance, actions and attitudes. It also shows in how we talk to our customers and how we answer the telephone.

### 3. TERMS OF ENGAGEMENT

Employment may be on the basis of weekly, monthly, casual or temporary engagement. For employees covered by awards, their conditions of employment are contained in the appropriate awards and agreements. The conditions of employment of other employees are set out in their letter of appointment.

#### Employment

Supervisors and managers are to ensure that all employees:

- Have a current position description.
- Have a letter of appointment.
- Are inducted as per the Company's procedure contained in section 12 of the HR Manual.
- Hold valid licences for any equipment they are required to operate

Hold valid licenses for any equipment they are required to operate.

All employees are bound by the various company policies which form part of their contract of employment. Notice of termination of service by either the company or the employee must be in accordance with either the appropriate award, letter of employment or general community standard.

The company may dismiss an employee without notice in the case of serious misconduct, including unacceptable behaviour.

### 4. PERSONAL RECORDS

It is important that the company always has up to date details of your address, dependants, telephone number, next of kin etc. Please advise your supervisor manager as soon as possible after any such change has occurred.

### 5. PAYMENT OF WAGES AND SALARIES

Net salaries or wages are paid by electronic funds transfer into an employee nominated bank account. Deductions from wages for superannuation credit union, medical benefits, life insurance, social club and other approved deductions will be made upon request.

### 6. TIME RECORDING AND ABSENCE FROM WORK

All employees are required to start work on time. Please give your supervisor or manager as much notice as possible if you cannot commence work on time.

Driver run sheets are to be filled out everyday with relevant information.

Alltrans Heavy Haulage - Induction Manual (version 1SR)



**7. LEAVING & RETURNING TO DEPOT & PARKING OF VEHICLES**

Personal vehicle parking facilities are made available by the company. Such areas are to be used as intended and indiscriminate parking in unauthorised areas will not be allowed. The company will not accept responsibility for loss or damage to vehicles, their content or personal property. Alltrans truck parking must be in designated areas. All trucks on start up and returning to depot must not idle for more than 5min and speed limits in the depot must be adhered to.

**8. PERSONAL APPEARANCE AND UNIFORMS**

As employees and representatives of the company, it is important that we present ourselves professionally at all times, particularly to our customers and to the general public. To assist in promoting the company's image employees must be well groomed and attired at all times when at work or on company business.

All permanent operations staff will be issued with a full set of company clothing after a one-month qualifying period. The issue is for a minimum of 12 months. The clothing will be renewed after this period taking into account reasonable wear and tear. Employees are required to maintain the clothing provided in good condition.

The clothing provided shall only be worn when employees are engaged in work for Alltrans Heavy Haulage and shall remain the property of the employer.

Employees leaving Alltrans Heavy Haulage within 12 months shall return the clothing or be charged for the clothing on a pro-rata basis.

You must wear appropriate footwear and clothing for your own protection. With respect to footwear, you are not permitted to wear thongs, slippers, sandshoes or gumboots. Safety boots are relevant personal safety equipment must be worn in operational area and when specified by a customer as a condition of access to their premises.

**9. LEAVE ENTITLEMENTS****Annual Leave**

Employees are entitled to four weeks annual leave for each 12 months completed service. Leave arrangements are to be made taking into account workloads and seasonal vacation in the business. Annual leave entitlements are not to be accumulated.

**Personal Leave (Bereavement, Sick and Carer's Leave)**

Bereavement, sick and carer's leave are now combined and referred to as personal leave. For employees covered by awards, the entitlements are set in the appropriate award. For other employees, entitlements are set out in their letter of appointment.

**Special Leave Without Pay**

When absence from work is unavoidable due to urgent or unusual circumstances, employees may request leave without pay from their Supervisor or Manager. A written application must be completed stating the reason for absence and expected duration.

**Long Service Leave**

Long Service Leave entitlements accrue in accordance with the legislative provisions applicable in the state of employment, or are contained in the appropriate federal or state awards. Please ask your supervisor about your entitlement.

**10. PROBLEM RESOLUTION**

If you are having a grievance or problem of any kind, please raise the matter with your supervisor or manager. Matters of a personal nature will remain confidential between you and the company.

**11. CODE OF CONDUCT GENERAL REQUIREMENTS OF ALL EMPLOYEES****INTRODUCTION**

The purpose of this document is to define the duties and obligations of all employees within Alltrans Heavy Haulage, with respect to their employment. It does not address conditions of employment which are set out in personal contracts, awards or agreements, but is aimed at the standards of conduct generally required of employees by their employer.

Employees who have any queries about the contents of this document or who are unsure of any aspect insofar as it affects how they do their job should raise the matter with their manager or supervisor.

**GENERAL POLICY**

Employees are expected to act always in the best interests of the Company. This means acting professionally, honestly, ethically and within the law. All employees are representatives of the Company and must not indulge in any activity that could damage the Company's reputation. Any employee who are aware or who suspects impropriety by others, should advise their manager or supervisor or to a more senior manager if necessary. Employees should be aware that breaches of the standards required could lead to dismissal.

COMPLIANCE WITH THE LAW

Employees are required to conduct all their activities within the law. There is no excuse for employees who knowingly breach the law whilst acting within the course of their employment. Whilst employees are not expected to have a detailed knowledge of the law, they should be aware of those areas of law that affect their jobs, including the law relating to occupational health and safety, equal opportunity, harassment, etc.

PRIVACY

The manner in which personal information is managed within the Company is regulated by federal privacy legislation applying to the private sector. Details of the requirements as well as the rights and obligations of the Company and its employees are contained in the Employment Policies and Procedures Manual.

PRIVATE USE OF COMPANY COMMUNICATIONS SYSTEMS

Company provided communications systems (email, internet, facsimiles, telephones etc) must not be utilised for any other purpose other than legitimate business purposes. Whilst occasional private use is accepted, any usage that can be regarded as offensive or illegal will may result in termination of employment. The company monitors the operation of its communications systems and any recorded content.

LICENCES

Employees who operate company equipment that requires the operator to be licensed, must hold a valid licence whenever they operate such equipment. Employees whose licence is cancelled must inform their supervisor immediately.

DUTY OF CARE

Employers and employees have a legal duty of care that basically requires everybody, irrespective of their job, not to act or fail to act in any way that is or could be detrimental to the health, safety and welfare of themselves or others.

CONFLICT OF INTEREST

Employees must avoid any conflict of interest between their obligations to the Company and their personal activities or interests and should not take improper advantage of their position within the Company, or of access to information or knowledge gained in the course of their employment.

COMPLIANCE WITH TRADE PRACTICES LEGISLATION

All employees must in accordance with the Company's anti-competitive arrangements which result in decreased competition or disadvantage to customers and consumers, through price fixing or other such activities. comply with the trade practices legislative requirements, which prohibit

PROTECTION OF COMPANY PROPERTY

Employees are not permitted to use any Company property for their own use without approval from their manager or supervisor. Property in this context will include but is not limited to, equipment, computer hardware, networks and software. Employees are required to take reasonable care of Company property used in their jobs or entrusted to them.

CONFIDENTIAL INFORMATION

Employees must not disclose to any person or organisation any information which is confidential or which could be regarded as sensitive to the Company's interests, including all forms of Intellectual knowledge, without the express permission of their manager or supervisor. All forms Intellectual information and knowledge remain the property of the company at all times.

INDUCEMENTS AND GIFTS

Decisions or activities must not be influenced through offering or accepting inducements or gifts.

BUSINESS RECORDS AND DOCUMENTATION

All business or employment records and documentation must be prepared accurately and any procedures associated with such must be strictly adhered to. Deliberate falsification or any documentary record, whether such is a Company document or not, may lead to dismissal, whether or no there is any gain to the employee(s) concerned or not.

THEFT AND MISAPPROPRIATION

Theft or misappropriation of Company property or property belonging to other employees or a third party whilst on Company premises, will result in dismissal and the matter will be referred to the police.

## 12. OUR OBLIGATIONS WITH RESPECT TO EQUAL OPPORTUNITY AND BEHAVIOUR IN THE WORKPLACE

As part of its duty of care, the Company must ensure that all of its employees are aware of our mutual obligations with respect to equal opportunity matters and the standard of behaviour required of us whilst in the course of our employment.

The Company's Employment Policy, which is displayed in all depots, states:

"The Company undertakes to provide a safe and healthy workplace free from unfair discrimination or harassment. It is a condition of employment that all employees observe this commitment and any legislation pertaining to these matters. The Company further undertakes to educate employees in such matters to assist compliance with the law.

The Company also undertakes to provide equal opportunity for all employees in selection, retention, development and promotion."

Harassment involves intimidate occurring through unwanted and unsolicited actions, requests or comments being made. Examples of harassment range from bullying, threats of violence, repeated foul language, racial taunts, etc. at the extreme to more subtle situations such as unwanted attention, touching, suggestive remarks, etc.

Unfair discrimination envisages unfair or unreasonable distinctions being made between people to the advantage of some and the disadvantage of others. Examples of unfair discrimination include failing to hire and or promote someone because of their race, religion, age, sex, marital or parental status, physical status, colour or ethnicity, pregnancy, etc. Broadly, harassment or unfair discrimination on any of the above grounds is unlawful.

Please Note: The Company does have guidelines for the confidential management of problems. These are contained in its . Should you require a copy of this section please ask your supervisor.

Our Responsibilities are as follows:

1. All of us must observe the obligations outlined above.
2. Managers and Supervisors are responsible for ensuring that harassment and unfair Discrimination does not occur in the workplace and that employees understand their Obligations.
3. Any staff involved in recruitment, development and training must ensure that all Activities are conducted fairly in accordance with this policy.

Please take the time to read this thoroughly. Should you have any concerns about this subject or require guidance in any way, you should refer to either your supervisor, or your Manager.

## 13. ACCREDITATIONS, MEMBERSHIPS & LICENCES TRUCKSAFE

Trucksafe is an industry initiative designed to deliver competitive advantages to accredited operators through the implementation of systems aimed at improving the safety and professionalism of trucking operators. Trucksafe has four main standards of driver health and safety, training, maintenance and management.

### PACIA (PLASTICS AND CHEMICAL INDUSTRIES ASSOCIATION)

Plastics and chemicals Industries Association (PACIA) aims to improve safety performance of chemical transport in Australia, helping to provide tangible benefits to both people and the environment. The PACIA scheme aims to reduce the duplication and audit costs to carriers associated with chemical companies complying with the "Responsible Care Transportation Code of Practice".

### FATIGUE MANAGEMENT SCHEME

The "Fatigue Management Scheme" is designed to provide a tool to manage transport operations and responsible driving hours. The scheme provides greater flexibility to both participating transport operators and drivers in the management of driving hours.

In order that driver participate in the FMS scheme all drivers, supervisors and managers must undertake an approved course in fatigue management, then maintain the appropriate log to record driving hours.

### MASS MANAGEMENT

The Mass Management Scheme is an initiative between Vic Roads and the Transport Industry.

The Mass Management system allows accredited operators to use documented assurance system to demonstrate compliance with regulation thereby achieving commercial gains by increases allowed in respect of loads.

### QUALITY ASSURANCE

Quality Assurance Systems (QAS) the only certificate body in Australia that has the rights to the famous five ticks mark. Alltrans Heavy Haulage has been committed to their Quality System and ISO9002 accreditation since 1998, through all participating Divisions.

The Quality Management System refers to the activities within an organisation which are carried out to satisfy the quality related expectations its customers and as such Quality Assurance has contributed to Alltrans Heavy Haulage high standards ensuring its professionalism of a leading national transport company.

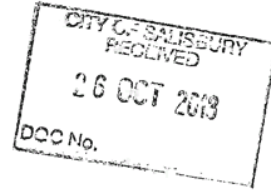
Alltrans Heavy Haulage - Induction Manual (version 1SR)



Attachment 2  
Vehicle Log Details provided by Applicant

alltranshh

From: Kym Burgess <kym.burgess@klprofiling.com.au>  
Sent: Thursday, 18 October 2018 8:23 AM  
To: 'alltranshh@bigpond.com.au'  
Subject: FW: FLOAT SHIFTS



UPDATED 18/10/18 at 8:00 AM

Thursday 18<sup>th</sup> OCTOBER 2018

Pick up RX700 at K&L Depot at 5:00 AM and Deliver RX700 to Goldsworthy Crescent (Kibby Avenue End) by 6:45 AM (Wait on site) reload and deliver back to K&L Depot *Jewell*

Pick up WIRTGEN W1000F at K&L Depot at 4:30 AM and Deliver Golf Course Road overtaking lane in Renmark by 7:30 AM (Wait on site) reload and move to Hittmann Road and Halfway House reload and deliver back to K&L Depot *Jewell*

Fr. 19<sup>th</sup> OCTOBER 2018

WORK CANCELLED AT THIS STAGE

Saturday 20<sup>th</sup> OCTOBER 2018

WORK CANCELLED

Sunday 21<sup>st</sup> OCTOBER 2018

Pick up RX700 from K&L Depot at 4:00 PM and deliver to Junction of Adelaide Road and Agnes Gillespie Drive at Hayborough by 6:00 PM (DO NOT WAIT ON SITE)

MONDAY 22<sup>nd</sup> OCTOBER 2018

Pick up RX600 from Grenfell Street at 5:45 AM and deliver RX600 to Marlborough Street @ Henley Beach by 6:30 AM (Wait on-site) reload ready for Nightshift

Deliver RX600 to CURRIE Street between Light Square and Rosina Street by 6:45 PM (Wait on-site) reload ready for Tuesday

TUESDAY 23<sup>rd</sup> OCTOBER 2018

To be confirmed

WEDNESDAY 24<sup>th</sup> OCTOBER 2018

To be confirmed

Regards

Kym Burgess  
GENERAL MANAGER

alltranshh

From: Kym Burgess <kym.burgess@klprofiling.com.au>  
Sent: Tuesday, 16 October 2018 12:43 PM  
To: 'alltranshh@bigpond.com.au'; 'filsmitheighties@gmail.com';  
'Jakeovermeyer@gmail.com'  
Cc: 'douggolding1965@gmail.com'; 'chrisandkatiejones@hotmail.com'  
Subject: FW: FLOAT SHIFTS



UPDATED 16/10/18 at 12:30 AM

TUESDAY 16<sup>th</sup> OCTOBER 2018

Pick up RX700 from K&L Yard at 7:30 AM and deliver RX700 to Anzac Highway and Everard Avenue @ Ashford by 8:30 AM, Park in Everard (WAIT ONSITE) Reload and deliver back to K&L Depot

Wednesday 17<sup>th</sup> OCTOBER 2018

WORK CANCELLED AT THIS STAGE

Thursday 18<sup>th</sup> OCTOBER 2018

Pick up RX700 at K&L Depot at 6:00 AM and Deliver RX700 to Goldsworthy Crescent (Kibby Avenue End) by 6:45 AM (Wait on site) reload and deliver back to K&L Depot

YCI

Pick up WIRTGEN W1000F at K&L Depot at 4:30 AM and Deliver Golf Course Road overtaking lane in Renmark by 7:30 AM (Wait on site) reload and move to Hittmann Road and Halfway House reload and deliver back to K&L Depot

(GWER)

Friday 19<sup>th</sup> OCTOBER 2018

WORK CANCELLED AT THIS STAGE

Saturday 20<sup>th</sup> OCTOBER 2018

TO BE CONFIRMED

Sunday 21<sup>st</sup> OCTOBER 2018

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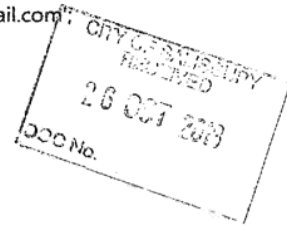
Deliver RX600 to CURRIE Street between Light Square and Rosina Street by 6:45 PM (Wait on-site) reload ready for Tuesday

TUESDAY 23<sup>rd</sup> OCTOBER 2018

Regards

alltranshh

**From:** Kym Burgess <kym.burgess@klprofiling.com.au>  
**Sent:** Tuesday, 16 October 2018 8:29 AM  
**To:** 'alltranshh@bigpond.com.au'; 'filmsitheighties@gmail.com'  
**Cc:** 'Jakeovermeyer@gmail.com'; 'douggolding1965@gmail.com'; 'chrisandkatiejones@hotmail.com'  
**Subject:** FW: FLOAT SHIFTS



**UPDATED 16/10/18 at 7:00 AM**

TUESDAY 16<sup>th</sup> OCTOBER 2018

Pick up RX700 from K&L Yard at **7:30 AM** and deliver RX700 to Anzac Highway and Everard Avenue @ Ashford by **8:30 AM**, Park in Everard (**WAIT ONSITE**) Reload and deliver back to K&L Depot

Wednesday 17<sup>th</sup> OCTOBER 2018

**WORK CANCELLED AT THIS STAGE**

Thursday 18<sup>th</sup> OCTOBER 2018

Pick up RX700 at K&L Depot at **6:00 AM** and Deliver RX700 to Goldsworthy Crescent (Kibby Avenue End) by **6:45 AM** (**Wait on site**) reload and deliver back to K&L Depot

Friday 19<sup>th</sup> OCTOBER 2018

**WORK CANCELLED AT THIS STAGE**

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**TO BE CONFIRMED**

Sunday 21<sup>st</sup> OCTOBER 2018

Pick up RX700 from K&L Depot at **4:00 PM** and deliver to Junction of Adelaide Road and Agnes Gillespie Drive at Hayborough by **6:00 PM** (**DO NOT WAIT ON SITE**)

MONDAY 22<sup>nd</sup> OCTOBER 2018

Pick up RX600 from Grenfell Street at **5:45 AM** and deliver RX700 to Marlborough Street @ Henley Beach by **6:30 AM** (**Wait on-site**) reload ready for **Nightshift**

Deliver RX600 to CURRIE Street between Light Square and Rosina Street by **6:45 PM** (**Wait on-site**) reload ready for Tuesday

TUESDAY 23<sup>rd</sup> OCTOBER 2018

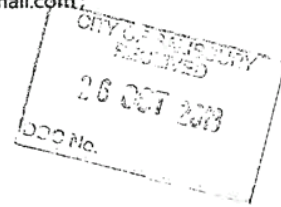
Regards

Kym Burgess  
**GENERAL MANAGER**



alltranshh

**From:** Kym Burgess <kym.burgess@klprofiling.com.au>  
**Sent:** Monday, 15 October 2018 7:53 AM  
**To:** 'alltranshh@bigpond.com.au'; 'filsmitheighties@gmail.com'  
**Cc:** 'douggolding1965@gmail.com'; 'Jakeovermeyer@gmail.com'; 'chrisandkatiejones@hotmail.com'  
**Subject:** FW: FLOAT SHIFTS



**UPDATED 15/10/18 at 7:00 AM**

MONDAY 15<sup>th</sup> OCTOBER 2018

Pick up RX 600 from K&L Depot at **6:00 AM** and deliver to **HAWKINS COURT** @ Athelstone (Wait on-site) multiple shifts, reload and leave on float for Monday nightshift

Deliver RX 600 to Location – Grenfell St. between Gawler Pl. and Pulteney St. Northern side **6:45 PM (DO NOT Wait on-site)**

TUESDAY 16<sup>th</sup> OCTOBER 2018

Pick up RX700 from K&L Yard at **7:30 AM** and deliver RX700 to Anzac Highway and Everard Avenue @ Ashford by **8:30 AM**, Park in Everard (**WAIT ONSITE**) Reload and leave on float for Wednesday

Wednesday 17<sup>th</sup> OCTOBER 2018

Deliver RX700 to Goldsworthy Crescent (Kibby Avenue End) by **6:45 AM (Wait on site)** reload and deliver back to K&L Depot

Thursday 18<sup>th</sup> OCTOBER 2018

**WORK CANCELLED AT THIS STAGE**

Friday 19<sup>th</sup> OCTOBER 2018

**WORK CANCELLED AT THIS STAGE**

Saturday 20<sup>th</sup> OCTOBER 2018

**TO BE CONFIRMED**

Sunday 21<sup>st</sup> OCTOBER 2018

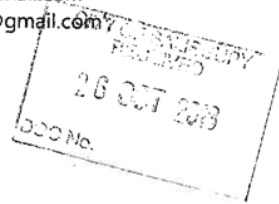
Pick up RX700 from K&L Depot at **4:00 PM** and deliver to Junction of Adelaide Road and Agnes Gillespie Drive at Hayborough by **6:00 PM (DO NOT WAIT ON SITE)**

Regards

Kym Burgess  
**GENERAL MANAGER**

alltranshh

**From:** Kym Burgess <kym.burgess@klprofiling.com.au>  
**Sent:** Friday, 12 October 2018 11:53 AM  
**To:** 'alltranshh@bigpond.com.au'; 'filmsitheighties@gmail.com'  
**Cc:** 'douggolding1965@gmail.com'; 'Jakeovermeyer@gmail.com'; 'chrisandkatiejones@hotmail.com'  
**Subject:** FW: FLOAT SHIFTS



UPDATED 12/10/18 at 11:00 AM

FRIDAY 12<sup>th</sup> OCTOBER 2018

Pick up RX600 from Rentoul Avenue @ Netherby at 11:00 AM and leave on float for Friday nightshift

*JWEEK*

Pick up RX700 from Patawalunga Frontage area at 12:00 PM LUNCHTIME reload and leave on float for SATURDAY

Deliver RX600 to Hanson Road near Uley Road @ Elizabeth Downs by 6:00 PM (WAIT ON SITE) Reload and deliver back to K&L Depot

*21962*

Saturday 13<sup>th</sup> OCTOBER 2018

Deliver RX700 to West Thebarton Road area by 4:45 AM (Wait on site) reload and deliver back to K&L YARD

*21965*

SUNDAY 14<sup>th</sup> OCTOBER 2018

Pick up 1m profiler from K&L Depot at 6:00 PM and deliver to Location – Grenfell St. and King William St. Northern side by 6:45 PM (Wait on-site) reload and deliver back to K&L Depot

*21969*

MONDAY 15<sup>th</sup> OCTOBER 2018

Pick up RX 600 from K&L Depot at 6:00 AM and deliver to HAWKINS COURT @ Athelstone (Wait on-site) multiple shifts, reload and leave on float for Monday nightshift

*21970*

Deliver RX 600 to Location – Grenfell St. between Gawler Pl. and Pulteney St. Northern side 6:45 PM (DO NOT Wait on-site)

TUESDAY 16<sup>th</sup> OCTOBER 2018

Pick up RX700 from K&L Yard at 7:30 AM and deliver RX700 to Anzac Highway and Everard Avenue @ Ashford by 8:30 AM, Park in Everard (WAIT ONSITE) Reload and leave on float for Wednesday

Wednesday 17<sup>th</sup> OCTOBER 2018

Deliver RX700 to Goldsworthy Crescent @ Glenelg North by 6:45 AM (WAIT ON-SITE) reload and the go Mayfair Street @ Maylands (Janet to Clifton St) reload and deliver to Lewis Road @ Glynde (Barnes Road to Glyndurn Road) DO NOT WAIT ON SITE

Thursday 18<sup>th</sup> OCTOBER 2018

Pick up RX700 from Lewis Road @ 12:30 PM and deliver to Wheaton Road @ Stepney, DO NOT WAIT ON SITE

Friday 19<sup>th</sup> OCTOBER 2018

alltranshh

From: Kym Burgess <kym.burgess@klprofiling.com.au>  
Sent: Friday, 12 October 2018 7:21 AM  
To: 'alltranshh@bigpond.com.au'; 'filmsitheighties@gmail.com'  
Cc: 'douggolding1965@gmail.com'; 'Jakeovermeyer@gmail.com';  
'chrisandkatiejones@hotmail.com'  
Subject: FW: FLOAT SHIFTS



UPDATED 12/10/18 at 7:00 AM

Pick up RX 00 from Rentoul Avenue @ Netherby at 11:00 AM and leave on float for Friday nightshift

Pick up RX700 from Patawalunga Frontage area at 12:00 PM LUNCHTIME reload and leave on float for SATURDAY

Deliver RX600 to Hanson Road near Uley Road @ Elizabeth Downs by 6:00 PM (WAIT ON SITE) Reload and deliver back to K&L Depot

Deliver RX700 to West Thebarton Road area by 4:45 AM reload and deliver back to K&L YARD

Pick up 1m profiler from K&L Depot at 6:00 PM and deliver to Location – Grenfell St. and King William St. Northern side by 6:45 PM reload and deliver back to K&L Depot

Pick up RX 600 from K&L Depot at 6:00 AM and deliver to HAWKINS COURT @ Athelstone (Wait on-site) multiple shifts, reload and leave on float for Monday nightshift

Deliver RX 600 to Location – Grenfell St. between Gawler Pl. and Pulteney St. Northern side 6:45 PM

Pick up RX700 from K&L Yard at 7:30 AM and deliver RX700 to Anzac Highway and Everard Avenue @ Ashford by 8:30 AM, Park in Everard Reload and leave on float for Wednesday

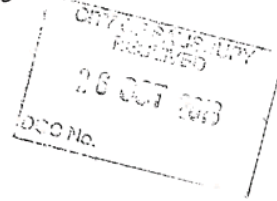
Deliver RX700 to HOLDFAST BAY AREA

NORWOOD PAYNEHAM AREA

NORWOOD PAYNEHAM AREA

alltranshh

From: Kym Burgess <kym.burgess@klprofiling.com.au>  
Sent: Thursday, 11 October 2018 7:21 AM  
To: 'alltranshh@bigpond.com.au'; 'filsmiteighties@gmail.com'  
Cc: 'douggolding1965@gmail.com'; 'Jakeovermeyer@gmail.com';  
'chrisandkatiejones@hotmail.com'  
Subject: FW: FLOAT SHIFTS



UPDATED 11/10/18 at 7:00 AM

THURSDAY 11<sup>th</sup> OCTOBER 2018

Pick up RX700 from Aldridge Terrace @ 8:00 AM and deliver to Patawalunga Frontage (Anderson Ave to Margaret Street Section) (DO NOT Wait on site)

FRIDAY 12<sup>th</sup> OCTOBER 2018

Pick up RX600 from Rentoul Avenue @ Netherby at 11:00 AM and leave on float for Friday nightshift

*Shrek*

*21960*

Pick up RX700 from Patawalunga Frontage area at 12:00 PM LUNCHTIME reload and leave on float for SATURDAY

*21961*

Deliver RX600 to Hanson Road near Uley Road @ Elizabeth Downs by 6:00 PM (WAIT ON SITE) Reload and deliver back to K&L Depot

*21962*

Saturday 13<sup>th</sup> OCTOBER 2018

Deliver RX700 to West Thebarton Road area by 4:45 AM (Wait on site) reload and deliver to Marlborough Street @ Henley Beach (DO NOT Wait on-site)

SUNDAY 14<sup>th</sup> OCTOBER 2018

Pick up 1m profiler from K&L Depot at 6:00 PM and deliver to Location – Grenfell St. and King William St. Northern side by 6:45 PM (Wait on-site) reload and deliver back to K&L Depot

MONDAY 15<sup>th</sup> OCTOBER 2018

Pick up RX 600 from K&L Depot at 6:00 AM and deliver to Tabitha Drive @ Athelstone (Wait on-site) multiple shifts, reload and leave on float for Monday nightshift

Deliver RX 600 to Location – Grenfell St. between Gawler Pl. and Pulteney St. Northern side 6:45 PM (DO NOT Wait on-site)

TUESDAY 16<sup>th</sup> OCTOBER 2018

Deliver RX700 to Mitcham Area (To be confirmed)

Wednesday 17<sup>th</sup> OCTOBER 2018

Deliver RX700 to Mitcham Area (To be confirmed)

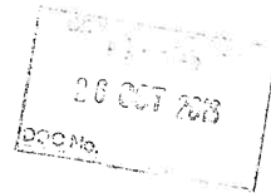
Thursday 18<sup>th</sup> OCTOBER 2018

(To be confirmed)

alltranshh

**From:** Kym Burgess <kym.burgess@klprofiling.com.au>  
**Sent:** Wednesday, 10 October 2018 8:01 AM  
**To:** 'alltranshh@bigpond.com.au'; 'filmsitheighties@gmail.com';  
'chrisandkatiejones@hotmail.com'  
**Cc:** 'douggolding1965@gmail.com'; 'douggolding1965@gmail.com'  
**Subject:** FW: FLOAT SHIFTS

**UPDATED 10/10/18 at 8:00 AM**



Wednesday 10<sup>th</sup> OCTOBER 2018

Deliver RX700 to ALDRIDGE TERRACE @ MARLESTON by 5:45 AM, (DO NOT wait on site)

THURSDAY 11<sup>th</sup> OCTOBER 2018

Pick up RX700 from Aldridge Terrace @ 8:00 AM and deliver to Patawalonga Frontage (Anderson Ave to Margaret Street Section) (DO NOT Wait on site)

FRIDAY 12<sup>th</sup> OCTOBER 2018

Pick up RX600 from Rentoul Avenue @ Netherby at 11:00 AM and leave on float for Friday nightshift

Pick up RX700 from Patawalonga Frontage area at 12:00 PM LUNCHTIME reload and leave on float for SATURDAY

Deliver RX600 to Hanson Road near Uley Road @ Elizabeth Downs by 5:00 PM (WAIT ON SITE) Reload and deliver back to K&L Depot

Saturday 13<sup>th</sup> OCTOBER 2018

Deliver RX700 to West Thebarton Road area by 4:45 AM (Wait on site) reload and deliver to Marlborough Street @ Henley Beach (DO NOT Wait on-site)

SUNDAY 14<sup>th</sup> OCTOBER 2018

Pick up 1m profiler from K&L Depot at 6:00 PM and deliver to Location – Grenfell St. and King William St. Northern side by 6:45 PM (Wait on-site) reload and deliver back to K&L Depot

MONDAY 15<sup>th</sup> OCTOBER 2018

Pick up RX700 from Marlborough Street @ Henley Beach by 12:30 PM ready for Tuesday

Pick up RX 600 from K&L Depot at 6:00 AM and deliver to Tabitha Drive @ Athelstone (Wait on-site) multiple shifts, reload and leave on float for Monday nightshift

Deliver RX 600 to Location – Grenfell St. between Gawler Pl. and Pulteney St. Northern side 6:45 PM (DO NOT Wait on-site)

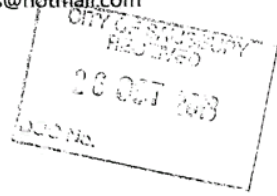
TUESDAY 16<sup>th</sup> OCTOBER 2018

Deliver RX700 to Mitcham Area (To be confirmed)

Wednesday 17<sup>th</sup> OCTOBER 2018

alltranshh@bigpond.com

From: Kym Burgess <kym.burgess@klprofiling.com.au>  
Sent: Tuesday, 9 October 2018 12:32 PM  
To: 'alltranshh@bigpond.com.au'; 'filsmitheighties@gmail.com'  
Cc: 'Jakeovermeyer@gmail.com'; 'chrisandkatiejones@hotmail.com'  
Subject: FW: FLOAT SHIFTS



UPDATED 9/10/18 at 12:30 PM

Tuesday 9<sup>th</sup> OCTOBER 2018

Pick up RX600 from K&L Depot at 6:00 AM and Deliver RX600 to Milne Road and Tonkin Street @ Vale Park by 6:45 AM (Wait on-site) reload and leave on deliver to Rentoul Avenue @ Netherby (DO NOT Wait on-site)

pick up RX700 to at TT Plaza at 10:00 PM load and LEAVE ON FLOAT FOR WEDNESDAY MORNING

Wednesday 10<sup>th</sup> OCTOBER 2018

Deliver RX700 to ALDRIDGE TERRACE @ MARLESTON by 6:45 AM, (wait on site) Reload and deliver to Patawalonga Frontage (Anderson Ave to Margaret Street Section) (DO NOT Wait on site)

THURSDAY 11<sup>th</sup> OCTOBER 2018

TO BE CONFIRMED

FRIDAY 12<sup>th</sup> OCTOBER 2018

Pick up RX600 from Rentoul Avenue @ Netherby at 11:00 AM and deliver leave on float for Friday nightshift

Pick up RX700 from Patawalonga Frontage area at 12:00 PM LUNCHTIME reload and leave on float for SATURDAY

pick up RX600 K&L Depot at 6:00 PM and deliver to Hanson Road near Uley Road @ Elizabeth Downs by 7:00 PM (WAIT ON SITE) Reload and deliver back to K&L Depot

Saturday 13<sup>th</sup> OCTOBER 2018

Deliver RX700 to West Thebarton Road area by 4:45 AM (Wait on site) reload and deliver back to K&L Depot

SUNDAY 14<sup>th</sup> OCTOBER 2018

Pick up 1m profiler from K&L Depot at 6:00 PM and deliver to Location – Grenfell St. and King William St. Northern side by 6:45 PM (Wait on-site) reload and deliver back to K&L Depot

MONDAY 15<sup>th</sup> OCTOBER 2018

Pick up RX700 from K&L Depot at 5:45 AM and Deliver RX700 to Marlborough Street @ Henley Beach by 6:45 AM (Wait on-site) reload ready for Tuesday

Pick up RX 600 from K&L Depot at 6:00 PM and deliver Location – Grenfell St. between Gawler Pl. and Pulteney St. Northern side 6:45 PM (DO NOT Wait on-site)

Regards

Kym Burgess  
**GENERAL MANAGER**



Telephone: 8360 2308  
Facsimile: 8266 9367  
Mobile: 0419 404 488  
Email: [kym.burgess@klprofiling.com.au](mailto:kym.burgess@klprofiling.com.au)  
Web site: [www.klprofiling.com.au](http://www.klprofiling.com.au)

Please consider the environment before printing this email.

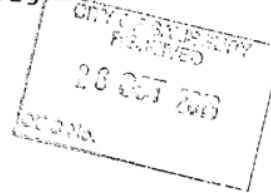
**CONFIDENTIAL COMMUNICATION**

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**Computer Viruses:** Any loss/damage incurred by using this material is not the sender's responsibility. No warranty is made that this material is free from computer virus or other defect.

alltranshh

**From:** Kym Burgess <kym.burgess@klprofiling.com.au>  
**Sent:** Tuesday, 9 October 2018 7:09 AM  
**To:** 'alltranshh@bigpond.com.au'; 'filsmitheighties@gmail.com'; 'douggolding1965@gmail.com'  
**Cc:** 'chrisandkatiejones@hotmail.com'; 'Jakeovermeyer@gmail.com'  
**Subject:** FW: FLOAT SHIFTS



UPDATED 9/10/18 at 8:00 AM

Tuesday 9<sup>th</sup> OCTOBER 2018

Pick up RX600 from K&L Depot at 6:00 AM and Deliver RX600 to Milne Road and Tonkin Street @ Vale Park by 6:45 AM (Wait on-site) reload and leave on deliver to Rentoul Avenue @ Netherby (DO NOT Wait on-site)

Pick up RX700 to at TT Plaza at 10:00 PM load and deliver back to K&L Depot

Wednesday 10<sup>th</sup> OCTOBER 2018

TO BE CONFIRMED

THURSDAY 11<sup>th</sup> OCTOBER 2018

Pick up RX700 from K&L Depot at 5:45 AM and deliver to PATAWALONGA FRONTAGE (Anderson Ave to Margaret Street Section) by 6:45 AM (DO NOT Wait on site)

FRIDAY 12<sup>th</sup> OCTOBER 2018

Pick up RX600 from Rentoul Avenue @ Netherby at 11:00 AM and deliver leave on float for Friday nightshift

Pick up RX700 from Patawalunga Frontage area at 12:00 PM LUNCHTIME reload and leave on float for Friday Nightshift

Deliver RX700 to West Thebarton Road area by 6:45 PM (Wait on site) reload and deliver back to K&L Depot

Pick up RX600 K&L Depot at 6:00 PM and deliver to Hanson Road near Uley Road @ Elizabeth Downs by 7:00 PM (WAIT ON SITE) Reload and deliver back to K&L Depot

Saturday 13<sup>th</sup> OCTOBER 2018

To be confirmed

SUNDAY 14<sup>th</sup> OCTOBER 2018

Pick up 1m profiler from K&L Depot at 6:00 PM and deliver to Location – Grenfell St. and King William St. Northern side by 6:45 PM (Wait on-site) reload and deliver back to K&L Depot

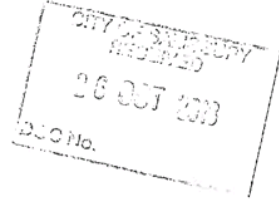
MONDAY 15<sup>th</sup> OCTOBER 2018

Pick up RX700 from K&L Depot at 5:45 AM and Deliver RX700 to Marlborough Street @ Henley Beach by 6:45 AM (Wait on-site) reload ready for Tuesday



alltranshh

From: Kym Burgess <kym.burgess@klprofiling.com.au>  
Sent: Monday, 8 October 2018 7:59 AM  
To: 'alltranshh@bigpond.com.au'; 'filmsitheighties@gmail.com'  
Cc: 'douggolding1965@gmail.com'; 'Jakeovermeyer@gmail.com';  
'chrisandkatiejones@hotmail.com'  
Subject: FW: FLOAT SHIFTS



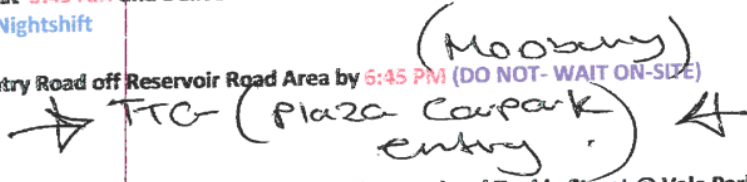
UPDATED 8/10/18 at 8:00 AM

Monday 8<sup>th</sup> OCTOBER 2018

Pick up RX700 from K&L Depot at 5:45 AM and Deliver RX700 to Marlborough Street @ Henley Beach by 5:45 AM (Wait on-site) reload ready for Nightshift

Deliver RX700 to McDonalds Entry Road off Reservoir Road Area by 6:45 PM (DO NOT- WAIT ON-SITE)

Tuesday 9<sup>th</sup> OCTOBER 2018



Pick up RX600 from K&L Depot at 6:00 AM and Deliver RX600 to Milne Road and Tonkin Street @ Vale Park by 6:45 AM (Wait on-site) reload and leave on deliver to Rentoul Avenue @ Netherby (DO NOT Wait on-site)

Pick up RX700 to at TT Plaza at 10:00 PM load and deliver back to K&L Depot

Wednesday 10<sup>th</sup> OCTOBER 2018

TO BE CONFIRMED

THURSDAY 11<sup>th</sup> OCTOBER 2018

Pick up RX600 from Rentoul Avenue @ Netherby at 11:00 AM and deliver back to K&L Depot

Pick up RX700 from K&L Depot at 6:00 AM and deliver to PATAWALONGA FRONTAGE (Anderson Ave to Margaret Street Section) by 6:45 AM (DO NOT Wait on site)

FRIDAY 12<sup>th</sup> OCTOBER 2018

Pick up RX700 from Patawalunga Frontage area at 12:00 PM LUNCHTIME reload and leave on float for Friday Nightshift

Deliver RX700 to West Thebarton Road area by 6:45 PM (Wait on site) reload and deliver back to K&L Depot

Pick up RX600 K&L Depot at 6:00 PM and deliver to Hanson Road near Uley Road @ Elizabeth Downs by 7:00 PM (WAIT ON SITE) Reload and deliver back to K&L Depot

Saturday 13<sup>th</sup> OCTOBER 2018

To be confirmed

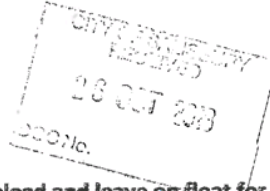
SUNDAY 14<sup>th</sup> OCTOBER 2018

Item 5.1.4 - Attachment 2 - Vehicle Log Details provided by Applicant

alltranshh

**From:** Kym Burgess <kym.burgess@klprofiling.com.au>  
**Sent:** Friday, 5 October 2018 7:45 AM  
**To:** 'alltranshh@bigpond.com.au'; 'filsmitheighties@gmail.com'  
**Cc:** 'douggolding1965@gmail.com'; 'Jakeovermeyer@gmail.com'  
**Subject:** FW: FLOAT SHIFTS

**UPDATED 5/10/18 at 8:00 AM**  
 Friday 5<sup>th</sup> OCTOBER 2018



Deliver RX600 to Adelaide Road @ Murray Bridge by **6:45 AM (WAIT ON SITE)** reload and leave on float for Friday Night

Deliver RX600 to Semaphore Road and Jagoe Street by **9:00 PM (WAIT ON SITE)** Reload and leave on float for Saturday

**1. JASON ROAD PROJECT DEFERRED TILL NEXT FRIDAY**

Saturday 6<sup>th</sup> OCTOBER 2018

Deliver RX600 to Berth 6 by **6:00AM (WAIT ON SITE)** reload and leave on float for Saturday NIGHTSHIFT

Deliver RX600 to Oaklands Road Rail crossing by **6:45 PM (WAIT ON SITE)** reload and deliver back to K&L Depot

Sunday 7<sup>th</sup> OCTOBER 2018

**NO WORK**

Monday 8<sup>th</sup> OCTOBER 2018

Pick up RX700 from K&L Depot at **5:45 AM** and Deliver RX700 to Marlborough Street @ Henley Beach by **6:45 AM (Wait on-site)** reload ready for Nightshift

Deliver RX700 to McDonalds Entry Road off Reservoir Road Area by **6:45 PM (DO- WAIT ON-SITE ON SITE)**

Tuesday 9<sup>th</sup> OCTOBER 2018

Pick up RX600 from K&L Depot at **5:45 AM** and Deliver RX600 to Holdfast Bay Area **6:45 AM (Wait on-site)** reload and leave on float for **WEDNESDAY**

Pick up RX700 to at TT Plaza at **10:00 PM** load and deliver back to K&L Depot

Wednesday 10<sup>th</sup> OCTOBER 2018

Deliver RX600 to Rentoul Avenue @ Netherby by **6:30 AM (DO NOT Wait on-site)**

**THURSDAY 10<sup>th</sup> OCTOBER 2018**

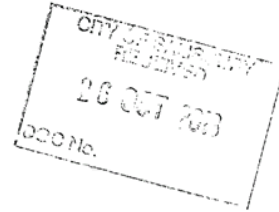
Pick up RX600 from Rentoul Avenue @ Netherby at **11:00 AM** and deliver back to K&L Depot

Pick up RX700 from K&L Depot at **6:00 AM** and deliver to Holdfast Bay area by **6:45 AM (Wait on site)** reload and leave on float for Friday

alltranshh

**From:** Kym Burgess <kym.burgess@klprofiling.com.au>  
**Sent:** Thursday, 4 October 2018 8:10 AM  
**To:** 'alltranshh@bigpond.com.au'; 'filsmitheighties@gmail.com'  
**Cc:** 'douggolding1965@gmail.com'; 'chrisandkatiejones@hotmail.com'  
**Subject:** FW: FLOAT SHIFTS

**UPDATED 4/10/18 at 8:00 AM**



Thursday 4<sup>th</sup> OCTOBER 2018

Deliver RX700 to Douglas Grove at Glenelg North by **6:45 AM (WAIT ON SITE)** reload and deliver back to K&L Depot

Deliver RX600 to St Andrews Crescent at Novar Gardens by **6:45 AM (WAIT ON SITE)** reload and leave on float for Friday

Friday 5<sup>th</sup> OCTOBER 2018

Deliver RX600 to Adelaide Road @ Murray Bridge by **6:45 AM (WAIT ON SITE)** reload and leave on float for Friday Night

Deliver RX600 to Semaphore Road and Jagoe Street by **9:00 PM (WAIT ON SITE)** Reload and deliver back to K&L Depot

Pick up RX700 K&L Depot at **6:00 PM** and deliver to Hanson Road near Uley Road @ Elizabeth Downs by **7:00 PM (WAIT ON SITE)** Reload and leave on float for Saturday

Saturday 6<sup>th</sup> OCTOBER 2018

Deliver RX700 to Berth 6 by **5:00AM (WAIT ON SITE)** reload and leave on float for Saturday NIGHTSHIFT

Deliver RX700 to Oaklands Road Rail crossing by **6:45 PM (WAIT ON SITE)** reload and leave on float for Monday

Sunday 7<sup>th</sup> OCTOBER 2018

NO WORK

Monday 8<sup>th</sup> OCTOBER 2018

Deliver RX700 to Marlborough Street @ Henley Beach by **6:30 AM (Wait on-site)** reload ready for Nightshift

Deliver RX700 to Modbury Area by **7:00 PM (WAIT ON-SITE)** reload and leave on float for Tuesday

Tuesday 9<sup>th</sup> OCTOBER 2018

Deliver RX700 to Holdfast Bay Area **6:30 AM (Wait on-site)** reload and leave on float for Nightshift

Deliver RX700 to Modbury Area by **7:00 PM (WAIT ON-SITE)** reload and leave on float for Tuesday

Wednesday 10<sup>th</sup> OCTOBER 2018

Deliver RX700 to Holdfast Bay Area **6:30 AM (Wait on-site)** reload and leave on float for THURSDAY

K e L

alltranshh

**From:** Lawrence Parry <Lawrence.Parry@klprofiling.com.au>  
**Sent:** Friday, 10 August 2018 9:45 AM  
**To:** Alltrans  
**Cc:** Kym Burgess; chrisandkatiejones@hotmail.com; filsmighties@gmail.com  
**Subject:** float shifts

Hey Mate

Sunday 12<sup>th</sup> august

P/up 1metre at 6am and deliver to Woodville rd Port rd intersection. Traffic on ch31. Wait onsite multiple float shifts and deliver back to yard.

21667  
26 OCT 2018  
10:01 AM  
DUPLO

P/up rx700 at 8am and deliver to ridersafe Smart rd St Agnes wait onsite and return to yard.

21668

^^ this stage we have 1 job Friday the 17<sup>th</sup> at night, and 2 jobs Saturday the 18<sup>th</sup>

Regards Phil.

Lawrence Parry  
**DIRECTOR / OPERATIONS MANAGER**



Telephone (08) 8360 2309  
Fax (08) 8266 9367  
Mobile 0419 444 905  
Email [lawrence.parry@klprofiling.com.au](mailto:lawrence.parry@klprofiling.com.au)

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alltranshh

**From:** Kym Burgess <kym.burgess@klprofiling.com.au>  
**Sent:** Friday, 27 July 2018 10:16 AM  
**To:** 'alltranshh@bigpond.com.au'; 'filmsitheighties@gmail.com'  
**Cc:** 'Jakeovermeyer@gmail.com'; 'chrisandkatiejones@hotmail.com'  
**Subject:** FW: FLOAT SHIFTS

UPDATED 27/07/18 at 9:30 AM



SATURDAY 28<sup>th</sup> JULY 2018

NO WORK

SUNDAY 29<sup>th</sup> JULY 2018

NO WORK

MONDAY 30<sup>th</sup> JULY 2018

DO 21603

Pick up RX 700 from K&L Depot at 6:00 AM and Deliver RX700 to Ayr Avenue @ Torrens Park by 6:45 AM (Wait on-site) reload and leave on float for Tuesday

TUESDAY 31<sup>st</sup> JULY 2018

Deliver RX700 to Blenheim Street (off Grote Street) in Adelaide City by 6:45 AM (Wait on-site) reload and deliver back to K & L Profiling yard

WEDNESDAY 1<sup>st</sup> AUGUST 2018

TO BE CONFIRMED

THURSDAY 2<sup>nd</sup> AUGUST 2018

TO BE CONFIRMED

FRIDAY 3<sup>rd</sup> AUGUST 2018

Pick up RX 700 from K&L Depot at 8:00 PM and Deliver RX700 to King Road Park & Ride Carpark by 8:45 PM (Wait on-site) reload and deliver back to K & L Depot

Regards

Kym Burgess  
**GENERAL MANAGER**



Telephone: 8360 2308

alltranshh

**From:** Kym Burgess <kym.burgess@klprofiling.com.au>  
**Sent:** Wednesday, 25 July 2018 10:42 AM  
**To:** 'alltranshh@bigpond.com.au'; 'filmsitheighties@gmail.com'  
**Cc:** 'Jakeovermeyer@gmail.com'; 'chrisandkatiejones@hotmail.com'  
**Subject:** FW: FLOAT SHIFTS

**UPDATED 25/07/18 at 7:30 AM**

WEDNESDAY 25th JULY 2018

Work Cancelled due to wet weather

THURSDAY 26th JULY 2018

Work Cancelled due to wet weather

FRIDAY 27th JULY 2018

TO BE CONFIRMED

SATURDAY 28th JULY 2018

TO BE CONFIRMED

SUNDAY 29th JULY 2018

TO BE CONFIRMED

MONDAY 30th JULY 2018

Pick up RX 700 from K&L Depot at **6:00 AM** and Deliver RX700 to Ayr Avenue @ Torrens Park by **6:45 AM** (Wait on-site) reload and leave on float for Tuesday

TUESDAY 31st JULY 2018

Deliver RX700 to Blenheim Street (off Grote Street) in Adelaide City by **6:45 AM** (Wait on-site) reload and leave on float for Wednesday

WEDNESDAY 1st AUGUST 2018

Deliver RX700 to Day Terrace @ West Croydon by **6:45 AM** (Wait on-site) reload and leave on float for Wednesday

Regards

Kym Burgess  
**GENERAL MANAGER**



alltranshh

**From:** Kym Burgess <kym.burgess@klprofiling.com.au>  
**Sent:** Tuesday, 24 July 2018 7:56 AM  
**To:** 'alltranshh@bigpond.com.au'; 'filmsitheighties@gmail.com'  
**Cc:** 'douggolding1965@gmail.com'; 'Jakeovermeyer@gmail.com';  
 'chrisandkatiejones@hotmail.com'  
**Subject:** FW: FLOAT SHIFTS



UPDATED 24/07/18 at 7:30 AM

TUESDAY 24<sup>th</sup> JULY 2018

Deliver RX700 to George Street @ Thebarton by 6:45 AM (WAIT ON SITE) Reload and leave on float for Wednesday

WEDNESDAY 25<sup>th</sup> JULY 2018

Deliver RX700 to Ayr Avenue @ Torrens Park by 6:45 AM (Wait on-site) reload and deliver back to K&L Depot  
*cancel 21597 on*

THURSDAY 26<sup>th</sup> JULY 2018

TO BE CONFIRMED

FRIDAY 27<sup>th</sup> JULY 2018

TO BE CONFIRMED

SATURDAY 28<sup>th</sup> JULY 2018

TO BE CONFIRMED

SUNDAY 29<sup>th</sup> JULY 2018

TO BE CONFIRMED

MONDAY 30<sup>th</sup> JULY 2018

TO BE CONFIRMED

Regards

Kym Burgess  
**GENERAL MANAGER**

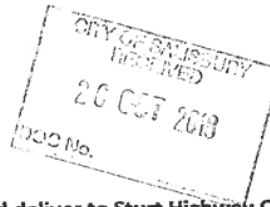


Telephone: 8360 2308  
 Facsimile: 8266 9367  
 Mobile: 0419 404 488  
 Email: [kym.burgess@klprofiling.com.au](mailto:kym.burgess@klprofiling.com.au)

alitranshh

**From:** Kym Burgess <kym.burgess@klprofiling.com.au>  
**Sent:** Thursday, 19 July 2018 10:34 AM  
**To:** 'alitranshh@bigpond.com.au'; 'filsmiteighties@gmail.com'  
**Cc:** 'douggolding1965@gmail.com'; 'Jakeovermeyer@gmail.com'; 'chrisandkatiejones@hotmail.com'  
**Subject:** FW: FLOAT SHIFTS

**UPDATED 019/07/18 at 10:30 AM**



FRIDAY 20<sup>th</sup> JULY 2018

Pick up Wirtgen W1000F 1.0 metre profiler from K&L Depot at **4:00 AM** and deliver to Sturt Highway Overtaking lane @ Lyrup by **7:30 AM (WAIT ON-SITE)** reload then go to Golf Course Road overtaking lane at Renmark **(WAIT ON-SITE)** and deliver back to K&L Depot

SATURDAY 21<sup>st</sup> JULY 2018

No Work

SUNDAY 22<sup>nd</sup> JULY 2018

No Work

MONDAY 23<sup>rd</sup> JULY 2018

Pick up RX600 from K&L Yard at **6:00 AM** and deliver to ninth avenue Royston Park by **6:45 AM (Do Not wait on site)**  
- pick up after RX700 float shift

Pick up RX700 from George Street @ Thebarton at **12:00 PM (Lunchtime)** and deliver back to K&L Depot

TUESDAY 24<sup>th</sup> JULY 2018

TO BE CONFIRMED

WEDNESDAY 25<sup>th</sup> JULY 2018

Pick up RX700 from K&L Depot at **6:00 AM** and deliver to Ayr Avenue @ Torrens Park by **6:45 AM (Wait on-site)** reload and deliver back to K&L Depot

THURSDAY 26<sup>th</sup> JULY 2018

TO BE CONFIRMED

FRIDAY 27<sup>th</sup> JULY 2018

TO BE CONFIRMED

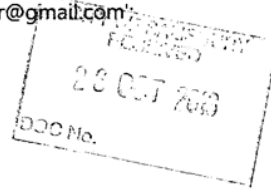
Regards

Kym Burgess  
**GENERAL MANAGER**



**alltranshh@bigpond.com**

**From:** Kym Burgess <kym.burgess@klprofiling.com.au>  
**Sent:** Thursday, 19 July 2018 6:21 AM  
**To:** 'alltranshh@bigpond.com.au'; 'filsmiteighties@gmail.com'  
**Cc:** 'douggolding1965@gmail.com'; 'Jakeovermeyer@gmail.com';  
'chrisandkatiejones@hotmail.com'  
**Subject:** FW: FLOAT SHIFTS



**UPDATED 019/07/18 at 7:00 AM**

**FRIDAY 20<sup>th</sup> JULY 2018**

Pick up Wirtgen W1000F 1.0 metre profiler from K&L Depot at **4:00 AM** and deliver to Sturt Highway Overtaking Lane @ Lyrup by **7:30 AM (WAIT ON-SITE)** reload then go to Golf Course Road overtaking lane at Renmark (**WAIT ON-SITE**) and deliver back to K&L Depot

**SATURDAY 21<sup>st</sup> JULY 2018**

No Work

**SUNDAY 22<sup>nd</sup> JULY 2018**

No Work

**MONDAY 23<sup>rd</sup> JULY 2018**

Pick up RX700 from George Street @ Thebarton at **12:00 PM (Lunchtime)** and deliver back to K&L Depot

**TUESDAY 24<sup>th</sup> JULY 2018**

TO BE CONFIRMED

**WEDNESDAY 25<sup>th</sup> JULY 2018**

Pick up RX700 from K&L Depot at **6:00 AM** and deliver to Ayr Avenue @ Torrens Park by **6:45 AM (Wait on-site)** reload and deliver back to K&L Depot

**THURSDAY 26<sup>th</sup> JULY 2018**

TO BE CONFIRMED

**FRIDAY 27<sup>th</sup> JULY 2018**

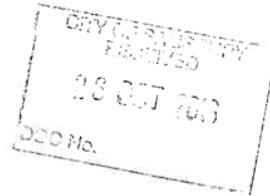
TO BE CONFIRMED

Regards

Kym Burgess  
**GENERAL MANAGER**

alltranshh

**From:** Kym Burgess <kym.burgess@klprofiling.com.au>  
**Sent:** Wednesday, 18 July 2018 11:31 AM  
**To:** 'alltranshh@bigpond.com.au'; 'filmsitheighties@gmail.com'  
**Cc:** 'douggolding1965@gmail.com'; 'Jakeovermeyer@gmail.com';  
 'chrisandkatiejones@hotmail.com'  
**Subject:** FW: FLOAT SHIFTS



UPDATED 018/07/18 at 11:00 AM

THURSDAY 19<sup>th</sup> JULY 2018

NO WORK – RAIN 20mm

FRIDAY 20<sup>th</sup> JULY 2018

Pick up Wirtgen W1000F 1.0 metre profiler from K&L Depot at **4:00 AM** and deliver to Sturt Highway Overtaking lane @ Lyrup by **7:30 AM (WAIT ON-SITE)** reload then go to Golf Course Road overtaking lane at Renmark (**WAIT ON-SITE**) and deliver back to K&L Depot

SATURDAY 21<sup>st</sup> JULY 2018

No Work

SUNDAY 22<sup>nd</sup> JULY 2018

No Work

MONDAY 23<sup>rd</sup> JULY 2018

Pick up RX600 from K&L Depot at **6:00 AM** and deliver to Ayr Avenue @ Torrens Park by **6:45 AM (Wait on-site)** reload and deliver back to K&L Depot

TUESDAY 24<sup>th</sup> JULY 2018

Pick up RX700 from George Street @ Thebarton at **12:00 PM (Lunchtime)** and deliver back to K&L Depot

Regards

Kym Burgess  
**GENERAL MANAGER**



Telephone: **8360 2308**  
 Facsimile: **8266 9367**  
 Mobile: **0419 404 488**

**alltranshh**

**From:** Kym Burgess <kym.burgess@klprofiling.com.au>  
**Sent:** Tuesday, 17 July 2018 6:50 AM  
**To:** 'alltranshh@bigpond.com.au'; 'filsmitheighties@gmail.com'  
**Cc:** 'douggolding1965@gmail.com'; 'Jakeovermeyer@gmail.com'; 'chrisandkatiejones@hotmail.com'  
**Subject:** FW: FLOAT SHIFTS

**UPDATED 017/07/18 at 6:00 AM**



TUESDAY 17<sup>th</sup> JULY 2018

No work

WEDNESDAY 18<sup>th</sup> JULY 2018

Pick up RX700 from K&L Depot at **9:00 AM** and deliver to KINTORE Street @ Thebarton (Dew Street End) **(DO NOT WAIT ON SITE)**

THURSDAY 19<sup>th</sup> JULY 2018

TO BE CONFIRMED

FRIDAY 20<sup>th</sup> JULY 2018

TO BE CONFIRMED

SATURDAY 21<sup>st</sup> JULY 2018

No Work

SUNDAY 22<sup>nd</sup> JULY 2018

No Work

MONDAY 23<sup>rd</sup> JULY 2018

Pick up RX600 from K&L Depot at **5:00 AM** and deliver to Ayr Avenue @ Torrens Park by **6:45 AM** (Wait on-site) reload and deliver back to K&L Depot

Pick up RX700 from George Street @ Thebarton at **12:00 PM** (Lunchtime) and deliver back to K&L Depot

Regards

Kym Burgess  
**GENERAL MANAGER**

alltranshh@bigpond.com

**From:** Kym Burgess <kym.burgess@klprofiling.com.au>  
**Sent:** Monday, 16 July 2018 8:08 AM  
**To:** 'alltranshh@bigpond.com.au'; 'filsmitheighties@gmail.com'  
**Cc:** 'douggolding1965@gmail.com'; 'Jakeovermeyer@gmail.com';  
 'chrisandkatiejones@hotmail.com'  
**Subject:** FW: FLOAT SHIFTS



UPDATED 016/07/18 at 8:00 AM

MONDAY 16<sup>th</sup> JULY 2018

1 DAY BOOKING NOW MOVED TO WEDNESDAY

TUESDAY 17<sup>th</sup> JULY 2018

TO BE CONFIRMED

WEDNESDAY 18<sup>th</sup> JULY 2018

Pick up WIRTGEN W1000F 1 metre profiler from K&L Depot at 6:30 AM and deliver to Fulham Gardens Shopping Centre @ Fulham Gardens by 7:30 AM entrance off Tapleys Hill Road just before McDonalds (Wait on-site) reload and deliver back to K&L Depot

THURSDAY 19<sup>th</sup> JULY 2018

Pick up RX700 from K&L Depot at 6:00 AM and deliver to George Street @ Thebarton (Dew Street End) by 6:45 AM (DO NOT WAIT ON SITE)

Pick up RX600 from K&L Depot at 5:00 AM and deliver to Parafield Park and Ride @ PARAFIELD – UNLOAD IN SIP LANE ON KINGS ROAD by 6:00 AM (DO NOT WAIT ON SITE)

FRIDAY 20<sup>th</sup> JULY 2018

Pick up RX600 from Parafield Park and Ride @ PARAFIELD at 1:00 PM and deliver back to K&L Depot

SATURDAY 21<sup>st</sup> JULY 2018

No Work

SUNDAY 22<sup>nd</sup> JULY 2018

No Work

MONDAY 23<sup>rd</sup> JULY 2018

Pick up RX600 from K&L Depot at 6:00 AM and deliver to Ayr Avenue @ Torrens Park by 6:45 AM (Wait on-site) reload and deliver back to K&L Depot

Pick up RX700 from George Street @ Thebarton at 12:00 PM (Lunchtime) and deliver back to K&L Depot

**alltranshh**

**From:** Kym Burgess <kym.burgess@klprofiling.com.au>  
**Sent:** Friday, 13 July 2018 12:00 PM  
**To:** 'alltranshh@bigpond.com.au'; 'filsmitheighties@gmail.com'  
**Cc:** 'douggolding1965@gmail.com'; 'Jakeovermeyer@gmail.com'; 'chrisandkatiejones@hotmail.com'  
**Subject:** FW: FLOAT SHIFTS



**UPDATED 013/07/18 at 12:00 PM**

Friday 13<sup>th</sup> JULY 2018

Deliver RX700 to Birmingham Street at Mile End (OFF SOTH ROAD) by **6:45 AM (DO NOT WAIT ON-SITE)**

Pi p RX 700 from Birmingham Street at Mile End (OFF SOUTH ROAD) AT **2:00 pm** and deliver back to K&L Yard

SATURDAY 14<sup>th</sup> July 2018

**NO WORK**

SUNDAY 15<sup>th</sup> JULY 2018

**NO WORK**

MONDAY 16<sup>th</sup> JULY 2018

**MONDAY BOOKING NOW MOVED TO WEDNESDAY**

TUESDAY 17<sup>th</sup> JULY 2018

**TO BE CONFIRMED**

WEDNESDAY 18<sup>th</sup> JULY 2018

Pick up WIRTGEN W1000F 1 metre profiler from K&L Depot at **6:30 AM** and deliver to Fulham Gardens Shopping Centre @ Fulham Gardens by **7:30 AM** entrance off Tapleys Hill Road just before McDonalds (Wait on-site) reload and deliver back to K&L Depot

THURSDAY 19<sup>th</sup> JULY 2018

Pick up RX700 from K&L Depot at **6:00 AM** and deliver to George Street @ Thebarton (Dew Street End) by **6:45 AM (DO NOT WAIT ON SITE)**

Pick up RX600 from K&L Depot at **5:00 AM** and deliver to Parafield Park and Ride @ PARAFIELD – UNLOAD IN SIP LANE ON KINGS ROAD by **6:00 AM (DO NOT WAIT ON SITE)**

FRIDAY 20<sup>th</sup> JULY 2018

Pick up RX600 from Parafield Park and Ride @ PARAFIELD at **1:00 PM** and deliver back to K&L Depot

SATURDAY 21<sup>st</sup> JULY 2018

alltranshh

**From:** Kym Burgess <kym.burgess@klprofiling.com.au>  
**Sent:** Friday, 13 July 2018 8:14 AM  
**To:** 'alltranshh@bigpond.com.au'; 'filsmiteighties@gmail.com'  
**Cc:** 'douggolding1965@gmail.com'; 'chrisandkatiejones@hotmail.com';  
 'Jakeovermeyer@gmail.com'  
**Subject:** FW: FLOAT SHIFTS



UPDATED 013/07/18 at 8:00 AM

Friday 13<sup>th</sup> JULY 2018

Deliver RX700 to Birmingham Street at Mile End (OFF SOTH ROAD) by 5:45 AM (DO NOT WAIT ON-SITE)

Pick up RX 700 from Birmingham Street at Mile End (OFF SOUTH ROAD) AT 2:00 pm and deliver back to K&L Yard

Saturday 14<sup>th</sup> July 2018

NO WORK

SUNDAY 15<sup>th</sup> JULY 2018

NO WORK

MONDAY 16<sup>th</sup> JULY 2018

Pick up WIRTGEN W000F 1 metre profiler from K&L Depot at 6:30 AM and deliver to Fulham Gardens Shopping Centre @ Fulham Gardens by 7:30 AM entrance off Tapleys Hill Road just before McDonalds (Wait on-site) reload and deliver back to K&L Depot

TUESDAY 17<sup>th</sup> JULY 2018

TO BE CONFIRMED

WEDNESDAY 18<sup>th</sup> JULY 2018

TO BE CONFIRMED

THURSDAY 19<sup>th</sup> JULY 2018

Pick up RX700 from K&L Depot at 6:00 AM and deliver to George Street @ Thebarton (Dew Street End) by 6:45 AM (DO NOT WAIT ON SITE)

Pick up RX600 from K&L Depot at 5:00 AM and deliver to Parafield Park and Ride @ PARAFIELD – UNLOAD IN SIP LANE ON KINGS ROAD by 6:00 AM (DO NOT WAIT ON SITE)

FRIDAY 20<sup>th</sup> JULY 2018

Pick up RX600 from Parafield Park and Ride @ PARAFIELD at 1:00 PM and deliver back to K&L Depot

SATURDAY 21<sup>st</sup> JULY 2018

SEM – George Street Thebarton

**alltranshh@bigpond.com**

**From:** Kym Burgess <kym.burgess@klprofiling.com.au>  
**Sent:** Thursday, 12 July 2018 7:32 AM  
**To:** 'alltranshh@bigpond.com.au'; 'filmsitheighties@gmail.com';  
 'Jakeovermeyer@gmail.com'  
**Cc:** 'dougplding1965@gmail.com'; 'chrisandkatiejones@hotmail.com'  
**Subject:** FW: FLOAT SHIFTS



**UPDATED 012/07/18 at 7:00 AM**

THURSDAY 12<sup>th</sup> JULY 2018

Pick up **RX700** from K&L Depot at **5:30 PM** and deliver to Oakland's Road Rail Project by **6:45PM (WAIT N SITE)**

Pick up Wirtgen W1000F for K&L DEPOT at **4:00 AM** and Deliver WIRTGEN W1000F to Hittmann Road/Sturt Highway overtaking lane by near Banrock Station Wines/ Wrigley Flat Road by **6:45 AM (Wait on site)** reload and deliver back to K&L Depot (**MAY STAY OVERNIGHT TO BE CONFIRMED**)

Friday 13<sup>th</sup> JULY 2018

Deliver RX700 to Birmingham Street at Mile End (OFF SOTH ROAD) by **6:45 AM (DO NOT WAIT ON-SITE)**

Pick up RX 700 from Birmingham Street at Mile End (OFF SOUTH ROAD) AT **2:00 pm** and deliver back to K&L Yard

SATURDAY 14<sup>th</sup> July 2018

**TO BE CONFIRMED**

SUNDAY 15<sup>th</sup> JULY 2018

**TO BE CONFIRMED**

MONDAY 16<sup>th</sup> JULY 2018

**THIS PROJECT NOW MOVED TO THE 23<sup>rd</sup> JULY** - Pick up RX700 from K&L Depot at **6:00 AM** and deliver to Ayr Avenue @ Torrens Park by **6:45 AM (Wait on-site)** reload and leave on float for Friday (**THIS JOB WAS ORIGINALLY ON THURSDAY 12/07**)

TUESDAY 17<sup>th</sup> JULY 2018

**BMD - Parafield Park & Ride**

WEDNESDAY 18<sup>th</sup> JULY 2018

**BMD - Parafield Park & Ride**

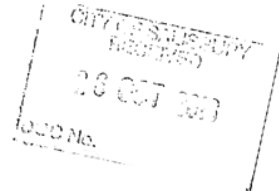
THURSDAY 19<sup>th</sup> JULY 2018

**SEM - George Street Thebarton**

FRIDAY 20<sup>th</sup> JULY 2018

alltranshh

**From:** Kym Burgess <kym.burgess@klprofiling.com.au>  
**Sent:** Wednesday, 11 July 2018 9:10 AM  
**To:** 'alltranshh@bigpond.com.au'; 'filmsitheighties@gmail.com'  
**Cc:** 'Jakeovermeyer@gmail.com'; 'chrisandkatiejones@hotmail.com'; 'douggolding1965@gmail.com'  
**Subject:** FW: FLOAT SHIFTS



UPDATED 011/07/18 at 9:00 AM

WEDNESDAY 11<sup>th</sup> JULY 2018

PROJECT NOW MOVED TO THURSDAY NIGHT – NO SHIFTS REQUIRED TODAY

THURSDAY 12<sup>th</sup> JULY 2018

Pick up RX600 from K&L Depot at **4:00 PM** and deliver to <sup>DO 21546</sup> Oakland's Road Rail Project by **5:30 PM** (DO NOT Wait on-site)

Pick up Wirtgen W1000F for K&L DEPOT at **4:00 AM** and Deliver WIRTGEN W1000F to Hittmann Road/Sturt Highway overtaking lane by near Banrock Station Wines/ Wrigley Flat Road by **6:45 AM** (Wait on site) reload and deliver back to K&L Depot (MAY STAY OVERNIGHT TO BE CONFIRMED)

Friday 13<sup>th</sup> JULY 2018

Deliver RX700 to Birmingham Street at Mile End (OFF SOTH ROAD) by **6:45 AM** (DO NOT WAIT ON-SITE)

Pick up RX 700 from Birmingham Street at Mile End (OFF SOUTH ROAD) AT **2:00 pm** and deliver back to K&L Yard

Pick up RX600 from Oaklands Road at **11:00 PM** and deliver back to K&L Depot

SATURDAY 14<sup>th</sup> July 2018

TO BE CONFIRMED

SUNDAY 15<sup>th</sup> JULY 2018

TO BE CONFIRMED

MONDAY 16<sup>th</sup> JULY 2018

Pick up RX700 from K&L Depot at **6:00 AM** and deliver to Ayr Avenue @ Torrens Park by **6:45 AM** (Wait on-site) reload and leave on float for Friday (THIS JOB WAS ORIGINALLY ON THURSDAY 12/07)

TUESDAY 17<sup>th</sup> JULY 2018

BMD – Parafield Park & Ride

WEDNESDAY 18<sup>th</sup> JULY 2018

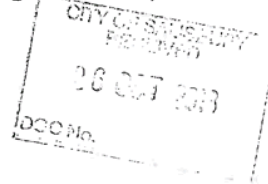
BMD – Parafield Park & Ride

THURSDAY 19<sup>th</sup> JULY 2018



alltranshh

**From:** Kym Burgess <kym.burgess@klprofiling.com.au>  
**Sent:** Tuesday, 10 July 2018 10:43 AM  
**To:** 'alltranshh@bigpond.com.au'; 'filmsitheighties@gmail.com'  
**Cc:** 'douggolding1965@gmail.com'; 'chrisandkatiejones@hotmail.com';  
 'Jakeovermeyer@gmail.com'  
**Subject:** FW: FLOAT SHIFTS



UPDATED 010/07/18 at 10:00 AM

TUESDAY 10<sup>th</sup> JULY 2018

PROJECT NOW MOVED TO THURSDAY NIGHT – NO SHIFTS REQUIRED TODAY

WEDNESDAY 11<sup>th</sup> JULY 2018

PROJECT NOW MOVED TO THURSDAY NIGHT – NO SHIFTS REQUIRED TODAY

THURSDAY 12<sup>th</sup> JULY 2018

Pick up RX600 from K&L Depot at 4:00 PM and deliver to Oakland's Road Rail Project by 5:30 PM (DO NOT Wait on-site) 2546

Pick up RX700 from K&L Depot at 6:00 AM and deliver to Ayr Avenue @ Torrens Park by 6:45 AM (Wait on-site) reload and leave on float for Friday

Pick up Wirtgen W1000F for K&L DEPOT at 4:00 AM and Deliver WIRTGEN W1000F to Hittmann Road/Sturt Highway overtaking lane by near Banrock Station Wines/ Wrigley Flat Road by 6:45 AM (Wait on site) reload and deliver back to K&L Depot (MAY STAY OVERNIGHT TO BE CONFIRMED)

Friday 13<sup>th</sup> JULY 2018

Deliver RX700 to Birmingham Street at Mile End (OFF SOTH ROAD) by 6:45 AM (DO NOT WAIT ON-SITE)

Pick up RX 700 from Birmingham Street at Mile End (OFF SOTH ROAD) AT 2:00 pm and deliver back to K&L Yard

Pick up RX600 from Oaklands Road at 11:00 PM and deliver back to K&L Depot

SATURDAY 14<sup>th</sup> July 2018

TO BE CONFIRMED

SUNDAY 15<sup>th</sup> JULY 2018

TO BE CONFIRMED

MONDAY 16<sup>th</sup> JULY 2018

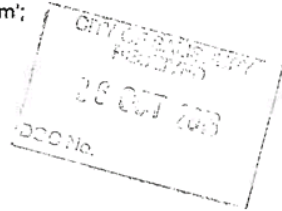
TO BE CONFIRMED

TUESDAY 17<sup>th</sup> JULY 2018

BMD – Parafield Park & Ride

alltranshh

**From:** Kym Burgess <kym.burgess@klprofiling.com.au>  
**Sent:** Monday, 9 July 2018 11:23 AM  
**To:** 'alltranshh@bigpond.com.au'; 'chrisandkatiejones@hotmail.com'  
**Cc:** 'filsmitheighties@gmail.com'; 'douggolding1965@gmail.com'; 'Jakeovermeyer@gmail.com'  
**Subject:** FW: FLOAT SHIFTS



UPDATED 09/07/18 at 11:30 AM

Pick up RX700 from K&L Depot at 6:00 AM and deliver to Elizabeth Street @ Norwood (wait on-site) reload and deliver back to K&L Profiling yard

TUESDAY 10<sup>th</sup> JULY 2018

PROJECT NOW MOVED TO THURSDAY NIGHT – NO SHIFTS REQUIRED TODAY

WEDNESDAY 11<sup>th</sup> JULY 2018

Pick up RX600 from K&L Depot at 5:30 PM and deliver to Oakland's Road Rail Project by 6:45 PM (DO NOT Wait on-

DO 21546

THURSDAY 12<sup>th</sup> JULY 2018

Pick up RX600 from K&L Depot at 5:30 PM and deliver to Oakland's Road Rail Project by 6:45 PM (DO NOT Wait on-

Pick up RX700 from K&L Depot at 6:00 AM and deliver to Ayr Avenue @ Torrens Park by 6:45 AM (Wait on-site) reload and leave on float for Friday

Pick up Wirtgen W1000F for K&L DEPOT at 4:00 AM and Deliver WIRTGEN W1000F to Hittmann Road/Sturt Highway taking lane by near Banrock Station Wines/ Wrigley Flat Road by 6:45 AM (wait on site) reload and deliver back to K&L Depot (MAY STAY OVERNIGHT TO BE CONFIRMED)

Friday 13<sup>th</sup> JULY 2018

Deliver RX700 to Birmingham Street at Mile End (OFF SOTH ROAD) by 6:45 AM (DO NOT WAIT ON-SITE)

Pick up RX 700 from Birmingham Street at Mile End (OFF SOTH ROAD) AT 2:00 pm and deliver back to K&L Yard

Pick up RX600 from Oaklands Road at 11:00 PM and deliver back to K&L Depot

SATURDAY 14<sup>th</sup> July 2018

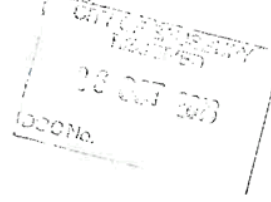
TO BE CONFIRMED

SUNDAY 15<sup>th</sup> JULY 2018

TO BE CONFIRMED

alltranshh

**From:** Kym Burgess <kym.burgess@klprofiling.com.au>  
**Sent:** Monday, 9 July 2018 7:08 AM  
**To:** 'alltranshh@bigpond.com.au'; 'filsmitheighties@gmail.com'  
**Cc:** 'douggolding1965@gmail.com'; 'Jakeovermeyer@gmail.com';  
'chrisandkatiejones@hotmail.com'  
**Subject:** FW: FLOAT SHIFTS



**UPDATED 09/07/18 at 7:00 AM**

MONDAY 9<sup>th</sup> JULY 2018

Pick up RX700 from K&L Depot at **6:00 AM** and deliver to Elizabeth Street @ Norwood (**Wait on-site**) reload and deliver back to K&L Profiling yard

TUESDAY 10<sup>th</sup> JULY 2018

Pick up RX600 from K&L Depot at **5:30 PM** and deliver to Oakland's Road Rail Project by **6:45 PM** (**DO NOT Wait on-**

WEDNESDAY 11<sup>th</sup> JULY 2018

**TO BE CONFIRMED**

THURSDAY 12<sup>th</sup> JULY 2018

Pick up RX700 from K&L Depot at **6:00 AM** and deliver to Ayr Avenue @ Torrens Park by **6:45 AM** (**Wait on-site**) reload and leave on float for Friday

Pick up Wirtgen W1000F for K&L DEPOT at **4:00 AM** and Deliver WIRTGEN W1000F to Hittmann Road/Sturt Highway overtaking lane by near Banrock Station Wines/ Wrigley Flat Road by **6:45 AM**; **wait on site**; reload and deliver back to K&L Depot (**MAY STAY OVERNIGHT TO BE CONFIRMED**)

FRI 13<sup>th</sup> JULY 2018

Deliver RX700 to Birmingham Street at Mile End (OFF SOTH ROAD) by **6:45 AM** (**DO NOT WAIT ON-SITE**)

Pick up RX 700 from Birmingham Street at Mile End (OFF SOTH ROAD) AT **2:00 pm** and deliver back to K&L Yard

Pick up RX600 from Oaklands Road at **11:00 PM** and deliver back to K&L Depot

SATURDAY 14<sup>th</sup> July 2018

**TO BE CONFIRMED**

SUNDAY 15<sup>th</sup> JULY 2018

**TO BE CONFIRMED**

alltranshh

**From:** Kym Burgess <kym.burgess@klprofiling.com.au>  
**Sent:** Friday, 6 July 2018 9:18 AM  
**To:** 'alltranshh@bigpond.com.au'; 'filsmitheighties@gmail.com'  
**Cc:** 'douggolding1965@gmail.com'; 'chrisandkatiejones@hotmail.com'; 'Jakeovermeyer@gmail.com'  
**Subject:** FLOAT SHIFTS

UPDATED 06/07/18 at 7:00 AM

SATURDAY 7<sup>th</sup> JULY 2018

NO WORK

SUNDAY 8<sup>th</sup> JULY 2018

WORK

MONDAY 9<sup>th</sup> JULY 2018

Pick up RX700 from K&L Depot at 6:00 AM and deliver to Elizabeth Street @ Norwood (Wait on-site) reload and deliver back to K&L Profiling yard

TUESDAY 10<sup>th</sup> JULY 2018

Pick up RX600 from K&L Depot at 5:30 PM and deliver to Oakland's Road Rail Project by 6:45 PM (DO NOT Wait on-

WEDNESDAY 11<sup>th</sup> JULY 2018

TO BE CONFIRMED

THURSDAY 12<sup>th</sup> JULY 2018

Pick up RX700 from K&L Depot at 6:00 AM and deliver to Ayr Avenue @ Torrens Park by 6:45 AM (Wait on-site) reload and leave on float for Friday

Pick up Wirtgen W1000F for K&L DEPOT at 4:00 AM and Deliver WIRTGEN W1000F to Hittmann Road/Sturt Highway overtaking lane by near Banrock Station Wines/ Wrigley Flat Road by 5:45 AM (wait on site) reload and deliver back to K&L Depot (MAY STAY OVERNIGHT TO BE CONFIRMED)

Friday 13<sup>th</sup> JULY 2018

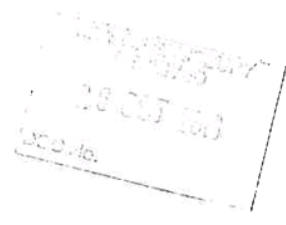
Deliver RX700 to Birmingham Street at Mile End (OFF SOTH ROAD) by 6:45 AM (DO NOT WAIT ON-SITE)

Pick up RX 700 from Birmingham Street at Mile End (OFF SOTH ROAD) AT 2:00 pm and deliver back to K&L Yard

Pick up RX600 from Oaklands Road at 11:00 PM and deliver back to K&L Depot

SATURDAY 14<sup>th</sup> July 2018

TO BE CONFIRMED



alltranshh

From: Kym Burgess <kym.burgess@klprofiling.com.au>  
Sent: Thursday, 5 July 2018 6:29 AM  
To: 'alltranshh@bigpond.com.au'; 'filsmiteighties@gmail.com'  
Cc: 'douggolding1965@gmail.com'; 'Jakeovermeyer@gmail.com';  
'chrisandkatiejones@hotmail.com'  
Subject: FW: Float Shifts Updated



UPDATED 05/07/18 at 7:00 AM

THURSDAY 5<sup>th</sup> JULY 2018

Pick up RX700 from St Andrews Crescent at 9:30 AM and deliver back to K&L Depot **only**

*Plus*

FRIDAY 6<sup>th</sup> JULY 2018

W (CANCELLED DUE TO WET WEATHER)

SATURDAY 7<sup>th</sup> JULY 2018

TO BE CONFIRMED

SUNDAY 8<sup>th</sup> JULY 2018

TO BE CONFIRMED

MONDAY 9<sup>th</sup> JULY 2018

TO BE CONFIRMED

TUESDAY 10<sup>th</sup> JULY 2018

TO BE CONFIRMED

WEDNESDAY 11<sup>th</sup> JULY 2018

TO BE CONFIRMED

THURSDAY 12<sup>th</sup> JULY 2018

Pick up Wirtgen W1000F for K&L DEPOT at 4:00 AM and Deliver WIRTGEN W1000F to Hittmann Road/Sturt Highway overtaking lane by near Banrock Station Wines/ Wrigley Flat Road by 8:45 AM (WAIT ON SITE) reload and deliver back to K&L Depot

Regards

Kym Burgess  
**GENERAL MANAGER**

<image001.png>

alltranshh

**From:** Kym Burgess <kym.burgess@klprofiling.com.au>  
**Sent:** Wednesday, 4 July 2018 8:55 AM  
**To:** 'alltranshh@bigpond.com.au'; 'filmsitheighties@gmail.com'  
**Cc:** 'douggolding1965@gmail.com'; 'chrisandkatiejones@hotmail.com'; 'Jakeovermeyer@gmail.com'  
**Subject:** FW: Float Shifts Updated



**UPDATED 04/07/18 at 7:00 AM**

**Crescent @ Novar Gardens by 6:45 AM (DO NOT WAIT ON-SITE)**

**WEDNESDAY 4<sup>th</sup> JULY 2018**

**Pick up RX700 from St Andrews Crescent at 12:00 PM (Lunchtime) and deliver back to K&L Depot only**

**THURSDAY 5<sup>th</sup> JULY 2018**

**WORK CANCELLED DUE TO WET WEATHER**

**FRIDAY 6<sup>th</sup> JULY 2018**

**WORK CANCELLED DUE TO WET WEATHER**

**SATURDAY 7<sup>th</sup> JULY 2018**

**TO BE CONFIRMED**

**SUNDAY 8<sup>th</sup> JULY 2018**

**TO BE CONFIRMED**

**MONDAY 9<sup>th</sup> JULY 2018**

**TO BE CONFIRMED**

**TUESDAY 10<sup>th</sup> JULY 2018**

**TO BE CONFIRMED**

**WEDNESDAY 11<sup>th</sup> JULY 2018**

**TO BE CONFIRMED**

**THURSDAY 12<sup>th</sup> JULY 2018**

**Pick up Wirtgen W1000F for K&L DEPOT at 4:00 AM and Deliver WIRTGEN W1000F to Hittmann Road/Sturt Highway overtaking lane by near Banrock Station Wines/ Wrigley Flat Road by 6:45 AM (wait on site) reload and deliver back to K&L Depot**

Robin

**From:** Kym Burgess <kym.burgess@klprofiling.com.au>  
**Sent:** Tuesday, 3 July 2018 7:12 AM  
**To:** 'alltranshh@bigpond.com.au'; 'filmsitheighties@gmail.com'  
**Cc:** 'douggolding1965@gmail.com'; 'Jakeovermeyer@gmail.com';  
'chrisandkatiejones@hotmail.com'  
**Subject:** FW: Float Shifts Updated



**UPDATED 03/07/18 at 7:00 AM**

TUESDAY 3<sup>rd</sup> JULY 2018

Deliver RX700 to St Andrews Crescent @ Novar Gardens by 6:45 AM **(DO NOT WAIT ON-SITE)**

WEDNESDAY 4<sup>th</sup> JULY 2018

Pick up RX700 from St Andrews Crescent at **12:00 PM (Lunchtime)** and deliver back to K&L Depot and pick up 1m profiler ready for Thursday

THURSDAY 5<sup>th</sup> JULY 2018

Deliver WIRTGEN W1000F to Hittman Road/Sturt Highway overtaking lane by near Banrock Station Wines/Wrigley Flat Road by **6:45 AM (Wait on site)** reload and deliver back to K&L Depot

FRIDAY 6<sup>th</sup> JULY 2018

**TO BE CONFIRMED**

SATURDAY 7<sup>th</sup> JULY 2018

**TO BE CONFIRMED**

SUNDAY 8<sup>th</sup> JULY 2018

**TO BE CONFIRMED**

MONDAY 9<sup>th</sup> JULY 2018

**TO BE CONFIRMED**

Regards

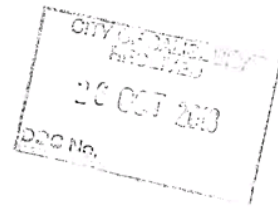
Kym Burgess  
**GENERAL MANAGER**

<image001.png>

Telephone: **8360 2308**  
Facsimile: **8266 9367**  
Mobile: **0419 404 488**

alltranshh

**From:** Kym Burgess <kym.burgess@klprofiling.com.au>  
**Sent:** Monday, 2 July 2018 6:53 AM  
**To:** 'alltranshh@bigpond.com.au'; 'filsmitheighties@gmail.com'  
**Cc:** 'douggolding1965@gmail.com'; 'Jakeovermeyer@gmail.com'; 'chrisandkatiejones@hotmail.com'  
**Subject:** FW: Float Shifts Updated



UPDATED 02/07/18 at 7:00 AM

MONDAY 2<sup>nd</sup> JULY 2018

Deliver RX700 to Marion Sports and Community Hub off Sturt Road by 7:00 AM (Wait on-site) re-load and deliver to Elizabeth Street @ Norwood (Kensington Road to the Parade) WAIT ON SITE Reload and Leave on float for Tuesday

Pick up Wirtgen W1000F 1m profiler at 6:00 AM and Deliver Wirtgen 1m profiler to Ring Road Car-park at Flinders U: f Sturt Road by 6:45 AM (WAIT On-site) reload and deliver back to K&L Depot

TUESDAY 3<sup>rd</sup> JULY 2018

Deliver RX700 to St Andrews Crescent @ Novar Gardens by 6:45 AM (DO NOT WAIT ON-SITE)

WEDNESDAY 4<sup>th</sup> JULY 2018

Pick up RX700 from St Andrews Crescent at 12:00 PM (Lunchtime) and deliver back to K&L Depot

THURSDAY 5<sup>th</sup> JULY 2018

Pick up WIRTGEN W1000F from K&L Depot at 5:30 AM and deliver to Jaeger Road/Sturt Highway overtaking lane by 7:00 AM (wait on site) reload and deliver back to K&L Depot

FRIDAY 6<sup>th</sup> JULY 2018

T: CONFIRMED

Regards

Kym Burgess  
**GENERAL MANAGER**

<image001.png>

Telephone: 8360 2308  
 Facsimile: 8360 2117  
 Mobile: 0419 404 488  
 Email: [kym.burgess@klprofiling.com.au](mailto:kym.burgess@klprofiling.com.au)  
 Web site: [www.klprofiling.com.au](http://www.klprofiling.com.au)

Please consider the environment before printing this email.

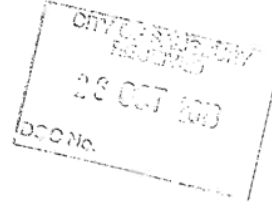
**CONFIDENTIAL COMMUNICATION**

This e-mail and any files transmitted with it are confidential and are intended solely for the use of the individual or entity to whom it is addressed. If you are not the intended recipient or the person responsible for delivering the e-mail to the



alltranshh

**From:** Kym Burgess <kym.burgess@klprofiling.com.au>  
**Sent:** Friday, 29 June 2018 9:17 AM  
**To:** 'alltranshh@bigpond.com.au'; 'filmsitheighties@gmail.com'  
**Cc:** 'douggolding1965@gmail.com'; 'chrisandkatiejones@hotmail.com'; 'Jakeovermeyer@gmail.com'  
**Subject:** FW: Float Shifts Updated



**UPDATED 29/06/18 at 9:00 AM**

Friday 29<sup>th</sup> June 2018

Unload RX600 at K&L Depot at **5:30 AM** and load up Wirtgen W1000F 1m profiler at and Deliver Wirtgen 1m profiler to Eighth Avenue and River Street at St Peters by **6:45 AM (WAIT ON-SITE)** reload and deliver back to K&L Depot

SATURDAY 30<sup>th</sup> June 2018

**CANCELLED**

SUNDAY 1<sup>st</sup> July 2018

Pick up RX700 from K&L Depot at **8:00 AM** and deliver to Elizabeth Street @ Norwood (Kensington Road to the Parade) **WAIT ON SITE** Reload and Leave on float for Monday

MONDAY 2<sup>nd</sup> JULY 2018

Deliver RX700 to Marion Sports and Community Hub off Sturt Road by **7:00 AM (Wait on-site)** re-load and deliver back to St Andrews Crescent @ Novar Gardens **(DO NOT WAIT ON-SITE)**

Pick up Wirtgen W1000F 1m profiler at **6:00 AM** and Deliver Wirtgen 1m profiler to Ring Road Car-park at Flinders Uni off Sturt Road by **6:45 AM (WAIT ON SITE)** reload and deliver back to K&L Depot

TUESDAY 3<sup>rd</sup> JULY 2018

**NO WORK AT THIS STAGE**

WEDNESDAY 4<sup>th</sup> JULY 2018

Pick up RX700 from St Andrews Crescent at **12:00 PM (Lunchtime)** and deliver back to K&L Depot

THURSDAY 5<sup>th</sup> JULY 2018

Pick up WIRTGEN W1000F from K&L Depot at **5:30 AM** and deliver to Jaeger Road/Sturt Highway overtaking lane by **7:00 AM (wait on site)** reload and deliver back to K&L Depot

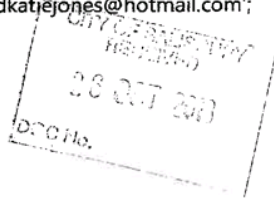
Regards

Kym Burgess  
**GENERAL MANAGER**

<image001.png>

**alltranshh**

**From:** Kym Burgess <kym.burgess@klprofiling.com.au>  
**Sent:** Thursday, 28 June 2018 12:00 PM  
**To:** 'alltranshh@bigpond.com.au'; 'filmsitheighties@gmail.com'  
**Cc:** 'douggolding1965@gmail.com'; 'chrisandkatiejones@hotmail.com'; 'Jakeovermeyer@gmail.com'  
**Subject:** FW: Float Shifts Updated



**UPDATED 28/06/18 at 11:00 AM**

Friday 29<sup>th</sup> June 2018

Unload RX600 at K&L Depot at **5:30 AM** and load up Wirtgen W1000F 1m profiler at and Deliver Wirtgen 1m profiler to Eighth Avenue and River Street at St Peters by 6:45 **AM (WAIT On-site)** reload and deliver back to K&L Depot

21509

Saturday 30<sup>th</sup> June 2018

**CANCELLED**

SUNDAY 1<sup>st</sup> July 2018

**CANCELLED**

MONDAY 2<sup>nd</sup> JULY 2018

Pick up RX700 from K&L Depot at **6:00 AM** and deliver to Marion Sports and Community Hub off Sturt Road by **7:00 AM (wait on-site)** re-load and deliver back to K&L Depot

Pick up Wirtgen W1000F 1m profiler at **6:00 AM** and Deliver Wirtgen 1m profiler to Ring Road Car-park at Flinders Uni off Sturt Road by 6:45 **AM (WAIT on-site)** reload and deliver back to K&L Depot

Tuesday 3<sup>rd</sup> JULY 2018

**TO BE CONFIRMED**

Regards

Kym Burgess  
**GENERAL MANAGER**

<image001.png>

Telephone: **8360 2308**  
Facsimile: **8360 8307**  
Mobile: **0419 404 488**  
Email: [kym.burgess@klprofiling.com.au](mailto:kym.burgess@klprofiling.com.au)  
Web site: [www.klprofiling.com.au](http://www.klprofiling.com.au)

Please consider the environment before printing this email.

**CONFIDENTIAL COMMUNICATION**

This e-mail and any files transmitted with it are confidential and are intended solely for the use of the individual or entity

Attachment 3  
Notice of Category 3 Application and Representations  
received

**DEVELOPMENT ACT 1993  
CITY OF SALISBURY**

**NOTICE OF APPLICATION FOR CATEGORY 3 DEVELOPMENT**  
Pursuant to Section 38(5) of the Development Act, 1993

An application for development has been lodged with the Council for assessment. The details are as follows:

<b>APPLICATION NO:</b>	361/1246/2017/3B
<b>APPLICANT:</b>	M J Beattie and J R Beattie and Medalstone Pty Ltd PO Box 211 DRY CREEK SA 5094
<b>NATURE OF DEVELOPMENT:</b>	TRUCK PARKING DEPOT & SIGNAGE
<b>LOCATION:</b>	106 Levels Road , Cavan SA 5094
<b>CERTIFICATE OF TITLE:</b>	CT-5061/130
<b>ZONE:</b>	Industry

The application may be examined at the Office of the Council located at 12 James Street, Salisbury during business hours (8.30am – 5pm Monday to Friday) and on Council's web site at [www.salisbury.sa.gov.au](http://www.salisbury.sa.gov.au). Any person or body may make representations in writing, or by email to [development@salisbury.sa.gov.au](mailto:development@salisbury.sa.gov.au), concerning this application and should address their representation to the Chief Executive Officer at PO Box 8, Salisbury or [representations@salisbury.sa.gov.au](mailto:representations@salisbury.sa.gov.au). Representations must be received **no later than Tuesday 6<sup>th</sup> February 2018**.

Each person making a submission should indicate whether they wish to appear personally, or be represented by another party, in support of their submission. Please note that should you nominate to be heard in support of your representation, you will be required to attend a Development Assessment Panel meeting held at the Council offices, scheduled on the fourth Tuesday of each month at 6.00pm (unless otherwise advised).

Please note that, pursuant to Section 38(8) of the Development Act, a copy of each representation received will be forwarded to the applicant to allow them to respond to all representations received.

Signed: Marie Molinaro, Development Officer

Date: 16 January 2018

***THIS IS THE FIRST AND ONLY PUBLICATION OF THIS NOTICE***



**STATEMENT OF REPRESENTATION**  
**Pursuant to Section 38 of the Development Act**

**To:** City of Salisbury  
 PO Box 8, SALISBURY SA 5108  
 Email: [representations@salisbury.sa.gov.au](mailto:representations@salisbury.sa.gov.au)

THIS SHEET PROVIDES YOU WITH THE OPPORTUNITY TO MAKE COMMENTS IN RELATION TO A PROPOSED DEVELOPMENT. PLEASE FIND ATTACHED DETAILS OF THE PROPOSED DEVELOPMENT.

<b>Application Number:</b>	361/1246/2017/3B
<b>Applicant</b>	M J Beattie and J R Beattie and Medalstone Pty Ltd
<b>Nature of Development:</b>	TRUCK PARKING DEPOT & SIGNAGE
<b>Location:</b>	106 Levels Road , Cavan SA 5094

**YOUR DETAILS:** (this information must be provided to ensure that this is a valid representation)

NAME(S): Darren & Emma GOLLEY

ADDRESS: 15 PETREL CRESCENT, MANSON LAKES

PHONE NO: [REDACTED] EMAIL: [REDACTED]

I am: (please tick one of the following boxes as appropriate)

- The owner/occupier of the property located at: 15 Petrel Crescent
- Other (please state): .....

**YOUR COMMENTS:**

I/We: (please tick the most appropriate box below)

- Support the proposed development.
- Oppose the proposed development.

**Whether you support or oppose this proposal you must provide written reasons below to ensure that this is a valid representation.**

Firstly, We oppose any development that requires any ongoing business related noise outside of what we believe we are entitled to in point 2 of the appeal. The proposal does not specify days of operation it just states hours, because of the very close proximity to a residential area Mon-Fri hrs 6-7am to 6-7pm would be the reasonable expectation;

PTO

361/1246/2017/3B

There should be no activity that provides the noise levels that disrupt my normal living & going about being a responsible citizen. That to me means nothing or no activity after 6-7pm until 6-7am and Mon-Fri ONLY. We have been living in this house in this location since Dec 2002 and since ALLTRANS have moved into the area the activity and noise levels after normal hours from them have been causing disruption to my family, by interrupting sleep, having to go inside (if entertaining) to hear people speak. The after hrs noise is affecting my evenings & weekends. We have to shut doors & windows when trucks are entering, exiting, loading/unloading leaving engines in a low pitched engine noise etc. I have taken many videos & reported to the EPA & council noise diaries & continue to do this. I believe that the letter written by Dobra Hobbs from Medalstone is completely lacking understanding and the actions of ALLTRANS. My noise diary totally contradict her statements. Just recently on 25<sup>th</sup> Jan 2018, a series of trucks started to enter ALLTRANS @ 10:20pm. I took pics & video of this action. **My concerns would be addressed by:** (state changes/actions to the proposal sought)

I would expect the following conditions to be placed on all business related activity in the entire area including ALLTRANS as they are approx 50m away from people trying to enjoy the outdoors & the areas we create outside.

No activity (this being truck noise, parking, leaving engines idle, reversing loading/unloading or anything that relates to business operation) After 6 or 7pm Monday to Friday for before 6-7am Monday to Friday. No activity on Sat, Sun or Public Holidays. There needs to be a tight & policed council presence & an avenue for breach of these conditions to be brought upon the companies that totally disregard the community. ALLTRANS hasn't made any attempt up until this point to understand their activity and if it causes issues.

I have been making the EPA & council aware of noise issues PTO since 2013. I understand that during business hours, normal operations can occur but, I will continue to bring this to the council & others attention if this continues. I will take things further if required.

Regulation 35(e) of the Development Regulations 2008 requires that a representation must indicate a person's desire to be heard. Please note that if you do not indicate that you wish to be heard, it will be taken that you do not wish to be heard by the Panel.

I/~~we~~:

- Do not wish to be heard in support of my representation.
- Wish to be heard in support of my representation, and I will be:
  - Appearing personally,
  - OR**
  - Represented by the following person: .....
  - Contact details: .....

*(Please note, matters raised in your written representation will be considered during the assessment and do not need to be repeated at the hearing).*

**Your written representation must be received by Council no later than 11.59pm on Tuesday 6<sup>th</sup> February 2018, to ensure that it is a valid representation and taken into account.**

**Representor's Declaration:**

I am aware that the representation will become a public document as prescribed in the Freedom of Information Act 1991, and will be made available to the applicant, agencies and other bodies pursuant to the Development Act 1993, and may be uploaded to the Council's website as an attachment to the hearing agenda.

Signature: D. Gell.....

Date: 28 / 1 / 2018.

**Please complete this checklist to ensure your representation is valid:**

- Name and address of person (or persons).
- If more than one person, details of person making the representation.
- Detail of reasons for making the representation.
- Indication whether or not the person (or persons) wishes to be heard.
- Submitted no later than 11.59pm on **Tuesday 6<sup>th</sup> February 2018.**



**STATEMENT OF REPRESENTATION**  
Pursuant to Section 38 of the Development Act

*Valid*

25 JAN 2018

**To:** City of Salisbury  
PO Box 8, SALISBURY SA 5108  
Email: [representations@salisbury.sa.gov.au](mailto:representations@salisbury.sa.gov.au)

THIS SHEET PROVIDES YOU WITH THE OPPORTUNITY TO MAKE COMMENTS IN RELATION TO A PROPOSED DEVELOPMENT. PLEASE FIND ATTACHED DETAILS OF THE PROPOSED DEVELOPMENT.

<b>Application Number:</b>	361/1246/2017/3B
<b>Applicant</b>	M J Beattie and J R Beattie and Medalstone Pty Ltd
<b>Nature of Development:</b>	TRUCK PARKING DEPOT & SIGNAGE
<b>Location:</b>	106 Levels Road , Cavan SA 5094

**YOUR DETAILS:** (this information must be provided to ensure that this is a valid representation)

NAME(S):..... **GIULIA PROPERTY TRUST**  
 ABN: 19.525.115.647  
 ADDRESS:..... **4 WYFIELD ST**  
**WATTLE PARK SA 5066**

PHONE NO: ..... EMAIL: .....

I am: (please tick one of the following boxes as appropriate)

- The owner/occupier of the property located at: *108 Levels Rd Cavan SA*
- Other (please state): .....

**YOUR COMMENTS:**

I/We: (please tick the most appropriate box below)

- Support the proposed development.
- Oppose the proposed development.

**Whether you support or oppose this proposal you must provide written reasons below to ensure that this is a valid representation.**

*We Support the proposed development and have no concerns  
 as we have worked next to them for 9 years with no problems  
 They are a small operation and keep a tidy depot*

PTO



361/1246/2017/3B

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**My concerns would be addressed by:** *(state changes/actions to the proposal sought)*

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PTO

Item 5.1.4 - Attachment 3 - Notice of Category 3 Application and Representations

Regulation 35(e) of the Development Regulations 2008 requires that a representation must indicate a person's desire to be heard. Please note that if you do not indicate that you wish to be heard, it will be taken that you do not wish to be heard by the Panel.

I/We:

Do not wish to be heard in support of my representation.

Wish to be heard in support of my representation, and I will be:

Appearing personally,

**OR**

Represented by the following person: .....

Contact details: .....

*(Please note, matters raised in your written representation will be considered during the assessment and do not need to be repeated at the hearing).*

**Your written representation must be received by Council no later than 11.59pm on Tuesday 6<sup>th</sup> February 2018, to ensure that it is a valid representation and taken into account.**

**Representor's Declaration:**

I am aware that the representation will become a public document as prescribed in the Freedom of Information Act 1991, and will be made available to the applicant, agencies and other bodies pursuant to the Development Act 1993, and may be uploaded to the Council's website as an attachment to the hearing agenda.

Signature:  .....

Date: 22 / 1 / 18.

**Please complete this checklist to ensure your representation is valid:**

- Name and address of person (or persons).
- If more than one person, details of person making the representation.
- Detail of reasons for making the representation.
- Indication whether or not the person (or persons) wishes to be heard.
- Submitted no later than 11.59pm on **Tuesday 6<sup>th</sup> February 2018.**

*Valid*



**STATEMENT OF REPRESENTATION**  
Pursuant to Section 38 of the Development Act

25 JAN 2018

To: City of Salisbury  
PO Box 8, SALISBURY SA 5108  
Email: [representations@salisbury.sa.gov.au](mailto:representations@salisbury.sa.gov.au)

THIS SHEET PROVIDES YOU WITH THE OPPORTUNITY TO MAKE COMMENTS IN RELATION TO A PROPOSED DEVELOPMENT. PLEASE FIND ATTACHED DETAILS OF THE PROPOSED DEVELOPMENT.

<b>Application Number:</b>	361/1246/2017/3B
<b>Applicant</b>	M J Beattie and J R Beattie and Medalstone Pty Ltd
<b>Nature of Development:</b>	TRUCK PARKING DEPOT & SIGNAGE
<b>Location:</b>	106 Levels Road , Cavan SA 5094

**YOUR DETAILS:** (this information must be provided to ensure that this is a valid representation)

NAME(S):..... **KORA BAY PTY LTD** **A.T.F. THE GIULIANI PROPERTY TRUST**  
A.T.F. THE GIULIANI PROPERTY TRUST **ABN 80 976 078 664**  
 ADDRESS:..... **4 WYFIELD ST WATTLE PARK SA 5066**  
4 WYFIELD ST WATTLE PARK SA 5066  
 PHONE NO: [REDACTED] ..... EMAIL: .....

I am: (please tick one of the following boxes as appropriate)

- The owner/occupier of the property located at: **110 Levels Rd CAVAN SA**
- Other (please state): .....

**YOUR COMMENTS:**

I/We: (please tick the most appropriate box below)

- Support the proposed development.
- Oppose the proposed development.

**Whether you support or oppose this proposal you must provide written reasons below to ensure that this is a valid representation.**

*We Support the proposed development, this company passes no problems in the street and has been operating out of this depot for many years*

PTO



Regulation 35(e) of the Development Regulations 2008 requires that a representation must indicate a person's desire to be heard. Please note that if you do not indicate that you wish to be heard, it will be taken that you do not wish to be heard by the Panel.

I/We:

Do not wish to be heard in support of my representation.

Wish to be heard in support of my representation, and I will be:

Appearing personally,

**OR**

Represented by the following person: .....


Contact details: .....

*(Please note, matters raised in your written representation will be considered during the assessment and do not need to be repeated at the hearing).*

**Your written representation must be received by Council no later than 11.59pm on Tuesday 6<sup>th</sup> February 2018, to ensure that it is a valid representation and taken into account.**

**Representor's Declaration:**

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Signature: ..... 

Date: 22 / 1 / 18

**Please complete this checklist to ensure your representation is valid:**

- Name and address of person (or persons).
- If more than one person, details of person making the representation.
- Detail of reasons for making the representation.
- Indication whether or not the person (or persons) wishes to be heard.
- Submitted no later than 11.59pm on **Tuesday 6<sup>th</sup> February 2018.**

*Valid*  
25 JAN 2018



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Pursuant to Section 38 of the Development Act

**To:** City of Salisbury  
PO Box 8, SALISBURY SA 5108  
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<b>Nature of Development:</b>	TRUCK PARKING DEPOT & SIGNAGE
<b>Location:</b>	106 Levels Road , Cavan SA 5094

**YOUR DETAILS:** (this information must be provided to ensure that this is a valid representation)

NAME(S): ..... *THE GIULIANI NOMINEES No 2* .....  
ABN 99 950 611 810  
 ADDRESS: ..... *4 WYFIELD ST* ..... *112 Levels Rd CAVAN* .....  
WATTLE PARK SA 5086  
 PHONE NO: [REDACTED] EMAIL: .....

I am: (please tick one of the following boxes as appropriate)

- The owner/occupier of the property located at: ..... *112 Levels Rd CAVAN* .....
- Other (please state): .....

**YOUR COMMENTS:**

I/We: (please tick the most appropriate box below)

- Support the proposed development.
- Oppose the proposed development.

**Whether you support or oppose this proposal you must provide written reasons below to ensure that this is a valid representation.**

..... *I support the proposed development and have* .....  
 ..... *no concerns with this company using the depot* .....  
 ..... *for transport, they have been at this address for* .....  
 ..... *9 years with no issues.* .....

PTO

361/1246/2017/3B

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**My concerns would be addressed by:** *(state changes/actions to the proposal sought)*

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PTO

Item 5.1.4 - Attachment 3 - Notice of Category 3 Application and Representations

Regulation 35(e) of the Development Regulations 2008 requires that a representation must indicate a person's desire to be heard. Please note that if you do not indicate that you wish to be heard, it will be taken that you do not wish to be heard by the Panel.

I/We:

Do not wish to be heard in support of my representation.

Wish to be heard in support of my representation, and I will be:

Appearing personally,

**OR**

Represented by the following person: .....

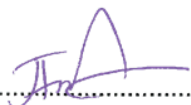
Contact details: .....

*(Please note, matters raised in your written representation will be considered during the assessment and do not need to be repeated at the hearing).*

**Your written representation must be received by Council no later than 11.59pm on Tuesday 6<sup>th</sup> February 2018, to ensure that it is a valid representation and taken into account.**

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Signature:  .....

Date: 22 / 1 / 18.

**Please complete this checklist to ensure your representation is valid:**

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- If more than one person, details of person making the representation.
- Detail of reasons for making the representation.
- Indication whether or not the person (or persons) wishes to be heard.
- Submitted no later than 11.59pm on **Tuesday 6<sup>th</sup> February 2018.**





**STATEMENT OF REPRESENTATION**  
Pursuant to Section 38 of the Development Act

page 1 of 3

To: City of Salisbury  
PO Box 8, SALISBURY SA 5108  
Email: [representations@salisbury.sa.gov.au](mailto:representations@salisbury.sa.gov.au)

THIS SHEET PROVIDES YOU WITH THE OPPORTUNITY TO MAKE COMMENTS IN RELATION TO A PROPOSED DEVELOPMENT. PLEASE FIND ATTACHED DETAILS OF THE PROPOSED DEVELOPMENT.

<b>Application Number:</b>	361/1246/2017/3B
<b>Applicant</b>	M J Beattie and J R Beattie and Medalstone Pty Ltd
<b>Nature of Development:</b>	TRUCK PARKING DEPOT & SIGNAGE
<b>Location:</b>	106 Levels Road, Cavan SA 5094

**YOUR DETAILS:** (this information must be provided to ensure that this is a valid representation)

NAME(S): Anthony R.N.

ADDRESS: 17 PRIORITY COURT NEWBY LAKES SA 5095

PHONE NO: [REDACTED] EMAIL: [REDACTED]

I am: (please tick one of the following boxes as appropriate)

- The owner/occupier of the property located at: above ADDRESS
- Other (please state): N/A

**YOUR COMMENTS:**

I/We: (please tick the most appropriate box below)

- Support the proposed development. with conditions below writing.
- Oppose the proposed development.

Whether you support or oppose this proposal you must provide written reasons below to ensure that this is a valid representation.

(\*) Old time story:  
In the last winter, our neighbours and we suffered  
the debris fire. I want to inform that at that time  
there was a truck of CAVAN Depot parked in the above  
area

page 1 of 3 PTO

Item 5.1.4 - Attachment 3 - Notice of Category 3 Application and Representations

page 2 of 3

361/1246/2017/3B

Levels

side of road in the front ALTRAMS Heavy Haulage of 1st  
 Level Rd CANAD SA 5094 DRIVERS parked it overnight  
 for couple of times at early morning by 5.00 AM to  
 6 AM or late night by 9 PM to 10 PM Weekly  
 drivers parked this truck with in ~~the~~ operating with  
 machinery noisy and smoky exhaust, smoke spread  
 to our houses damaged our environment IT IS  
 very risky in the winter, because smoke stayed  
 for long time and we suffered our health  
 particularly as aged persons in the seniors club with poor  
 health as slowly chronic bronchitis attacked us  
 we lost healthy daily activities we and lost food lost  
 money so we request the below actions from  
 local government, companies and private

My concerns would be addressed by: (state changes/actions to the proposal sought)

We appreciate if local government issue a law to apply  
 for drivers as their social responsibility as driving in  
 the road.

We appreciate if driver's company issue the regular  
 apply for license no more anytime operating past  
 truck with noisy and smoky exhaust longer five minutes  
 in the driving area.

We appreciate if truck drivers always think about  
 social responsibility in operate their trucks at any  
 situation. I can understand you all above representatives  
 Best regards I want to see all above representatives  
 should be take into account as active  
 policies.

*Issue*

Anthony Nguyen, 17 prion <sup>excise</sup> Newson Lakes 5095 PTO  
 NB: 041109343 dated 20 Jun 2018  
 PS: Thanks for one more notice about this matter AL CANAD 5094  
 of Truck parking Dept in 10B and 10B Level <sup>CAVIA 5094</sup> page 2 of 3

page 3 of 3.

Regulation 35(e) of the Development Regulations 2008 requires that a representation must indicate a person's desire to be heard. Please note that if you do not indicate that you wish to be heard, it will be taken that you do not wish to be heard by the Panel.

I/We: *applicant*  
*my family member and I are not well and not yet recovered from winter flu. So*  
 Do not wish to be heard in support of my representation. *It is not suitable for stock in these boxes.*

- Wish to be heard in support of my representation, and I will be:
- Appearing personally, *please*
- OR**
- Represented by the following person: .....
  - Contact details: .....

*(Please note, matters raised in your written representation will be considered during the assessment and do not need to be repeated at the hearing).*

**Your written representation must be received by Council no later than 11.59pm on Tuesday 6<sup>th</sup> February 2018, to ensure that it is a valid representation and taken into account.**

**Representor's Declaration:**

I am aware that the representation will become a public document as prescribed in the Freedom of Information Act 1991, and will be made available to the applicant, agencies and other bodies pursuant to the Development Act 1993, and may be uploaded to the Council's website as an attachment to the hearing agenda.

Signature: *[Signature]* Date: *20 / 01 / 2018*

**Please complete this checklist to ensure your representation is valid:**

- Name and address of person (or persons).
- If more than one person, details of person making the representation. *(only me)*
- Detail of reasons for making the representation.
- Indication whether or not the person (or persons) wishes to be heard. *(not well enough)*
- Submitted no later than 11.59pm on Tuesday 6<sup>th</sup> February 2018.



**STATEMENT OF REPRESENTATION**  
Pursuant to Section 38 of the Development Act

To: City of Salisbury  
PO Box 8, SALISBURY SA 5108  
Email: [representations@salisbury.sa.gov.au](mailto:representations@salisbury.sa.gov.au)

E-MAILED  
Date 6/2/18 By JJ

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<b>Application Number:</b>	361/1246/2017/3B
<b>Applicant</b>	M J Beattie and J R Beattie and Medalstone Pty Ltd
<b>Nature of Development:</b>	TRUCK PARKING DEPOT & SIGNAGE
<b>Location:</b>	106 Levels Road , Cavan SA 5094

**YOUR DETAILS:** (this information must be provided to ensure that this is a valid representation)

NAME(S):..... JAN JARRETT .....

ADDRESS:..... 108 LEVELS ROAD, CAVAN .....

PHONE NO: ..... [REDACTED] .. EMAIL: [REDACTED] ..

I am: (please tick one of the following boxes as appropriate)

- The owner/occupier of the property located at: 108 LEVELS ROAD, CAVAN 5094
- Other (please state): .....

**YOUR COMMENTS:**

*I/We:* (please tick the most appropriate box below)

- Support the proposed development.
- Oppose the proposed development.

**Whether you support or oppose this proposal you must provide written reasons below to ensure that this is a valid representation.**

WE SUPPORT THE PROPOSAL OF ALLTRANS AS WE ARE NOT  
AFFECTED BY NOISE FROM THEIR OPERATIONS DURING THE  
WEEK OR ON WEEKENDS.

PTO

361/1246/2017/3B

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**My concerns would be addressed by:** *(state changes/actions to the proposal sought)*

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PTO

Regulation 35(e) of the Development Regulations 2008 requires that a representation must indicate a person's desire to be heard. Please note that if you do not indicate that you wish to be heard, it will be taken that you do not wish to be heard by the Panel.

I/We:

- Do not wish to be heard in support of my representation.
- Wish to be heard in support of my representation, and I will be:
  - Appearing personally,
  - OR**
  - Represented by the following person: .....
  - Contact details: .....

*(Please note, matters raised in your written representation will be considered during the assessment and do not need to be repeated at the hearing).*

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Signature: ..... *(Herrett)* ..... Date: 6 / 2 / 18

**Please complete this checklist to ensure your representation is valid:**

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Attachment 4  
Relevant Development Plan Extracts and Location  
Maps consolidated 15 December 2016

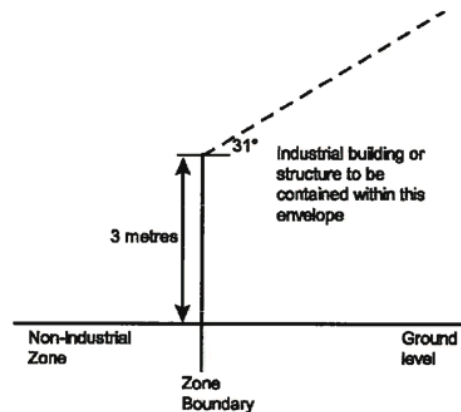
## Industrial Development

### OBJECTIVES

- 1 Industrial, warehouse, storage and transport distribution development on appropriately located land, integrated with transport networks and designed to minimise potential impact on these networks.
- 2 The development of small scale agricultural industries, wineries, mineral water extraction and processing plants, and home based industries in rural areas.
- 3 Industrially zoned allotments and uses protected from encroachment by adjoining uses that would reduce industrial development or expansion.
- 4 Industrial development occurring without adverse effects on the health and amenity of occupiers of land in adjoining zones.
- 5 Compatibility between industrial uses within industrial zones.
- 6 The improved amenity of industrial areas.
- 7 Co-location of industries in townships to enable promotion and implementation of innovative waste recovery practices, methods of power generation and reuse of by-products.

### PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Offices and showrooms associated with industrial, warehouse, storage and transport development should be sited at the front of the building with direct and convenient pedestrian access from the main visitor parking area.
- 2 Any building or structure on, or abutting the boundary of, a non-industrial zone should be restricted to a height of 3 metres above ground level at the boundary and a plane projected at 31 degrees above the horizontal into the development site from that 3 metre height, as shown in the following diagram:



- 3 Industrial development should enable all vehicles to enter and exit the site in a forward direction.
- 4 Industrial development abutting an arterial road, a non-industrial zone boundary, or significant open space should be developed in a manner that does not create adverse visual impacts on the locality.



**Salisbury Council  
General Section  
Industrial Development**

**5 Building facades facing a non-industrial zone, public road, or public open space should:**

- (a) use a variety of building finishes
- (b) not consist solely of metal cladding
- (c) contain materials of low reflectivity
- (d) incorporate design elements to add visual interest
- (e) avoid large expanses of blank walls.

**6 Industrial development should minimise significant adverse impact on adjoining uses due to hours of operation, traffic, noise, fumes, smell, dust, paint or other chemical over-spray, vibration, glare or light spill, electronic interference, ash or other harmful or nuisance-creating impacts.**

**7 Development within 50 metres of the Residential Zone boundary should:**

- (a) demonstrate appropriate acoustic performance
- (b) ensure that all noise sources including machinery, outside loading, unloading and other service areas are located away from the Residential Zone boundary
- (c) comprise buildings of masonry or equivalent construction to minimise the transmission of noise with openings located away from residential properties
- (d) limit operating hours to between 7am and 6 pm
- (e) where there is a railway on the boundary development should:
  - (i) ensure the rear walls of the industrial premises are sited on the rear boundary of the allotments
  - (ii) incorporate building materials that will minimise the reflection of railway traffic noise towards the residential area opposite
  - (iii) where a wall is not located on the boundary, landscaping, including mounding, land sculpting and/or thick planting, is to be established between the rear walls of the industrial premises and the railway in order to minimise the reflection of railway traffic noise.

**8 Landscaping should be incorporated as an integral element of industrial development along non-industrial zone boundaries.**

**9 Fencing (including colour-coated wire mesh fencing) adjacent to public roads should be set back in one of the following ways:**

- (a) in line with the building facade
- (b) behind the building line
- (c) behind a landscaped area that softens its visual impact.

**10 Marine aquaculture onshore storage, cooling and processing facilities should not impair the coastline and its visual amenity and should:**

- (a) be sited, designed, landscaped and developed at a scale and using external materials that minimise any adverse visual impact on the coastal landscape
- (b) be sited and designed with appropriate vehicular access arrangement
- (c) include appropriate waste treatment and disposal.

## Infrastructure

### OBJECTIVES

- 1 Infrastructure provided in an economical and environmentally sensitive manner.
- 2 Infrastructure, including social infrastructure, provided in advance of need.
- 3 Suitable land for infrastructure identified and set aside in advance of need.
- 4 The visual impact of infrastructure facilities minimised.
- 5 The efficient and cost-effective use of existing infrastructure.

### PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development should not occur without the provision of adequate utilities and services, including:
  - (a) electricity supply
  - (b) water supply
  - (c) drainage and stormwater systems
  - (d) waste disposal
  - (e) effluent disposal systems
  - (f) formed all-weather public roads
  - (g) telecommunications services
  - (h) social infrastructure, community services and facilities
  - (i) gas services.
- 2 Development should only occur where it provides, or has access to, relevant easements for the supply of infrastructure.
- 3 Development should incorporate provision for the supply of infrastructure services to be located within common service trenches where practicable.
- 4 Development should not take place until adequate and co-ordinated drainage of the land is assured.
- 5 Development in urban areas should not occur without provision of an adequate reticulated domestic quality mains water supply and an appropriate waste treatment system.
- 6 In areas where no reticulated water supply is available, buildings whose usage is reliant on a water supply should be equipped with an adequate and reliable on-site water storage system.
- 7 Electricity infrastructure should be designed and located to minimise its visual and environmental impacts.
- 8 Development and landscaping within 25 metres of the 275 kV overhead electricity lines should ensure that all clearances and safety restrictions are met.

## Interface between Land Uses

### OBJECTIVES

- 1 Development located and designed to minimise adverse impact and conflict between land uses.
- 2 Protect community health and amenity from adverse impacts of development.
- 3 Protect desired land uses from the encroachment of incompatible development.

### PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development should not detrimentally affect the amenity of the locality or cause unreasonable interference through any of the following:
  - (a) the emission of effluent, odour, smoke, fumes, dust or other airborne pollutants
  - (b) noise
  - (c) vibration
  - (d) electrical interference
  - (e) light spill
  - (f) glare
  - (g) hours of operation
  - (h) traffic impacts.
- 2 Development should be sited and designed to minimise negative impacts on existing and potential future land uses desired in the locality.
- 3 Development adjacent to a **Residential Zone** should be designed to minimise overlooking and overshadowing of adjacent dwellings and private open space.
- 4 Residential development adjacent to non-residential zones and land uses should be located, designed and/or sited to protect residents from potential adverse impacts from non-residential activities.
- 5 Sensitive uses likely to conflict with the continuation of lawfully existing developments and land uses desired for the zone should be designed to minimise negative impacts.
- 6 Non-residential development on land abutting a residential zone should be designed to minimise noise impacts to achieve adequate levels of compatibility between existing and proposed uses.

### Noise Generating Activities

- 7 Development that emits noise (other than music noise) should include noise attenuation measures that achieve the relevant *Environment Protection (Noise) Policy* criteria when assessed at the nearest existing noise sensitive premises.
- 8 Development with the potential to emit significant noise (e.g. industry) should incorporate noise attenuation measures that prevent noise from causing unreasonable interference with the amenity of noise sensitive premises.

## Landscaping, Fences and Walls

### OBJECTIVES

- 1 The amenity of land and development enhanced with appropriate planting and other landscaping works, using locally indigenous plant species where possible.
- 2 Functional fences and walls that enhance the attractiveness of development.

### PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development should incorporate open space and landscaping and minimise hard paved surfaces in order to:
  - (a) complement built form and reduce the visual impact of larger buildings (eg taller and broader plantings against taller and bulkier building components)
  - (b) enhance the appearance of road frontages
  - (c) screen service yards, loading areas and outdoor storage areas
  - (d) minimise maintenance and watering requirements
  - (e) enhance and define outdoor spaces, including car parking areas
  - (f) maximise shade and shelter
  - (g) assist in climate control within and around buildings
  - (h) minimise heat absorption and reflection
  - (i) maintain privacy
  - (j) maximise stormwater re-use
  - (k) complement existing vegetation, including native vegetation
  - (l) contribute to the viability of ecosystems and species
  - (m) promote water and biodiversity conservation.
- 2 Landscaping should:
  - (a) include the planting of locally indigenous species where appropriate
  - (b) be oriented towards the street frontage
  - (c) result in the appropriate clearance from powerlines and other infrastructure being maintained.
- 3 Landscaping should not:
  - (a) unreasonably restrict solar access to adjoining development
  - (b) cause damage to buildings, paths and other landscaping from root invasion, soil disturbance or plant overcrowding

## Orderly and Sustainable Development

### OBJECTIVES

- 1 Orderly and economical development that creates a safe, convenient and pleasant environment in which to live.
- 2 Development occurring in an orderly sequence and in a compact form to enable the efficient provision of public services and facilities.
- 3 Development that does not jeopardise the continuance of adjoining authorised land uses.
- 4 Development that does not prejudice the achievement of the provisions of the Development Plan.
- 5 Development abutting adjoining Council areas having regard to the policies of that Council's Development Plan.
- 6 Urban development contained within existing townships and settlements and located only in zones designated for such development.

### PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development should not prejudice the development of a zone for its intended purpose.
- 2 Land outside of townships and settlements should primarily be used for primary production and conservation purposes.
- 3 The economic base of the region should be expanded in a sustainable manner.
- 4 Urban development should form a compact extension to an existing built-up area.
- 5 Ribbon development should not occur along the coast, water frontages or arterial roads shown in *Overlay Maps - Transport*.
- 6 Development should be located and staged to achieve the economical provision of public services and infrastructure, and to maximise the use of existing services and infrastructure.
- 7 Where development is expected to impact upon the existing infrastructure network (including the transport network), development should demonstrate how the undue effect will be addressed.
- 8 Vacant or underutilised land should be developed in an efficient and co-ordinated manner to not prejudice the orderly development of adjacent land.

**Salisbury Council**  
**General Section**  
**Transportation and Access**

- 5 Land uses that generate large numbers of visitors such as shopping centres and areas, places of employment, schools, hospitals and medium to high density residential uses should be located so that they can be serviced by existing transport networks and encourage walking and cycling.
- 6 Development generating high levels of traffic, such as schools, shopping centres and other retail areas, entertainment and sporting facilities, should incorporate passenger pick-up and set down areas. The design of such areas should ensure interference to existing traffic is minimised and give priority to pedestrians, cyclists and public and community transport users.
- 7 The location and design of public and community transport set-down and pick-up points should maximise safety and minimise the isolation and vulnerability of users.
- 8 Development should provide safe and convenient access for all anticipated modes of transport including cycling, walking, public and community transport, and motor vehicles.
- 9 Development at intersections, pedestrian and cycle crossings, and crossovers to allotments should maintain or enhance sightlines for motorists, cyclists and pedestrians to ensure safety for all road users and pedestrians.
- 10 Driveway crossovers affecting pedestrian footpaths should maintain the level of the footpath.
- 11 Development should discourage commercial and industrial vehicle movements through residential streets and adjacent other sensitive land uses such as schools.
- 12 Industrial/commercial vehicle movements should be separated from passenger vehicle car-parking areas.
- 13 Development should make sufficient provision on site for the loading, unloading and turning of all traffic likely to be generated.

**Cycling and Walking**

- 14 Development should ensure that a permeable street and path network is established that encourages walking and cycling through the provision of safe, convenient and attractive routes with connections to adjoining streets, paths, open spaces, schools, public and community transport stops and activity centres.
- 15 Development should provide access, and accommodate multiple route options, for cyclists by enhancing and integrating with:
  - (a) open space networks, recreational trails, parks, reserves and recreation areas
  - (b) Adelaide's Metropolitan Open Space System.
- 16 Cycling and pedestrian networks should be designed to be permeable and facilitate direct and efficient passage to neighbouring networks and facilities.
- 17 New developments should give priority to and not compromise existing designated bicycle routes.
- 18 Where development coincides with, intersects or divides a proposed bicycle route or corridor, development should incorporate through-access for cyclists.
- 19 Developments should encourage and facilitate cycling as a mode of transport by incorporating end-of-journey facilities including:
  - (a) showers, changing facilities, and secure lockers
  - (b) signage indicating the location of bicycle facilities

- (c) secure bicycle parking facilities provided at the rate set out in [Table Sell3 - Off Street Bicycle Parking Requirements](#).
- 20 Pedestrian facilities and networks should be designed and provided in accordance with relevant provisions of the *Australian Standards and Austroads Guide to Traffic Engineering Practice Part 13*.
- 21 Cycling facilities and networks should be designed and provided in accordance with the relevant provisions of the *Australian Standards and Austroads Guide to Traffic Engineering Practice Part 14*.

**Access**

- 22 Development should have direct access from an all weather public road.
- 23 Development should be provided with safe and convenient access which:
  - (a) avoids unreasonable interference with the flow of traffic on adjoining roads
  - (b) provides appropriate separation distances from existing roads or level crossings
  - (c) accommodates the type and volume of traffic likely to be generated by the development or land use and minimises induced traffic through over-provision
  - (d) is sited and designed to minimise any adverse impacts on the occupants of and visitors to neighbouring properties.
- 24 Development should not restrict access to publicly owned land such as recreation areas.
- 25 The number of vehicle access points onto arterial roads shown on *Overlay Maps - Transport* should be minimised, and where possible access points should be:
  - (a) limited to local roads
  - (b) shared between developments.
- 26 The number of access points for cyclists and pedestrians onto all adjoining roads should be maximised.
- 27 Development with access from roads with existing or projected traffic volumes exceeding 6000 vehicles per day should be sited to avoid the need for vehicles to reverse on to or from the road.
- 28 Development with access from arterial roads or roads as shown on *Overlay Maps - Transport* should be sited to avoid the need for vehicles to reverse on to or from the road.
- 29 Driveways, access tracks and parking areas should be designed and constructed to:
  - (a) follow the natural contours of the land
  - (b) minimise excavation and/or fill
  - (c) minimise the potential for erosion from run-off
  - (d) avoid the removal of existing vegetation
  - (e) be consistent with *Australian Standard AS 2890 Parking facilities*.

**Access for People with Disabilities**

- 30 Development should be sited and designed to provide convenient access for people with a disability.
- 31 Where appropriate and practical, development should provide for safe and convenient access to the coast and beaches for disabled persons.

**Salisbury Council**  
**General Section**  
**Transportation and Access**

### **Vehicle Parking**

- 32 Development should provide off-street vehicle parking and specifically marked disabled car parking places to meet anticipated demand in accordance with Table Sall2 - Off Street Vehicle Parking Requirements or Table Sall2A - Off Street Vehicle Parking Requirements for Designated Areas (whichever applies) unless an agreement is reached between the Council and the applicant for a reduced number of parking spaces where one of the following applies:
- (a) a financial contribution is paid into the Council Car Parking Funds specified by the Council, in accordance with the gazetted rate per car park associated with the 'Car Park Fund Areas' identified on Concept Plan Map Sall27 - Salisbury District Centre Car Park Fund Area, Concept Plan Map Sall29 - Ingle Farm District Centre Car Park Fund Area and Concept Plan Map Sall32 - Mewson Lakes Town Centre Car Parking Fund Area
  - (b) it can be demonstrated that fewer car parks would be required to meet the car parking needs associated with the development.
- 33 Development should be consistent with *Australian Standard AS 2890 Parking facilities*.
- 34 Vehicle parking areas should be sited and designed in a manner that will:
- (a) facilitate safe and convenient pedestrian linkages to the development and areas of significant activity or interest in the vicinity of the development
  - (b) include safe pedestrian and bicycle linkages that complement the overall pedestrian and cycling network
  - (c) not inhibit safe and convenient traffic circulation
  - (d) result in minimal conflict between customer and service vehicles
  - (e) avoid the necessity to use public roads when moving from one part of a parking area to another
  - (f) minimise the number of vehicle access points to public roads
  - (g) avoid the necessity for backing onto public roads
  - (h) where reasonably possible, provide the opportunity for shared use of car parking and integration of car parking areas with adjoining development to reduce the total extent of vehicle parking areas and the requirement for access points
  - (i) not dominate the character and appearance of a site when viewed from public roads and spaces
  - (j) provide landscaping that will shade and enhance the appearance of the vehicle parking areas.
- 35 Vehicle parking areas should be designed to reduce opportunities for crime by:
- (a) maximising the potential for passive surveillance by ensuring they can be overlooked from nearby buildings and roads
  - (b) incorporating walls and landscaping that do not obscure vehicles or provide potential hiding places
  - (c) being appropriately lit
  - (d) having clearly visible walkways.
- 36 Where parking areas are not obviously visible or navigated, signs indicating the location and availability of vehicle parking spaces associated with businesses should be displayed at locations readily visible to customers.



## Industry Zone

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this zone.

### OBJECTIVES

- 1 A zone primarily accommodating a wide range of industrial, warehouse, storage and transport land uses.
- 2 Development that is compatible with existing and forecast noise nuisance from aircraft operations based at RAAF Edinburgh or Parafield Airport.
- 3 Provision of landscaped buffers adjacent to main roads and residential areas.
- 4 Water sensitive urban design and landscaping incorporated as an integral elements of development within the zone.

### PRINCIPLES OF DEVELOPMENT CONTROL

#### Land Use

- 1 The following forms of development are envisaged in the zone:
  - Industry
  - office in association with and ancillary to industry
  - transport distribution
  - warehouse.
- 2 Centre facilities such as shops, offices and consulting rooms should not occur unless it can be demonstrated that they primarily serve businesses in the zone and do not detract from the function of any centre zone or centres generally.
- 3 Development listed as non-complying is generally inappropriate.

#### Form and Character

- 4 Development should be set back from any road frontage in accordance with [Table Sall1- Building Setbacks from Road Boundaries](#).
- 5 Industrial buildings should not occupy more than 50 per cent of the total area of the site upon which they are located.
- 6 In areas where a uniform street setback pattern has not been established, buildings should be set back in accordance with the following criteria (subject to adequate provision of car parking spaces and landscaping between buildings and the road):
  - (a) buildings up to a height of 6 metres should be sited at least 8 metres from the primary street alignment
  - (b) buildings exceeding a height of 6 metres should be sited at least 10 metres from the primary street alignment
  - (c) where an allotment has two street frontages, no building should be erected within 4 metres of the secondary street alignment.

**Salisbury Council  
Zone Section  
Industry Zone**

- 7 Development involving the outdoor storage of goods or materials should:
- not be located adjacent to arterial roads and major roads, or facing residential properties unless it can be demonstrated that the amenity of the locality will be enhanced
  - ensure that storage and service areas are effectively screened from public view.
- 8 Industrial buildings should present an attractive façade by incorporating offices of masonry or similar construction at the front of the building, and through the use of architectural elements that will enhance the appearance of the locality, such as surface treatments, form or decoration
- 9 Building facades facing land zoned for residential purposes should not contain openings or entrance ways that would result in the transmission of noise that would adversely affect the residential amenity.
- 10 Access points which are required to cross an open swale stormwater drain should:
- be minimised by limiting the number of allotments with frontage to swales
  - serve 2 or more allotments where possible
  - be designed to facilitate efficient stormwater management and drainage.
- 11 Open swale stormwater drainage should:
- be used in conjunction with roadways to cater for major stormwater flows and where practicable, for minor (2 to 10 year) stormwater flows
  - be designed in an attractive form with grass-lined sides of no more than 1-in-5 gradient and a concrete base
  - allow for the planting of trees and shrubs at either side of the channel.
- 12 Any plant or equipment with potential to cause an environmental nuisance (including a chimney stack or air-conditioning plant) should be sited as far as possible from adjoining non-industrially zoned allotments, and should be designed to minimise its effect on the amenity of the locality.
- 13 Development should ensure that the following is achieved:
- at least 10 per cent of the site is landscaped
  - landscaping along allotment boundaries that adjoin roads or public reserve and at least one side boundary, for a width of at least 3 metres
  - landscaping within parking areas to break-up extensive areas of paving.
- 14 Freestanding structures should not exceed 6 metres in height and should be restricted to one such structure per 6 tenancies.
- 15 Advertisements and advertising hoardings should not include any of the following:
- flashing or animated signs
  - bunting, streamers, flags, or wind vanes
  - roof-mounted advertisements projected above the roofline
  - parapet-mounted advertisements projecting above the top of the parapet.
- 16 Advertisements and advertising hoardings that are internally illuminated should be unobtrusive and not conspicuous when viewed from adjacent residential properties.

## Greater Levels Policy Area 8

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this policy area.

### OBJECTIVES

- 1 A policy area primarily accommodating a range of industrial activities.
- 2 The provision of unique entrance treatment identifying each industrial area at the junction of the major access points with arterial roads or major collector roads.

### PRINCIPLES OF DEVELOPMENT CONTROL

#### Form and Character

- 1 Development should be designed and sited to incorporate unique entrance treatments that incorporate co-ordinated area identification signage and pavement treatment to denote the entrance to the industrial area.
- 2 Land division should be designed to allow for the establishment of unique entrance treatments that identify each industrial area at the junction of the major access points with arterial roads or major collector roads.
- 3 Development on sites adjacent to Diagonal Road should:
  - (a) incorporate a 10 metre wide landscaped and mounded buffer area along the boundary of the site contiguous with the respective road boundary
  - (b) provide a building setback distance of 25 metres from the road to any building erected on the site.

#### Land Division

- 4 Land division should ensure that:
  - (a) all open space is located in the area marked 'Landscape Buffer' as shown on [Concept Plan Map Sall8 - Mawson Lakes](#)
  - (b) allotments do not have direct access to arterial roads
  - (c) construction of spur-lines linking with the Port Augusta to Adelaide railway and adjacent marshalling yards do not unduly detract from the adjacent Open Space Zone, and is adequately screened from the view of the general public.

**PROCEDURAL MATTERS****Complying Development**

Complying developments are prescribed in Schedule 4 of the *Development Regulations 2008*.

**Non-complying Development**

Development (including building work, a change in the use of land, or division of an allotment) for the following is non-complying:

Form of development	Exceptions
Advertisement and/or Advertising hoarding	Except where it complies with all of the following: (a) it does not move, rotate or incorporate flashing lights (b) it does not wholly or partly consist of bunting, streamers, flags, wind vanes and the like (c) if attached to a building, it does not wholly or partly extend above the top of the wall or the fascia.
Amusement machine centre	
Animal keeping	
Builder's yard	Except where it achieves all of the following: (a) it is located on an allotment which does not adjoin an arterial, controlled-access arterial or proposed controlled-access arterial road (b) it is located at least 20 metres from an arterial, controlled-access arterial or proposed controlled-access arterial road.
Bus depot where it is located within the Infrastructure Policy Area 9	
Caravan park	
Community centre	
Consulting room	
Dairy	
Dwelling	Except where: (a) ancillary to and in association with industrial development (b) located on the same allotment.
Educational establishment	Except where: (a) ancillary to and in association with industrial development (b) located on the same allotment.
General industry where it is located within the Pooraka Policy Area 11	
Horse keeping	
Horticulture	
Hospital	
Hotel	
Intensive animal keeping where it is located in the Pooraka Policy Area 11	

**Salisbury Council  
Zone Section  
Industry Zone**

<b>Form of development</b>	<b>Exceptions</b>
Motel	
Nursing home	
Office	Except where it achieves all of the following: (a) ancillary to and in association with industrial development (b) located on the same allotment (c) it achieves one of the following: (i) it is located outside of the <b>Pooraka Policy Area 11</b> (ii) it is located within the <b>Pooraka Policy Area 11</b> and it has a maximum floor area of no more than 250 square metres.
Place of worship	Except where it has a floor area greater than 500 square metres and is less than 60 metres from the boundary of the <b>Industry Zone</b> .
Pre-school	
Prescribed mining operations	
Primary school	
Residential flat building	
Road transport terminal where it is located in the <b>Infrastructure Policy Area 9</b> at Walkley Heights	
Shop or group of shops	Except where the gross leasable area is less than 250 square metres and it is located in one of the following policy areas or suburb: (a) <b>Greater Levels Policy Area 8</b> (b) <b>Pooraka Policy Area 11</b> (c) the suburb of Greenfields.
Special industry	
Stock sales yard	Except where it is located outside of the <b>Pooraka Policy Area 11</b> .
Stock slaughter works	Except where it is a poultry slaughter works and it is located within the <b>Burton Poultry Processing Policy Area 5</b> .
Telecommunication facility where it is located within 100 metres of a State Heritage Place	
Tourist accommodation	
Waste reception, storage, treatment or disposal in <b>Infrastructure Policy Area 9</b>	Except where it is located outside of the <b>Pooraka Policy Area 11</b> and/or the <b>Infrastructure Policy Area 9</b> .
Wrecking yard	Except where it achieves all of the following: (a) it is located on an allotment which does not adjoin an arterial, controlled-access arterial or proposed controlled-access arterial road (b) it is located at least 20 metres from an arterial, controlled-access arterial or proposed controlled-access arterial road.

**Public Notification**

Categories of public notification are prescribed in Schedule 9 of the *Development Regulations 2008*.

## Table Sal/2 - Off Street Vehicle Parking Requirements

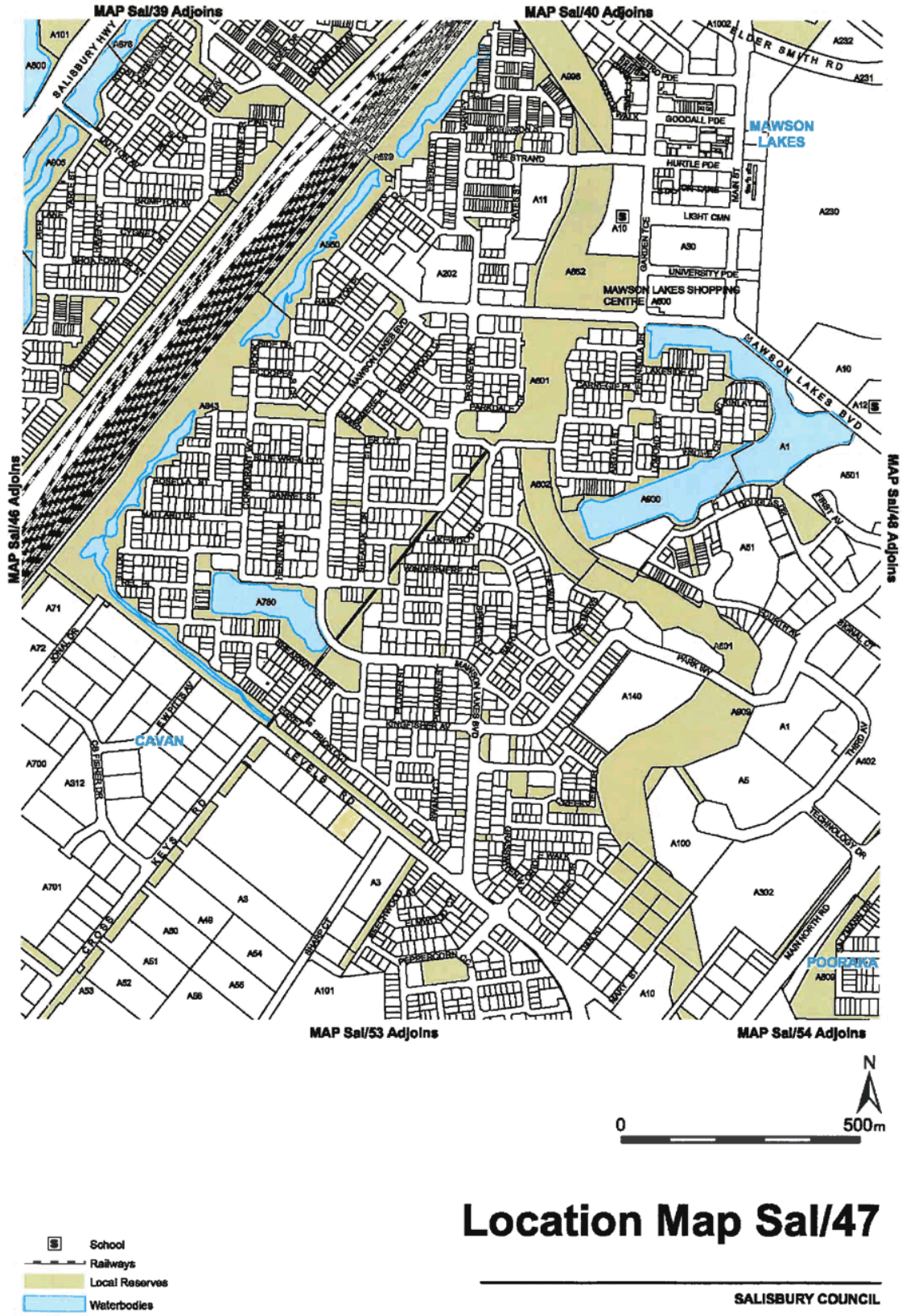
The following vehicle parking requirements do not apply:

- (a) to the Mixed Use (Bulky Goods, Entertainment, Leisure) Zone except where the form of development is light industry whereby the rates for industry, warehouse, stores are applicable
- (b) to development that is subject to the requirements in [Table Sal/2A - Off Street Vehicle Parking Requirements for Designated Areas](#).

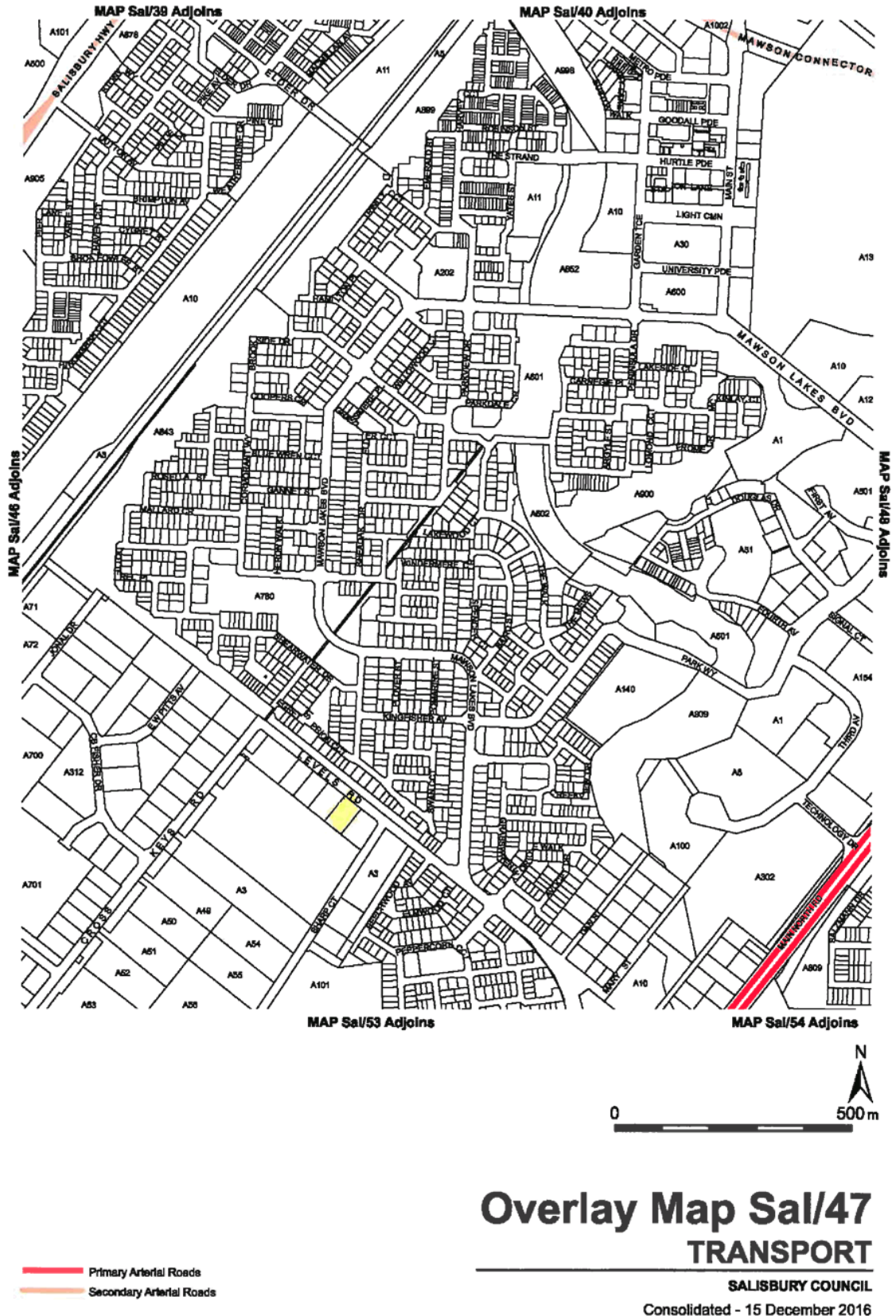
Form of Development	Number of Required Car Parking Spaces
<b>Accommodation</b>	
Aged Care / retirement home	1 space per unit
Serviced apartment	1 space per unit plus 1 space per employee
Motel	1 space per unit
<b>Commercial</b>	
Bulky goods outlet	3 spaces per 100 square metres of gross leasable floor area
Cinema	1 space per 4 cinema seats
Hotel	1 space per 2 square meters of floor area available to the public
Public bar	1 space per 6 square metres of floor area available to the public
Lounge or beer garden	1 space per 6 square metres of floor area available to the public
Gaming room	1 space per 2 machines
Office	1 space per 25 square metres, with a minimum of 4 spaces per office
Restaurant	Greater of 1 space for every 3 seats or 1 space for every 15 square metres of dining area
Service trade premises	3 spaces per 100 square metres
Shop	7 spaces per 100 square metres of gross leasable area for shops outside of centre zones 5 spaces per 100 square metres of gross leasable area for shops within centre zones
<b>Community/civic</b>	
Child care centre	1 space per 4 children
Community centre	10 spaces per 100 square metres of total floor area
Library	4 spaces per 100 square metres
Place of worship	Greater of 1 space for every 3 seats or every 3 attendees
<b>Dwellings</b>	

**Salisbury Council**  
**Table Section**  
**Table Sall2 - Off Street Vehicle Parking Requirements**

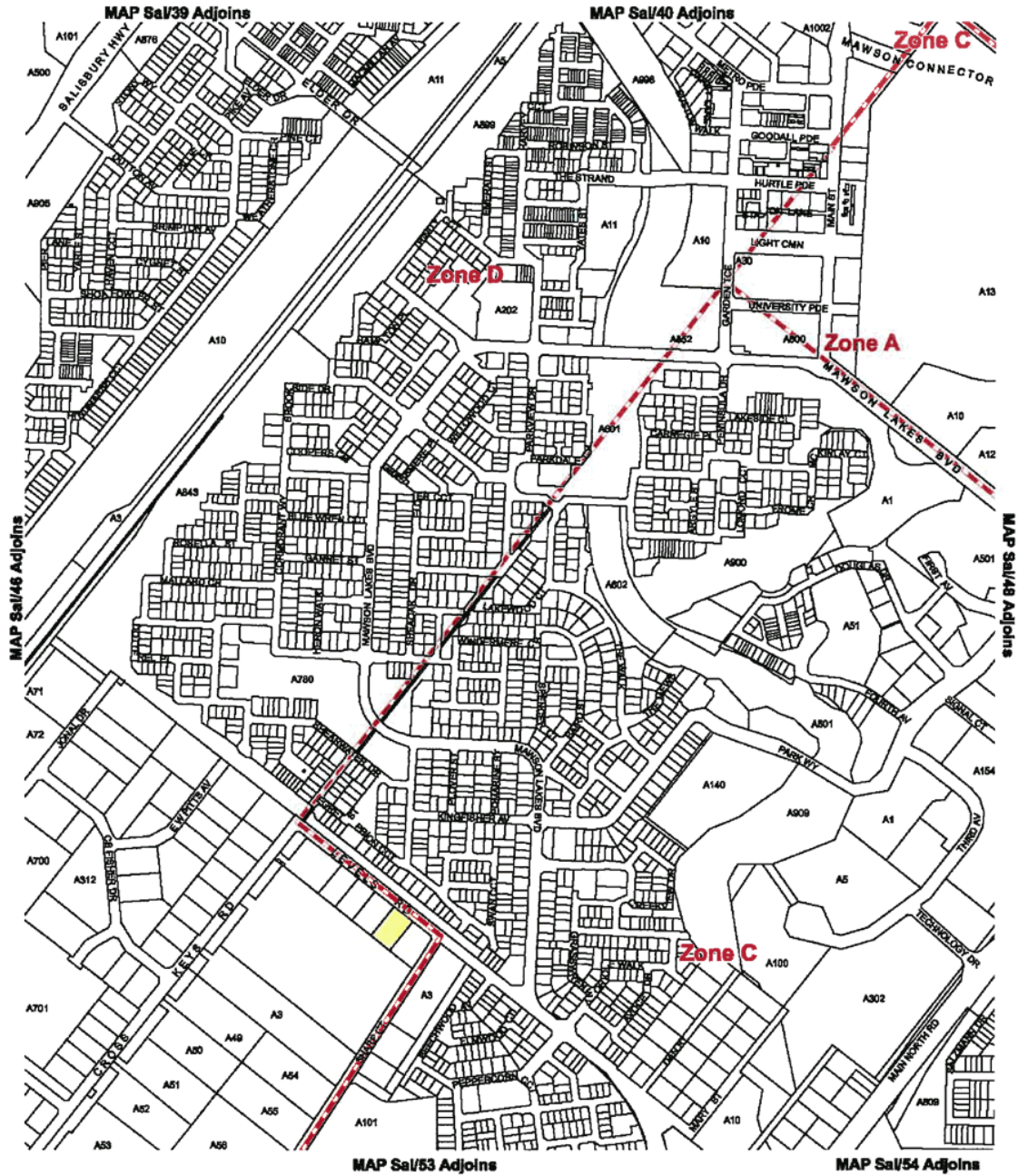
<b>Form of Development</b>	<b>Number of Required Car Parking Spaces</b>
Detached dwelling Semi Detached Dwelling Row Dwelling	2 spaces per dwelling, one of which is to be covered
Residential flat building Multiple dwelling Group dwelling	1 space per dwelling, plus 0.5 on-site visitor car parking spaces per dwelling
<b>Industry, warehouses, stores</b>	
Office component	1 space per 30 square metres
Plus	Plus
Non-office component	
Up to 200 square metres	1 space per 50 square metres
Plus 200-2000 square metres	1 additional space for every 75 square metres
Plus greater than 2000 square metres	1 additional space for every 150 square metres
Or	Or
For labour intensive industries, inclusive of office component (whichever ever is greater)	0.75 car parking spaces per employee
<b>Medical</b>	
Consulting room	10 per 100 square metres of total floor area, with a minimum of 3 spaces per tenancy
Hospital	2.5 spaces per bed
Nursing home	1 space for every 4 beds
The following vehicle parking requirements apply to development specifically within the <b>Mixed Use (Bulky Goods, Entertainment and Leisure) Zone</b> :	
<b>Form of Development</b>	<b>Minimum number of required vehicle parking spaces</b>
All forms of development (except Light Industry)	3 spaces per 100 square metres of gross leasable floor area







Item 5.1.4 - Attachment 4 - Relevant Development Plan Extracts and Location Maps consolidated 15 December 2016



NOTE: Airport Building Heights should be read in conjunction with concept plan map showing heights for Edinburgh Defence Airfield

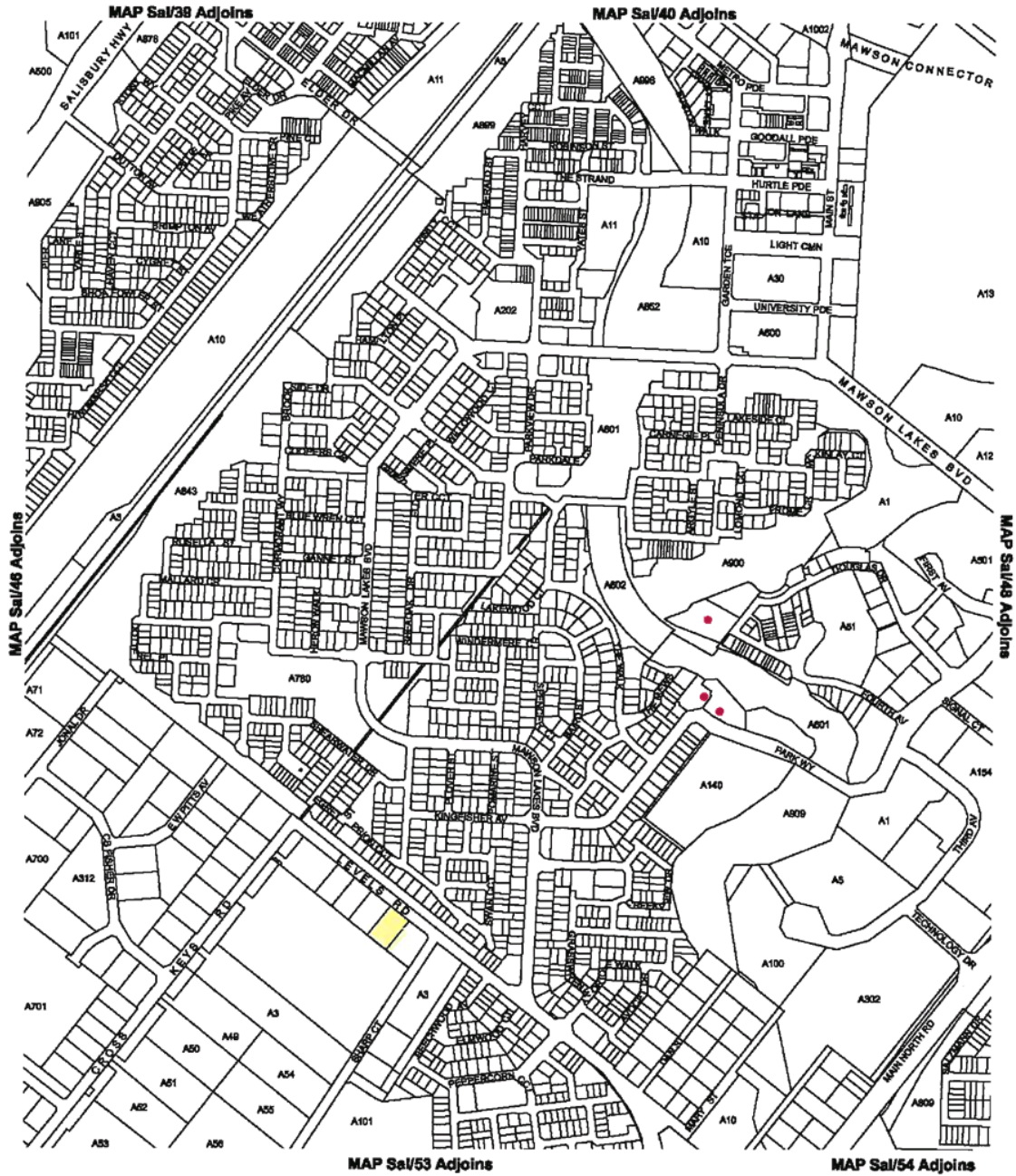
- Airport Building Heights**  
 Referral to Commonwealth Secretary for Dept. of Transport and Regional Services
- Zone A All Structures
  - Zone C All Structures Exceeding 15 metres above existing ground level
  - Zone D All Structures Exceeding 45 metres above existing ground level

 Airport Building Heights

## Overlay Map Sal/47 DEVELOPMENT CONSTRAINTS

SALISBURY COUNCIL

Consolidated - 15 December 2016



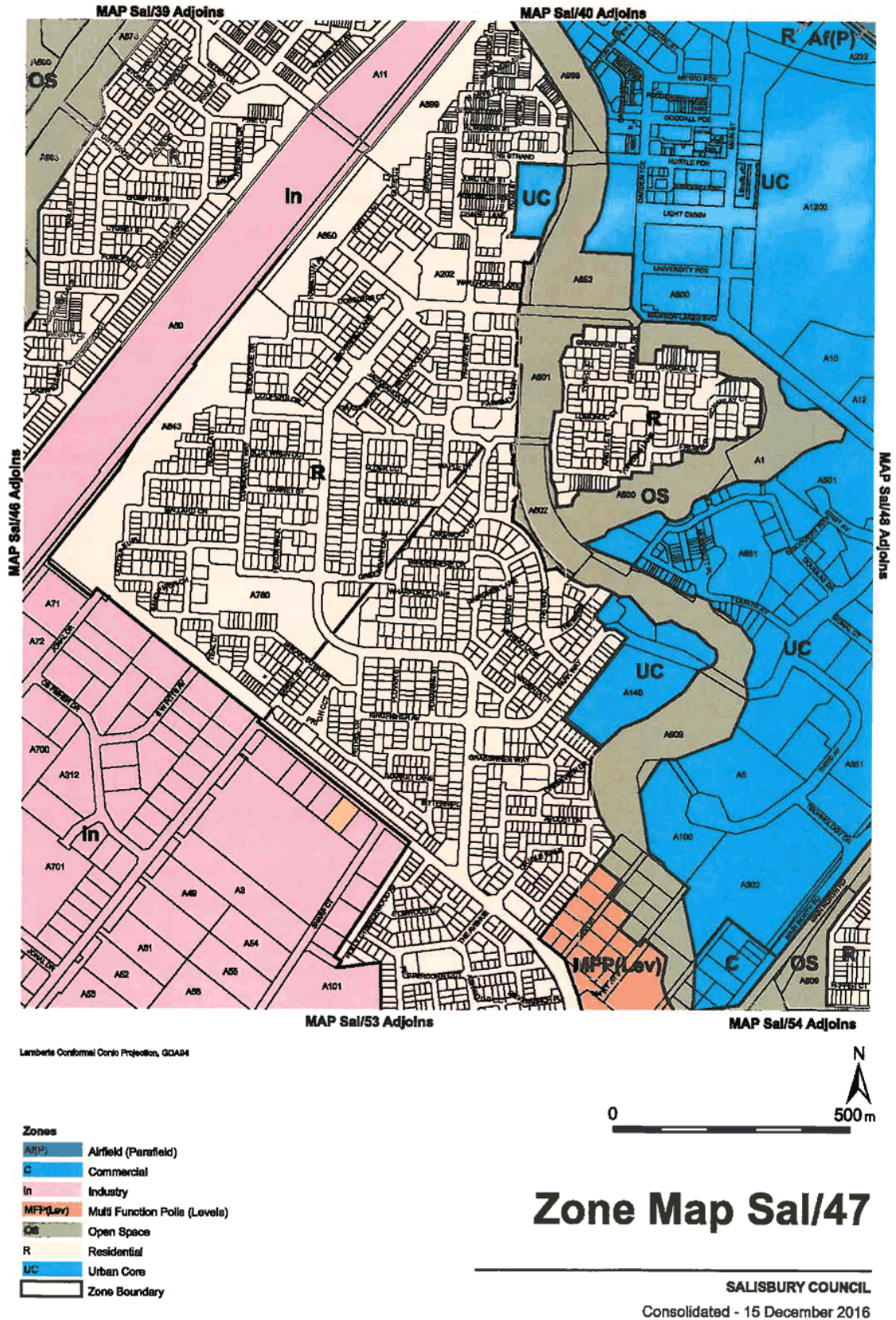
Heritage points are indicative only.  
For further information on State and Local Heritage Places and Contributory items please refer to the relevant tables within this document.

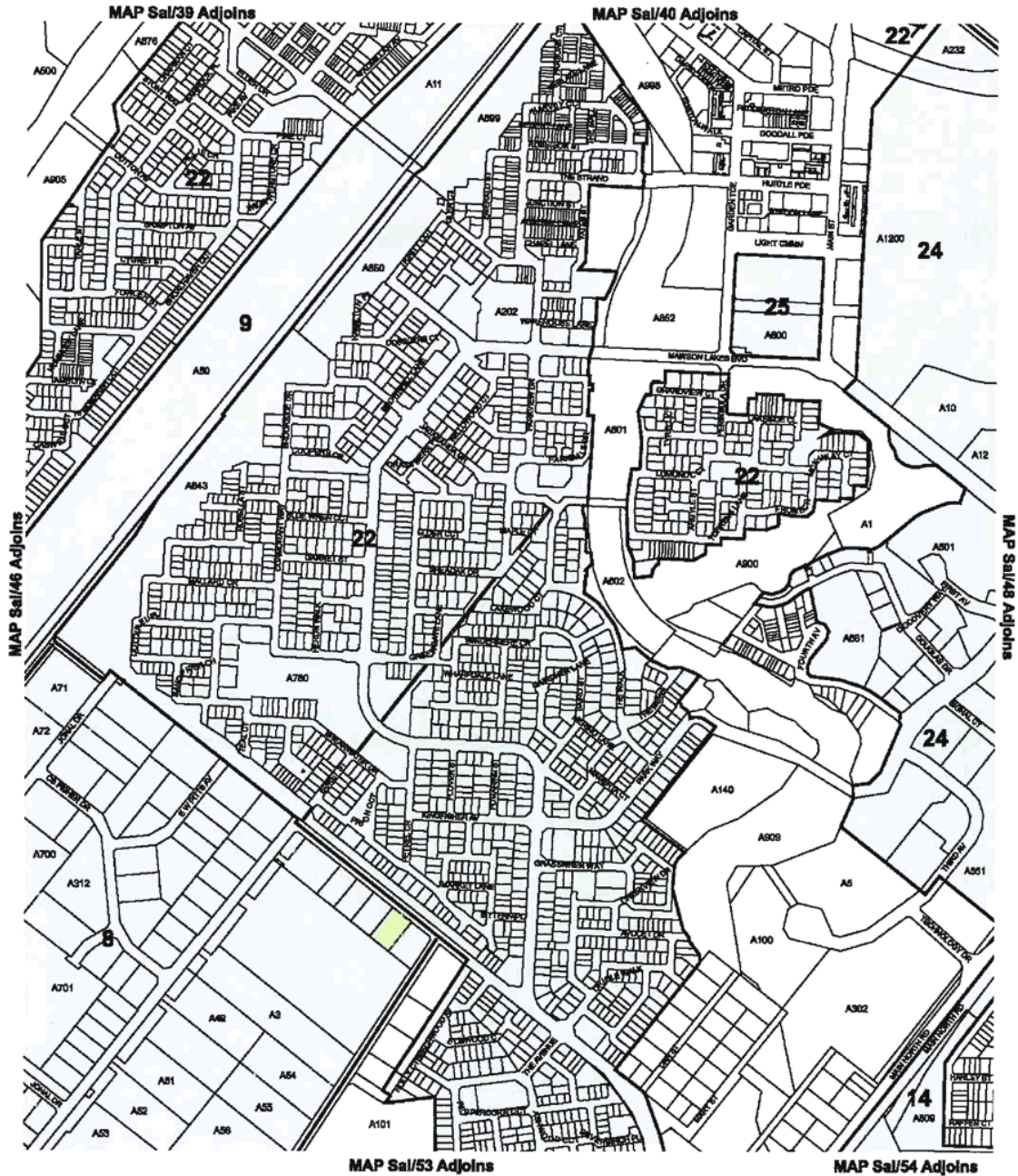


## Overlay Map Sal/47 HERITAGE

● State heritage place

**SALISBURY COUNCIL**  
Consolidated - 15 December 2016





Lamberts Conformal Conic Projection, GDAB4  
**Policy Area**  
 14 Landscape Buffer  
 22 Mewson Lakes  
 24 Mewson Innovation  
 25 Main Shopping  
 6 Greater Levels  
 9 Infrastructure

# Policy Area Map Sal/47

 Policy Area Boundary

**SALISBURY COUNCIL**  
 Consolidated - 15 December 2016

Item 5.1.4 - Attachment 4 - Relevant Development Plan Extracts and Location Maps consolidated 15 December 2016



<b>ITEM</b>	5.1.5
	<b>COUNCIL ASSESSMENT PANEL</b>
<b>DATE</b>	4 December 2018
<b>APPLICATION NO.</b>	361/1755/2018/2T
<b>APPLICANT</b>	City of Salisbury
<b>PROPOSAL</b>	Removal of a regulated tree on road reserve
<b>LOCATION</b>	Adjacent 3 Weemala Road, Salisbury East SA 5109
<b>CERTIFICATE OF TITLE</b>	Not applicable
<b>AUTHOR</b>	Karyn Brown, Development Officer Planning, City Development

### 1. DEVELOPMENT APPLICATION DETAILS

<b>Zone/Policy Area</b>	Residential Zone
<b>Application Type</b>	On-Merit
<b>Public Notification</b>	Representations received: Two Representations to be heard: One
<b>Referrals - Statutory</b>	None
<b>Referrals – Internal</b>	Independent arborist report
<b>Development Plan Version</b>	Salisbury (City) Development Plan Consolidated 15 December 2016
<b>Assessing Officer</b>	Karyn Brown, Development Officer - Planning
<b>Recommendation</b>	Approval with Conditions
<b>Meeting Date</b>	4 December 2018

### 2. REPORT CONTENTS

#### Assessment Report

Attachment 1:	Council Arboricultural Assessment and Supporting Documentation
Attachment 2:	Notice of Category 2 Development and Copies of Representations
Attachment 3:	Independent Arborist Report
Attachment 4:	Relevant Development Plan Provisions (Consolidated 15 December 2016)

### 3. EXECUTIVE SUMMARY

Council's Tree Services staff lodged a development application seeking Development Approval to remove a regulated Council street tree. The subject tree is a *Eucalyptus intertexta* (Inland Red Box) located adjacent to 3 Weemala Road, Salisbury East.

This report provides a detailed assessment of the Application against the relevant provisions of the Salisbury (City) Development Plan. The assessment found that the tree meets the provisions of the Council Development Plan to warrant approval to remove the tree, having regard to its health, structure and appearance.

Accordingly, it is recommended that Development Approval be granted.

#### 4. SUBJECT SITE

The site of the development comprises road verge in front of 3 Weemala Road, Salisbury East. The road reserve in this location is approximately 3.5m wide with the formed road carriageway being approximately 7m wide. The carriageway is sealed with upright kerb and gutter on both sides.

The tree is on the western side of the road. The tree is located 2.6m from the front boundary fence and 1.2m from the back of kerb.

The tree is approximately 13m north-west of the intersection with Arney Street and approximately 32m south-west of the bend in the road which becomes Glen Street. Photos of the tree are provided below.

**Photo 1.**  
*Looking south-west from the intersection of Weemala Road and Glen towards the subject tree*





**Photo 2.**  
*Looking north-east  
towards the subject  
tree*



**Photo 3.**  
*Looking north-east  
from the verge  
towards the subject  
tree*



**Photo 4.**  
*Looking north-west from the intersection of Weemala Road and Arney Street towards the subject tree*



**Photo 5.**  
*Looking north-east from the verge towards the trunk of the subject tree*



## 5. LOCALITY



The locality comprises a low density residential character formed predominantly of single storey detached dwellings, built mostly during the 1970s and sited upon large allotments. This reflects the original subdivision pattern for this area which remains largely intact.

Both sides of the street contain approximately the same amount of trees. The eastern side of Weemala Road does however two large eucalyptus trees on the verge in front of 10 Weemala Road.

A locality plan and contextual plan are provided below.

Locality Plan - Aerial



Legend (Source: Geocortex)	
	Subject tree
	Locality boundary

Locality Plan – Cadastre



Item 5.1.5



Legend (Source: Geocortex)	
	Subject tree
	Locality boundary
	Properties notified (occupiers within the subject site were also notified)
	Representations received

Contextual Plan



Legend (Source: Geocortex)	
	Subject tree
	Zone boundary

**6. DESCRIPTION OF THE PROPOSED DEVELOPMENT**

The application seeks Development Approval to remove one regulated *Eucalyptus intertexta* (Inland Red Box) tree which is located within the Council road reserve, in front of 3 Weemala Road, Salisbury.

The tree has a single trunk. The trunk circumference at 1.0m above ground level is 2.39m, meaning that the tree is of regulated size.

The tree has a height of 19.0m and a canopy spread of approximately 19.0m (north/south direction) and 16.0m (east/west direction).

A copy of the supporting documentation is contained in Attachment 1.

**7. CLASSIFICATION**

The site is located within the Residential Zone of the Salisbury (City) Development Plan, as consolidated 15 December 2016. Removal of a regulated tree is not listed as being either a Complying or Non-complying form of development in the Zone. Accordingly, the application was assessed as an on-merit form of development.

## 8. PUBLIC NOTIFICATION

The proposed development is not listed as being a Category 1 or Category 2 form of development in the Development Plan. However, Part 2, Clause 25 of Schedule 9 of the *Development Regulations 2008*, lists tree damaging activity (including removal) of a regulated tree on land owned or occupied by a Council where the Council is the relevant authority as a Category 2 form of development.

- 25 *Except where the activity is undertaken under Section 54A of the Act, any development which comprises a tree-damaging activity in relation to a regulated tree on land owned or occupied by a council where the council is the relevant authority in relation to the development.*

Therefore, the application was processed as a Category 2 form of development.

The Category 2 public notification process took place between 5 October 2018 and 18 October 2018. Two (2) representations were received during the public notification period, with one representation supporting the tree's removal and one representation opposing the tree's removal. The representors are listed below:

Representations received		
Representations received		Wish to be Heard
1	J J Monaghan 5 Weemala Road SALISBURY EAST SA 5109	√
2	J Dainty and L M Franklin 12 Weemala Road SALISBURY EAST SA 5109	

A copy of the Category 2 public notice and submissions received are contained in Attachment 2. The content of the representation are summarised in the table below. Please note that the applicant did not provide a response to the representations.

Summary of Representations	
Representation	
<i>J J Monaghan, 5 Weemala Road, Salisbury East</i>	
<ul style="list-style-type: none"> <li>• Opposes the removal of the tree.</li> <li>• Trees should only be removed for very compelling reasons.</li> <li>• Just because the tree is a "poor specimen" does not seem to be a very good reason for removal.</li> <li>• Spoke to an arborist from SA Power Networks who was trimming trees in the area and it was his opinion that removal was not necessary.</li> <li>• The tree has a magnificent canopy and provides a substantial habitat for a variety of birds.</li> <li>• The tree provides shade and contributes to the lessening of climate change.</li> </ul>	
<i>J Dainty and L M Franklin, 12 Weemala Road, Salisbury East</i>	
<ul style="list-style-type: none"> <li>• Supports the removal of the tree.</li> <li>• Believes that if the tree is declining then it should be removed for safety reasons.</li> </ul>	

## 9. REFERRALS – STATUTORY

The application was not subject to any referrals under Schedule 8 of the *Development Regulations 2008*.

## **10. REFERRALS – INTERNAL**

Council sought an independent tree report from Symatree. The report provides a detailed assessment of the health and structure of the tree. A copy of the report is contained in Attachment 3. The Symatree arborist concluded that:

*The tree is considered to be structurally unstable and has reached the end of its safe useful life.*

*The tree is not worthy of retention.*

*Removal of the tree is supported.*

Further analysis of the Symatree report is contained under the assessment section of this report.

## **11. ASSESSMENT**

A detailed assessment of the application has taken place against the relevant provisions of the Salisbury (City) Council Development Plan, as described below under headings.

An extract of the relevant provisions of the Development Plan are highlighted in Attachment 4.

### Appearance of the Tree

The Council-wide ‘Regulated Trees’ module in the Development Plan, Objective 2 states that:

- 2 *Development in balance with preserving regulated trees that demonstrate one or more of the following attributes:*
  - (a) *significantly contributes to the character or visual amenity of the locality*
  - (b) *indigenous to the locality*
  - (c) *a rare or endangered species*
  - (d) *an important habitat for native fauna.*

An assessment of the aesthetic benefits of the tree and its contribution to the character and visual amenity of the locality has taken place which involved a visual inspection of the tree.

The tree was observed as being a notable visual element in the locality, having regard to its 19.0m height and wide canopy spread.

Symatree has also formed the opinion that the tree significantly contributes to the character and visual amenity of the locality.

The Symatree arborist noted the following in their report:

*The height and spread of the subject tree's crown and location gives it a strong visual presence within the locality. Being a large mature specimen the subject tree does contribute to the character and visual amenity of the local area.*

#### Health and Structure of the Tree

Under the Council-wide 'Regulated Trees' module in the Development Plan, Principle of Development Control 2 states that:

- 2 *A regulated tree should not be removed or damaged other than where it can be demonstrated that one or more of the following apply:*
  - (a) *The tree is diseased and its life expectancy is short;*
  - (b) *The tree represents a material risk to public or private safety;*
  - (c) *The tree is causing damage to a building;*
  - (d) *Development that is reasonable and expected would not otherwise be possible;*
  - (e) *The work is required for the removal of dead wood, treatment of disease, or is in the general interests of the health of the tree.*

The Symatree report provides expert advice with respect to the health, life expectancy and structure of the tree. Key extracts relating to these matters are summarised from the report:

#### *Health and Life Expectancy*

*Although the subject tree is in good health, the tree has been assessed as having a poor structure which has compromised the tree's structural stability.*

*The subject tree has symptoms that suggest the tree is in structural decline with a safe useful life reduced to less than 5 years.*

*The tree is not worthy of retention and has reached the end of its safe useful life.*

#### *Structure*

*The main trunk presents as stable. No defects, borer damage or signs of decay. Overall tree health appears to be in good with minor volumes of dead wood noted.*

*The tree structure consists of tightly held codominant main stems and a secondary branch union with included bark unions. Both are considered significant structural defects. Attachment points with included bark do not form connective tissues between stems and are prone to failure or collapse. The bulging noted indicates these defects have extended right between these stems and the tree is laying reaction wood in an attempt to provide additional structural strength. Bulging also indicates these unions are quite weak and at higher risk of failure.*

*As these leaders continue to increase in diameter and length the leverage applied to the attachment points will also increase. The open nature of the canopy, due to past vegetation clearance has only exaggerated these defects and the likelihood of branch failure. Main leader failure is inevitable in the foreseeable future.*



Material risk to public or private safety

The Symatree report provides expert advice with respect to the material risk to public and private safety. Key extracts relating to these matters are summarised from the report:

*This tree is a significant risk to property and private safety due to the predisposing factors identified for failure or collapse and its location, extending over the footpath and road.*

*All reasonable remedial treatments and measures have been determined to be ineffective*

The tree contributes to the character and visual amenity of the locality in accordance with Objective 2(a). However, there are clear grounds which are applicable under Principles of Development Control 2(a) and (b) to warrant removal of the tree and the planting of a replacement tree.

**12. CONCLUSION**

This report has assessed an Application for the removal of a regulated Council street tree adjacent to 3 Weemala Road, Salisbury East. It is concluded that removal of the tree is warranted having regard to its health, structure and material risk to public and private safety.

Accordingly, it is recommended that Development Approval be granted.

**13. STAFF RECOMMENDATION**

That the Council Assessment Panel resolve that:

- A. The proposed development is not considered to be seriously at variance with the Salisbury Development Plan – Consolidated 15 December 2016.
- B. Pursuant to Section 33 of the *Development Act 1993*, Development Approval is **GRANTED** to application number 361/1755/2018/2T for Removal of a regulated tree on road reserve in accordance with the plans and details submitted with the application and subject to the following conditions:

Development Plan Consent Conditions

1. The development shall be carried out in accordance with the details submitted with the application and the following stamped approved plans and documents, except where otherwise varied by the conditions herein:

Drawing No.	Document Type	Date	Prepared By
File Number: DA 361/1755/2018/2T	Tree Assessment 3 Weemala Road, Salisbury East	Date Received Stamped by Council 31 October 2018	Symatree

*Reason: To ensure the proposal is established in accordance with the submitted plans and documents.*

2. That semi-mature native tree species (>1m in height) indigenous to the local area shall be planted on a two for one basis to compensate for the removal of the regulated tree. The replacement tree planting shall be maintained in good condition at all times and replaced if necessary.

*Reason: To comply with requirements of Regulation 117 (2) of the Development Regulations 2008.*

### **CO-ORDINATION**

Officer: GMCiD MDS  
Date: 19.11.18 14.11.18

### **ATTACHMENTS**

This document should be read in conjunction with the following attachments:

1. Council Arboricultural Assessment and Supporting Documentation
2. Notice of Category 2 Development and Copies of Representations
3. Independent Arborist Report
4. Relevant Development Plan Provisions (Consolidated 15 December 2016)

# **Attachment 1**

## Council Arboricultural Assessment and Supporting Documentation



## CITY OF SALISBURY DEVELOPMENT APPLICATION FORM

**361/                      /2016/**

Please use **BLOCK LETTERS** and **Black or Blue Ink**

APPLICANT NAME:		City Infrastructure	
POSTAL ADDRESS:			
OWNER NAME: (This must be completed)		City of Salisbury	<input type="checkbox"/> as above
OWNER POSTAL ADDRESS:			<input type="checkbox"/> as above
OWNER PHONE NO:	OWNER FAX NO:	OWNER EMAIL:	
<b>CONTACT PERSON FOR FURTHER INFORMATION</b>			<input type="checkbox"/> as above
NAME:		TELEPHONE (W):	
Kevin Davey		(M): 0401 984 772	
EMAIL:		FAX:	
BUILDER NAME:		BUILDERS EMAIL:	
N/A			
BUILDER POSTAL ADDRESS:		CONTACT NO.:	LICENCE NO.:
CURRENT USE OF PROPERTY:			
DESCRIPTION OF PROPOSAL:		DEVELOPMENT COST	
Tree removal		\$	
<b>LOCATION OF PROPOSAL</b>			
Street No: 3	Street: Weemala Road	Suburb: Salisbury East	
Lot No:	Section:	Plan:	Volume: Folio:
<b>OFFICE USE ONLY</b>			
Registration Date:	/ /2016	Zone:	Ward:
<b>BUILDING RULES CLASSIFICATION</b>			
Classification sought:			
If Class 5, 6, 7, or 9 classification is sought, state the proposed number of employees			Male: Female:
If Class 9a classification is sought, state the number of persons for whom accommodation is provided:			
If Class 9b classification is sought, state the proposed number of occupants of the various spaces at the premises:			

I acknowledge that copies of this application and supporting documentation may be provided to interested persons in accordance with the Development Regulations, 1993. Developments requiring public notification will be made available to the public for comment via Council's web site at [www.salisbury.sa.gov.au](http://www.salisbury.sa.gov.au)

SIGNATURE: \_\_\_\_\_

DATE: 02 / 10 / 2018

## CITY OF SALISBURY DEVELOPMENT APPLICATION

### Arboricultural assessment of a *Eucalyptus intertexta* in relation to removal of a street tree.

### Summary

The *Eucalyptus intertexta* (Coolibah) located within the verge at the front of 3 Weemala Road, Salisbury East qualifies as a regulated tree.



## APPLICATION NUMBER:

## Details

DA NUMBER	
DATE OF INSPECTION	29 <sup>th</sup> August 2018
SITE	3 Weemala Road, Salisbury East
CONTACT	Kevin Davey
LOCATION	The tree is located within the Council verge and is the central specimen.
REASON FOR REQUEST	The tree displays a declining structure.
REGULATED OR SIGNIFICANT	Regulated Tree

## Species

*Eucalyptus intertexta* (Coolibah)

## Tree dimensions

Height	Approximately 17 metres
Crown spread	Approximately 10 metres
Trunk circumference 1 metre above ground level	2.40 metres
Age class of tree	Mature

## Visual Appearance:

Due to SA Power Networks line clearance pruning, the tree currently has moderate amenity value within the local area.

**Tree health**

This tree currently displays fair health with typical foliage density and colour.

**Tree structure**

Tree structure consists of a single trunk that develops into several main stems at above one metre from ground level. The primary union and secondary union are poor in form and display bark inclusion, (as shown).



## Development Assessment

### *Regulated Trees*

#### **OBJECTIVES**

1. The conservation of regulated trees that provide important aesthetic and/or environmental benefit.
2. Development in balance with preserving regulated trees that demonstrate one or more of the following attributes:

	<b>Objective</b>	<b>Satisfied</b>	<b>Comments</b>
a)	significantly contributes to the character or visual amenity of the locality	No	The tree provides moderate amenity value
b)	indigenous to the locality	No	This species is local to the eastern states
c)	a rare or endangered species	No	
d)	an important habitat for native fauna	No	

#### **PRINCIPLES OF DEVELOPMENT CONTROL**

1. Development should have minimum adverse effects on regulated trees.
2. A regulated tree should not be removed or damaged other than where it can be demonstrated that one or more of the following apply:

	<b>Principle of Development Control</b>	<b>Satisfied</b>	<b>Comments</b>
a)	The tree is diseased and its life expectancy is short;	Yes	The tree has a structural flaw that will reduce its life expectancy
b)	The tree represents a material risk to public or private safety;	Yes	The tree displays a compromised structure
c)	The tree is causing damage to a building;	No	
d)	Development that is reasonable and expected would not otherwise be possible;	N/A	
e)	The work is required for the removal of dead wood, treatment of disease, or is in the general interests of the health of the tree.	No	

Tree damaging activity other than removal should seek to maintain the health, aesthetic appearance and structural integrity of the tree.

### Appraisal

The tree displays poor form and associated poor structure. This is evident by the poor primary and secondary unions.



## Recommendation

Complete removal of the tree is recommended for the following reasons:

- The tree is a poor specimen that displays declining structure.
- Practical remedial measures are not available to reduce or reverse this.
- The Parks and Open Space Assets team supports the removal of this tree.

Kevin Davey  
Technical Officer  
Parks & Open Space Assets

Tree location



**Attachment 2**  
Notice of Category 2 Development  
and Copies of Representations

**DEVELOPMENT ACT 1993  
CITY OF SALISBURY**

**NOTICE OF APPLICATION FOR CATEGORY 2 DEVELOPMENT**  
Pursuant to Section 38(4) of the *Development Act 1993*

An application for development has been lodged with the Council for assessment. The details are as follows:

<b>APPLICATION NO:</b>	361/1755/2018/2T
<b>APPLICANT:</b>	City of Salisbury PO Box 8 SALISBURY SA 5108
<b>NATURE OF DEVELOPMENT:</b>	Removal Of A Regulated Tree On Road Reserve
<b>LOCATION:</b>	Adjacent 3 Weemala Road, Salisbury East SA 5109
<b>CERTIFICATE OF TITLE:</b>	Road Reserve
<b>ZONE:</b>	Residential

The application may be examined at the Office of the Council located at 12 James Street, Salisbury during normal business hours (8.30am – 5pm Monday to Friday) and on Council's web site at [www.salisbury.sa.gov.au](http://www.salisbury.sa.gov.au). Any person or body may make representations in writing, or by email [development@salisbury.sa.gov.au](mailto:development@salisbury.sa.gov.au), concerning this application and should address their representation to the Chief Executive Officer at PO Box 8, Salisbury or [representations@salisbury.sa.gov.au](mailto:representations@salisbury.sa.gov.au). Representations must be received **no later than Thursday 18<sup>th</sup> October 2018**.

Each person making a submission should indicate whether they wish to appear personally, or be represented by another party, in support of their submission. Please note that should you nominate to be heard in support of your representation, you will be required to attend a Development Assessment Panel meeting held at the Council offices, scheduled on the fourth Tuesday of each month at 6.00pm (unless otherwise advised).

Please note that pursuant to Section 38(8) of the *Development Act 1993*, a copy of each representation received will be forwarded to the applicant to allow them to respond to all representations received.

This development is classified as a Category 2 development under the Development Act. Please be aware that there is no right of appeal against Council's decision.

Signed: Karyn Brown, Development Officer

Date: 5 October 2018

***THIS IS THE FIRST AND ONLY PUBLICATION OF THIS NOTICE***

CATEGORY 2



**STATEMENT OF REPRESENTATION**  
Pursuant to Section 38 of the *Development Act 1993*

16 OCT 2018

To: City of Salisbury  
PO Box 8, SALISBURY SA 5108  
Email: [representations@salisbury.sa.gov.au](mailto:representations@salisbury.sa.gov.au)

THIS SHEET PROVIDES YOU WITH THE OPPORTUNITY TO MAKE COMMENTS IN RELATION TO A PROPOSED DEVELOPMENT. PLEASE FIND ATTACHED DETAILS OF THE PROPOSED DEVELOPMENT.

<b>Application No:</b>	361/1755/2018/2T
<b>Applicant:</b>	City of Salisbury
<b>Location:</b>	Adjacent 3 Weemala Road, Salisbury East SA 5109
<b>Nature of Development:</b>	Removal Of A Regulated Tree On Road Reserve

**YOUR DETAILS:** (this information must be provided to ensure that this is a valid representation)

NAME(S): JOHN MONAGHAN

ADDRESS: 5 WEEMALA RD SALISBURY EAST 5109

PHONE NO: [REDACTED] EMAIL: [REDACTED]

I am: (please tick one of the following boxes as appropriate)

- The owner/occupier of the property located at: 5 WEEMALA RD SALISBURY EAST
- Other (please state): .....

**YOUR COMMENTS:**

I/We: (please tick the most appropriate box below)

- Support the proposed development.
- Oppose the proposed development.

**Whether you support or oppose this proposal you must provide written reasons below to ensure that this is a valid representation.**

.....  
.....  
.....  
.....

PTO

**I do not support this proposal.**

I strongly believe that trees should only be removed for very compelling reasons, either that they are a danger to people or are causing harm to property. It appears that neither of these are an issue in this instance.

Just because it is a "poor specimen" does not seem to be a very good reason for removal. If poor specimens of all things were removed the human race would be substantially depleted! (it would certainly alleviate the problems in our hospitals). For a poor specimen it is a beautiful looking tree especially when it catches the evening sun.

Recently SA Power networks trimmed the trees in this area, removing limbs that could be dangerous or causing problems with power lines. I spoke to the arborist who trimmed the tree in question because I thought it was going to be removed and he indicated that in his opinion removal was not necessary. It is not the sort of tree that would without warning shed limbs in hot weather.

The tree has a magnificent canopy and provides a substantial habitat for a variety of parrots and other birds. It also provides a lot of shade and keeps the area around it cool in hot weather. It also contributes to lessening of climate change by absorbing carbon dioxide and providing a valuable source oxygen back into the atmosphere.

In January this year a tree on the other side of the road in question a beautiful looking ghost gum was removed because it dropped two limbs inside of of few weeks was removed. Although it was sad to see a tree that had taken almost 50 years to grow taken down in less than a day, I can understand the reason it was removed.

I am firm believer that we should be planting more trees not removing them unless strictly necessary.

Coincidentally an article supporting just this belief appeared in The Advertiser on 12 October. I am enclosing a copy of this article in support of my submission.

**My concerns would be addressed by:**

Regular monitoring of the tree to ensure that it is properly trimmed and not a danger to people or property.

# Extra trees to cool climate change fears

**SIMEON THOMAS-WILSON  
CITY EDITOR**

DEVELOPING metropolitan Adelaide into an "urban forest" will help address climate change concerns and urban heating, says a global city park expert.

Jayne Miller, the chair of World Urban Parks and the chief of the Pittsburgh Parks Conservancy, said there were many benefits of boosting tree coverage in Adelaide.

"There are a number of areas in Adelaide that I've seen where the tree canopy is not what it should be," she said.

"This is something that governments should be looking at as it reduces temperature and urban heat and also reduces power usage."



Adelaide is hosting the National Parks Cities Forum at the National Wine Centre.

Dan Raven-Ellison, the founder of the national park city concept, said London now had more trees than people and Melbourne was making big strides in the area.

"It isn't just the governments who are pushing it, it can be something as simple as people making more of their gardens," he said.

Minister for Environment

and Water David Speirs, pictured, said the forum was a great opportunity for SA to showcase its green city vision having shifted focus from the CBD to metropolitan Adelaide.

"Green Adelaide will deliver natural resource management functions in metropolitan Adelaide, focusing on enhancing the city's urban ecology and investing in the natural environment to improve overall community wellbeing," he said.

ADVE01201MA - VI

*THE ADVERTISER 12/10/18*

CATEGORY 2

Regulation 35(e) of the *Development Regulations 2008* requires that a representation must indicate a person's desire to be heard. Please note that if you do not indicate that you wish to be heard, it will be taken that you do not wish to be heard by the Panel.

I/We:

- Do not wish to be heard in support of my representation.
- Wish to be heard in support of my representation, and I will be:

Appearing personally,

OR

Represented by the following person: ..... DAMIEN PILKINGTON .....

Contact details: Po box 216 Salisbury SA 5708 ..... [REDACTED]

*(Please note, matters raised in your written representation will be considered during the assessment and do not need to be repeated at the hearing).*

**Your written representation must be received by Council no later than 11.59pm on Thursday 18<sup>th</sup> October 2018, to ensure that it is a valid representation and taken into account.**

**Representor's Declaration:**

I am aware that the representation will become a public document as prescribed in the *Freedom of Information Act 1991*, and will be made available to the applicant, agencies and other bodies pursuant to the *Development Act 1993*, and may be uploaded to the Council's website as an attachment to the hearing agenda.

Signature: .....  .....

Date: 15/10/18

**Please complete this checklist to ensure your representation is valid:**

- Name and address of person (or persons).
- If more than one person, details of person making the representation.
- Detail of reasons for making the representation.
- Indication whether or not the person (or persons) wishes to be heard.
- Submitted no later than 11.59pm on **Thursday 18<sup>th</sup> October 2018**.





**STATEMENT OF REPRESENTATION**  
Pursuant to Section 38 of the *Development Act 1993*

To: City of Salisbury  
PO Box 8, SALISBURY SA 5108  
Email: [representations@salisbury.sa.gov.au](mailto:representations@salisbury.sa.gov.au)

THIS SHEET PROVIDES YOU WITH THE OPPORTUNITY TO MAKE COMMENTS IN RELATION TO A PROPOSED DEVELOPMENT. PLEASE FIND ATTACHED DETAILS OF THE PROPOSED DEVELOPMENT.

<b>Application No:</b>	361/1755/2018/2T
<b>Applicant:</b>	City of Salisbury
<b>Location:</b>	Adjacent 3 Weemala Road, Salisbury East SA 5109
<b>Nature of Development:</b>	Removal Of A Regulated Tree On Road Reserve

**YOUR DETAILS:** (this information must be provided to ensure that this is a valid representation)

NAME(S): Jean Dainty & Lorraine Franken  
ADDRESS: 12 Weemala Rd Salisbury East 5109  
PHONE NO: [REDACTED] EMAIL: [REDACTED]

I am: (please tick one of the following boxes as appropriate)

- The owner/occupier of the property located at: 12 Weemala Rd
- Other (please state): .....

**YOUR COMMENTS:**

~~I/We:~~ (please tick the most appropriate box below)

- Support the proposed development.
- Oppose the proposed development.

**Whether you support or oppose this proposal you must provide written reasons below to ensure that this is a valid representation.**

If the tree is declining it should be removed for safety reasons.

PTO

10 OCT 2018



Regulation 35(e) of the *Development Regulations 2008* requires that a representation must indicate a person's desire to be heard. Please note that if you do not indicate that you wish to be heard, it will be taken that you do not wish to be heard by the Panel.

I/We:

Do not wish to be heard in support of my representation.

Wish to be heard in support of my representation, and I will be:

Appearing personally,

**OR**

Represented by the following person: .....

Contact details: .....

*(Please note, matters raised in your written representation will be considered during the assessment and do not need to be repeated at the hearing).*

**Your written representation must be received by Council no later than 11.59pm on Thursday 18<sup>th</sup> October 2018, to ensure that it is a valid representation and taken into account.**

**Representor's Declaration:**

I am aware that the representation will become a public document as prescribed in the *Freedom of Information Act 1991*, and will be made available to the applicant, agencies and other bodies pursuant to the *Development Act 1993*, and may be uploaded to the Council's website as an attachment to the hearing agenda.

Signature: ..... *[Handwritten Signature]* .....

Date: 7 / 10 / 2018

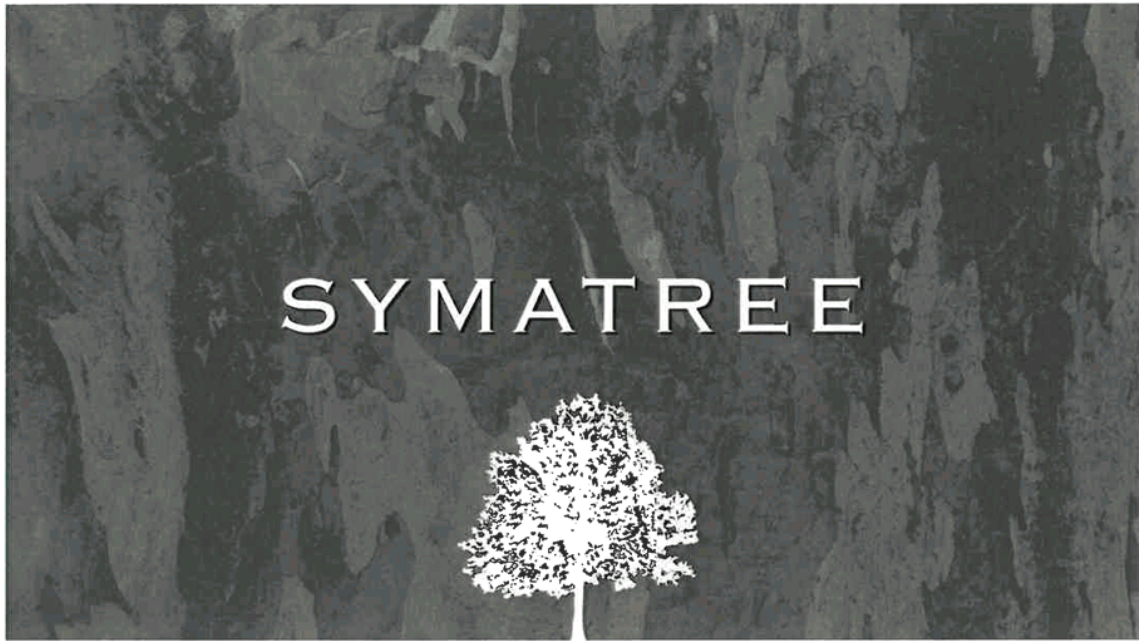
**Please complete this checklist to ensure your representation is valid:**

- Name and address of person (or persons).
- If more than one person, details of person making the representation.
- Detail of reasons for making the representation.
- Indication whether or not the person (or persons) wishes to be heard.
- Submitted no later than 11.59pm on **Thursday 18<sup>th</sup> October 2018.**



# **Attachment 3**

## **Independent Arborist Report**



**Tree Assessment**  
**3 Weemala Road, Salisbury East**  
File Number: DA 361/1755/2018/2T

**Report prepared for**

Ms Karyn Brown  
Development Officer Planning  
City of Salisbury  
October 2018

**Report prepared by**

Sam Cassar

Cert. (Hort 3), Dip. (Hort 5), Dip (Arb 5), B.App.Sc (Hort), Grad. Dip. Design (Land.)



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Appraisal .....	7
Objectives and Principles of Development Control.....	8
Conclusion .....	10
Appendix A - Aerial Image .....	11

## Introduction

---

### Instructions

I was instructed by Council to assess a *Eucalyptus intertexta* located within the road reserve in front of 3 Weemala Road, Salisbury East. My brief was to undertake the following:

- Assess the general health and structure of the tree;
- Determine if the tree is regulated or significant; and
- Assess the tree against the relevant provisions of Council's Development Plan.

### Site Visit

An inspection was carried out on the 30 October 2018.

### Documents and Information provided

Council provided a copy of the development application form, tree report from Council and representations.

### Limitations

This report is limited to the time and method of inspection. The tree was inspected from ground level only. Neither a climbing inspection or a below-ground investigation was performed. No soil or plant material samples were taken for laboratory analysis.

This report reflects the state of the tree as found on the day. Any changes to site conditions or surrounds, such as construction works undertaken after the inspection, may alter the findings of the report.

The inspection period to which this report applies is three months from the date of the site visit, on the basis that current site conditions remain unchanged.

### Date of Report

This report was written on the 30 October 2018.



## Observations

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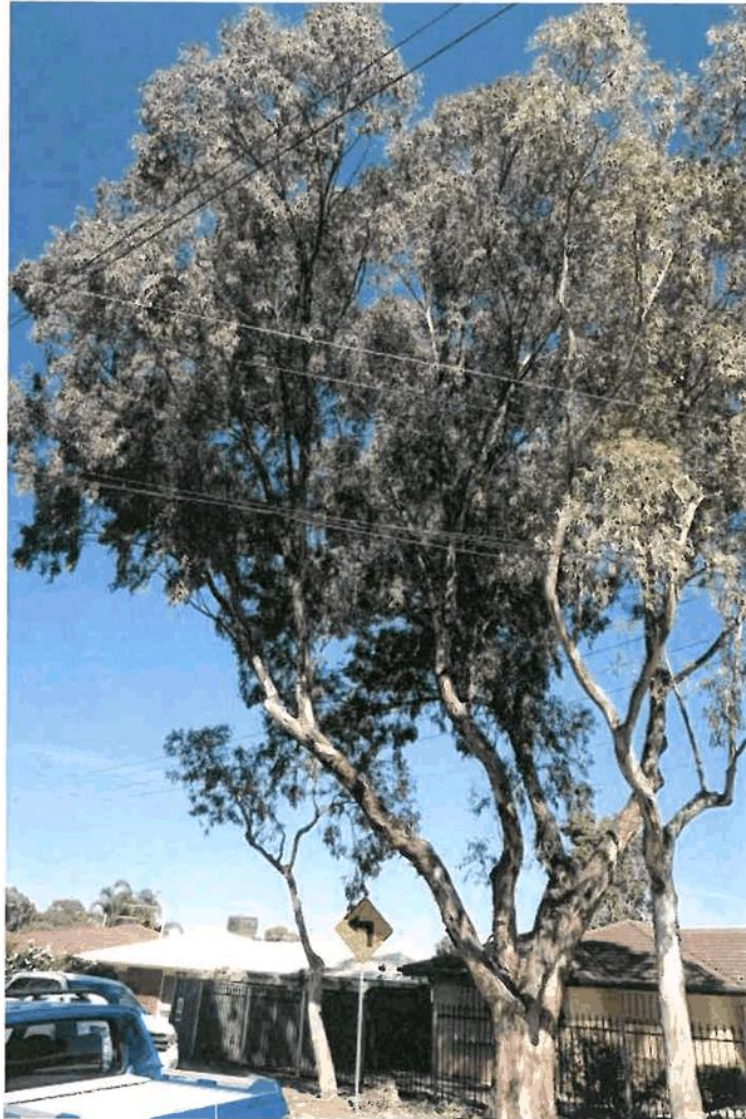


Figure 1. Subject tree, viewed from the northeast.

### Location of tree

The trunk centre is 2.6 metres from the front boundary fence and 1.2 metres from the back of kerb (refer Figure 1).

The trees' approximate location is identified on the aerial image listed in Appendix A.

## **Observations** (cont.)

---

### **Species**

*Eucalyptus intertexta*, commonly known as Inland Red Box.

### **Crown attributes**

Height: 19 metres (clinometer)

Width (canopy extent): 19 metres (north/ south direction) and 16 (east/ west direction).

### **Circumference at one metre above natural ground level**

Single trunk: 2.39 metres

The tree qualifies as a 'regulated' tree under the Development (Regulated Trees Variation) Regulations 2011.

### **Tree Health and Structure**

The subject tree divides between 2 -3 metres from ground into 4 main leaders to form an upright irregular shape crown that has a slight bias towards the east.

The main trunk presents as stable. No defects, borer damage or signs of decay. Overall tree health appears to be in good with minor volumes of dead wood noted.

The tree does not display a history of significant branch failure. The tree has been heavily pruned in the past due to clearance pruning from adjacent private property and overhead low voltage conductors with a number of pruning wounds apparent. Epicormic growth is noted at some of these cuts in response to this pruning.

A severe bark inclusion that is tightly held is noted between the main leaders (refer Figure 2). Another severe bark inclusion is noted, first attachment north-western main leader (refer Figure 3). Bulging and reactionary wood is present at the base of these attachment points.

A number of poorly weighted branches with poor taper are present throughout the crown. Some overextension issues are also apparent.

### **Property Damage**

There is no visible evidence of notable damage to adjacent private property.

**Observations** (cont.)

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Figure 3. Included bark union, main leaders, circled in red. Tree viewed from the west.



Figure 3. Included bark union, first attachment north-western main leader, circled in red. Tree viewed from the west.

## Appraisal

---

The subject tree is considered regulated under the Development (Regulated Trees Variation) Regulations 2011.

Although the subject tree is in good health, the tree has been assessed as having a poor structure which has compromised the tree's structural stability.

The tree structure consists of tightly held codominant main stems and a secondary branch union with included bark unions. Both are considered significant structural defects. Attachment points with included bark do not form connective tissues between stems and are prone to failure or collapse. The bulging noted indicates these defects have extended right between these stems and the tree is laying reaction wood in an attempt to provide additional structural strength. Bulging also indicates these unions are quite weak and at higher risk of failure.

As these leaders continue to increase in diameter and length the leverage applied to the attachment points will also increase. The open nature of the canopy, due to past vegetation clearance has only exaggerated these defects and the likelihood of branch failure. I consider that main leader failure is inevitable in the foreseeable future.

This tree is a significant risk to property and private safety due to the predisposing factors identified for failure or collapse and its location, extending over the footpath and road.

Aside from complete removal, reduction/thinning to reduce the load and hence the leverage placed on the defective unions would be the main method of alleviating the future risk of branch failure to address the defects identified. However given the defects are located within the main branching framework any pruning is considered ineffective.

## Objectives and Principles of Development Control

---

The subject tree has been identified as a Regulated Tree. The following comments have been made in regards to the relevant Objectives and Principles of Council's Development Plan (December 2016):

### Objectives

1. The conservation of regulated trees that provide important aesthetic and/or environmental benefit.
2. Development in balance with preserving regulated trees that demonstrate one or more of the following

- (a) significantly contributes to the character or visual amenity of the locality

Yes: The height and spread of the subject tree's crown and location gives it a strong visual presence within the locality. Being a large mature specimen the subject tree does contribute to the character and visual amenity of the local area.

- (b) indigenous to the locality

No: The species, *Eucalyptus intertexta* are not a local indigenous native species to the Adelaide Plains.

- (c) a rare or endangered species

No: The species (*Eucalyptus intertexta*) is not considered rare or endangered.

- (d) An important habitat for native fauna.

No: There is no evidence to indicate the tree is an important habitat for native fauna, no hollows suitable for nesting were observed within the crown of the subject tree

### Principles of Development Control (cont)

---

#### Principles of Development Control

1. Development should have minimum adverse effects on regulated trees.
2. A regulated tree should not be removed or damaged other than where it can be demonstrated that one or more of the following apply:
  - (a) the tree is diseased and its life expectancy is short

Yes: The subject tree has symptoms that suggest the tree is in structural decline with a safe useful life reduced to less than 5 years.
  - (b) the tree represents a material risk to public or private safety

Yes: The subject tree represents a material risk to public safety given the nature of the defects observed.
  - (c) the tree is causing damage to a building

No: There is no evidence to indicate the tree is causing damage to a building.
  - (d) development that is reasonable and expected would not otherwise be possible

N/A
  - (e) The work is required for the removal of dead wood, treatment of disease, or is in the general interests of the health of the tree.

## Conclusion

---

I conclude that the subject tree, a *Eucalyptus intertexta* is considered to be structurally unstable and reached the end of its safe useful life.

On the basis of the factors outlined, I consider that this tree is not worthy of retention and that **removal is therefore supported.**

With reference to Council's Development Plan, I consider that approval to remove this tree is legitimate in this case in line with Principle 2 (a) (b) of the Plan which states that:

Regulated trees should be preserved and tree-damaging activity should not be undertaken unless:

- (a) The tree is diseased and its life expectancy is reduced.
- (b) The subject tree represents a material risk to public safety given the nature of the defects observed.

All other reasonable remedial treatments and measures have been determined to be ineffective.

Thank you for the opportunity in providing this report. Should you have any questions or require further information, please do not hesitate in contacting me.



**Sam Cassar**

## Appendix A Aerial Image

3 Weemala Road, Salisbury East

Page 11 of 12







**Attachment 4**  
Relevant Development Plan  
Provisions  
(Consolidated 15 December 2016)



**Salisbury Council**

Consolidated – 15 December 2016

Please refer to the Salisbury Council page at [www.sa.gov.au/developmentplans](http://www.sa.gov.au/developmentplans) to see any amendments not consolidated.



**Government of South Australia**  
Department of Planning,  
Transport and Infrastructure

Consolidated - 15 December 2016

Salisbury Council  
General Section  
Regulated Trees

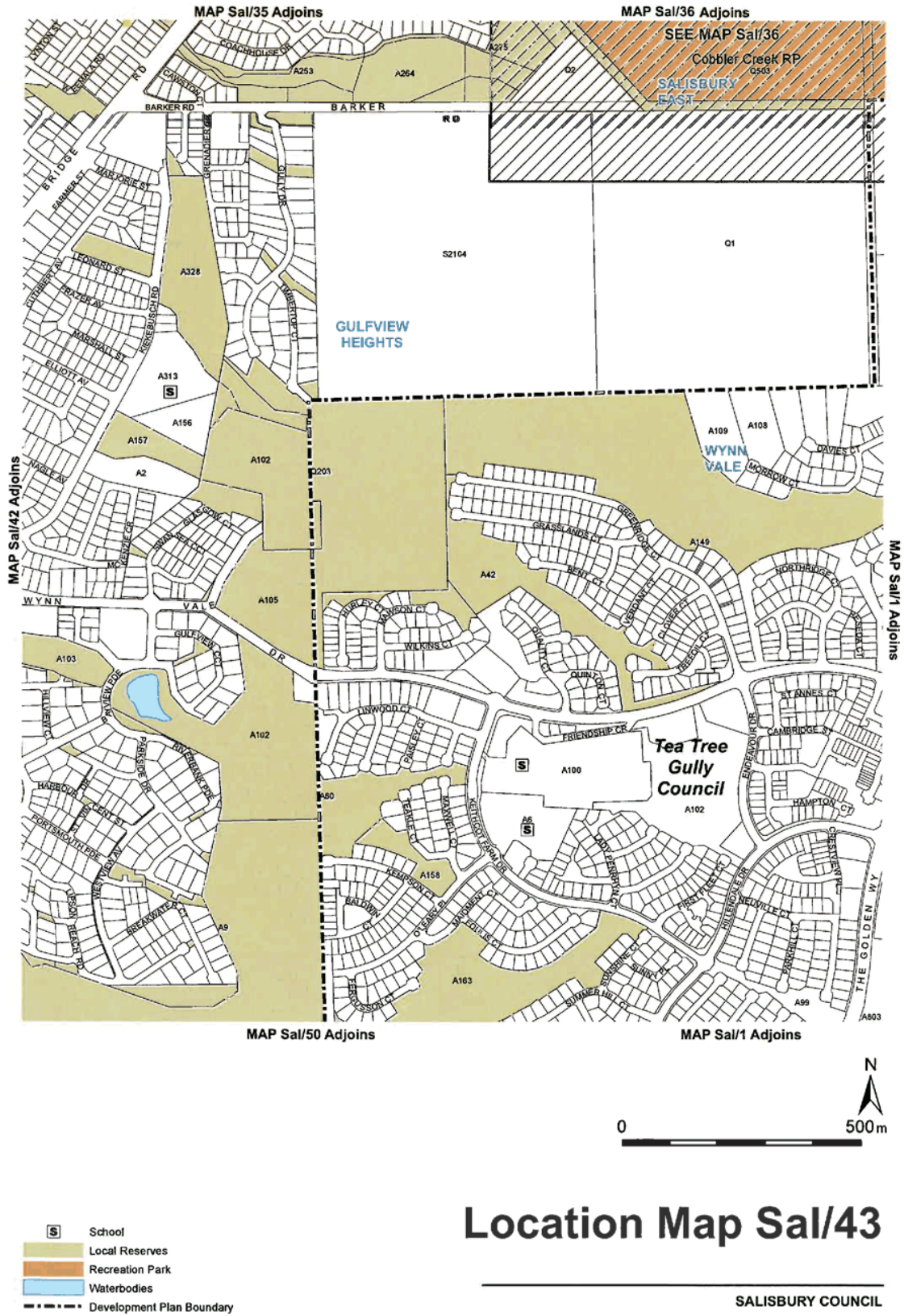
## Regulated Trees

### OBJECTIVES

- 1 The conservation of regulated trees that provide important aesthetic and/or environmental benefit.
- 2 Development in balance with preserving regulated trees that demonstrate one or more of the following attributes:
  - (a) significantly contributes to the character or visual amenity of the locality
  - (b) indigenous to the locality
  - (c) a rare or endangered species
  - (d) an important habitat for native fauna.

### PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development should have minimum adverse effects on regulated trees.
- 2 A regulated tree should not be removed or damaged other than where it can be demonstrated that one or more of the following apply:
  - (a) the tree is diseased and its life expectancy is short
  - (b) the tree represents a material risk to public or private safety
  - (c) the tree is causing damage to a building
  - (d) development that is reasonable and expected would not otherwise be possible
  - (e) the work is required for the removal of dead wood, treatment of disease, or is in the general interests of the health of the tree.
- 3 Tree damaging activity other than removal should seek to maintain the health, aesthetic appearance and structural integrity of the tree.



## Location Map Sal/43

SALISBURY COUNCIL  
Consolidated - 15 December 2016

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<b>ITEM</b>	5.2.1
	<b>COUNCIL ASSESSMENT PANEL</b>
<b>DATE</b>	04 December 2018
<b>HEADING</b>	Appointment of an Acting Presiding Member
<b>AUTHOR</b>	Chris Zafiroopoulos, Manager Development Services, City Development
<b>CITY PLAN LINKS</b>	4.3 Have robust processes that support consistent service delivery and informed decision making.
<b>SUMMARY</b>	The Council Assessment Panel is required to appoint an Acting Presiding Member.

### RECOMMENDATION

1. That \_\_\_\_\_ be appointed as the Acting Presiding Member to the Council Assessment Panel for the term to 31 May 2019, and the General Operating Procedures be updated to reflect the appointment.

### ATTACHMENTS

There are no attachments to this report.

## 1. BACKGROUND

- 1.1 As a consequence of the Council Elections, an Elected Member will be appointed to the Council Assessment Panel on the 26<sup>th</sup> November 2018 as part of the general appointments of members to committees following a general election. It is necessary for the Council Assessment Panel to appointment a new Acting Presiding Member, given the previous Acting Presiding Members was an Elected Member. The role of the Acting Presiding Member is to deputise for the Presiding Member when the Presiding Member is unavailable for a meeting.
- 1.2 The Planning, Development and Infrastructure Act 2016 provides that Council should establish a process for appointing an Acting Presiding Member to the Panel. The Council has resolved that the CAP determine the appointment of the Acting Presiding Member in its procedures.
- 1.3 The Panel has established a procedure in clauses 3.6 and 3.7 of its General Operating Procedures. The procedure requires the appointment of the Acting Presiding Member by resolution of the Panel. In the event both the Presiding Members and Acting Presiding Member are absent, the appointment will be made by resolution of those members present.

## 2. REPORT

- 2.1 The Acting Presiding Member has the same powers and duties of the Presiding Member when required to undertake the role. The role is important in the chairing of the meeting, including providing guidance to the other members on the Panel,

representors and the gallery in respect to the considerations of the Panel. The Panel should give consideration to the appointment of a person with the knowledge and experience for this role.

- 2.2 As the term of the Independent Members on the Council Assessment Panel will conclude on 31 May 2019, it is recommended that the Acting Presiding Member be appointed for this period.

**3. CONCLUSION / PROPOSAL**

- 3.1 That the Panel appoints the Acting Presiding Member to the Council Assessment Panel.

**CO-ORDINATION**

Officer: GMCiD  
Date: 19.11.18



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<b>ITEM</b>	5.2.2
	<b>COUNCIL ASSESSMENT PANEL</b>
<b>DATE</b>	04 December 2018
<b>HEADING</b>	Planning Reforms - Accredited Professionals Scheme Draft Regulations
<b>AUTHOR</b>	Chris Zafiroopoulos, Manager Development Services, City Development
<b>CITY PLAN LINKS</b>	4.3 Have robust processes that support consistent service delivery and informed decision making.
<b>SUMMARY</b>	The report provides information regarding the proposed <i>Accredited Professionals Scheme Draft Regulations</i> and seeks endorsement of the submission that was made to the draft regulations on behalf of the Panel.

### RECOMMENDATION

1. The report on the *Accredited Professionals Scheme Draft Regulations* be noted and the submission on behalf of the Assessment Panel dated 22 October 2018 forming an attachment to the agenda report be endorsed.

### ATTACHMENTS

This document should be read in conjunction with the following attachments:

1. Accredited Professionals Scheme Draft Regulations Submission

### BACKGROUND

- 1.1 The *Accreditation Professionals Scheme Regulations* were released by the Department of Planning, Transport and Infrastructure (DPTI) for consultation in August 2018. The draft regulations were prepared following the *Accreditation Professionals Discussion Paper* that the Panel considered in May 2018.
- 1.2 Information in relation the draft regulations were provided to Panel Members by email in October 2018, together with commentary on how issues raised by the Panel on the discussion paper have been considered in the draft regulations.
- 1.3 A submission in relation to the draft regulations was made on behalf of the Panel and this report seeks to formalise this submission.

### REPORT

- 1.4 The draft regulations provide the framework for the scheme which includes:
  - 1.4.1 Classes of accreditation and associated decision making powers.
  - 1.4.2 How the scheme will work, including registration, professional development, auditing and complaints processes.
  - 1.4.3 Skills and experience requirements
  - 1.4.4 A code of conduct.

- 1.5 The proposed accreditation requirements for an Assessment Panel Member is prescribed as follows:

Level 2: Assessment Panel Member

*Qualification and experience - Relevant planning qualification as determined by the chief executive under regulation 5 of the Planning, Development and Infrastructure (Accredited Professionals) Regulations 2018 and a minimum 2 years full time or equivalent experience considered appropriate by the accrediting authority and covering at least 6 months experience in at least three of the technical skills applying to a Level 1: Assessment Manager*

or

*Qualification in a planning related field (e.g. architecture, engineering, environmental management, law, construction management) and membership of an allied industry body to the satisfaction of the CE together with 2 years full time or equivalent experience considered appropriate by the accrediting authority and covering at least 6 months experience in at least three of the following technical skills:*

- *Planning or planning related policy development, review and/or policy interpretation and advice.*
- *Communication, negotiation and/or mediation for planning or planning related outcomes with stakeholders.*
- *Designing and/or delivering community engagement of planning or planning related matters.*
- *Administration and/or leadership of urban and regional governance.*
- *Project management of planning or planning related matters.*

*Recognised equivalent scheme - persons may be eligible for this level of accreditation if they are a member of an equivalent scheme that requires members to hold a planning or planning related tertiary qualifications together with 2 years full time or equivalent experience, covering at least 6 months experience in at least three of the level 1 technical skills.*

- 1.6 The recognised planning qualifications are listed from page 29 draft regulations package.

- 1.7 In relation to ongoing training, the Regulations provide in schedule 1, clause 3:

*3—Planning (level 2)*

*(1) This clause applies in relation to an Accredited professional—planning level 2 (Assessment panel member).*

*(2) The amount of continuing professional development required to be obtained by an accredited professional to which this clause applies in the relevant CPD period is 10 CPD units.*

*(3) The 10 CPD units required under subclause (2) must include at least the following number of units in the following professional competencies:*

- (a) 1 unit in performance based planning or design;*
- (b) 1 unit in decision making in development assessment;*
- (c) 1 unit in governance;*
- (d) 1 unit in ethics in planning.*

- 1.8 The Council Assessment Panel considered the discussion paper in May 2018 that informed the draft regulations. The Panel suggested further consideration be given

to specific matters including relationship with industry schemes, training needs being relative to a person's experience, costs implications given moderate sitting fees of members and clarity that the Mutual Liability Scheme will cover professional indemnity insurance of members. A summary of the response in the draft regulations to the Panel's comments was provided to members by email 10 October 2018.

- 1.9 A submission to the draft regulations was made following comments from members. The submission on behalf of the Panel is provided in Attachment 1.
- 1.10 DPTI has advised that it expects scheme will be introduced at the end of 2018 and independent members of Panels will have to be accredited by mid-2019. This will be monitored and advice provided to members given the current term of the independent members on the Salisbury CAP that will conclude 31 May 2019.

## **2. CONCLUSION / PROPOSAL**

- 2.1 The report on the Accredited Professionals Scheme Draft Regulations be noted and the submission dated 22 October 2018 on behalf of the Assessment Panel be endorsed.

### **CO-ORDINATION**

Officer: GMCiD  
Date: 16.11.18





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22 October 2018

Department of Planning, Transport and Infrastructure

By email: DPTI.PlanningEngagement@sa.gov.au

Dear Sir / Madam

**Re: Submission on the Accredited Professionals Scheme Draft Regulations**

Thank you for the opportunity to make a submission on the *Accredited Professionals Scheme Draft Regulations*.

The Salisbury Council Assessment Panel Submission on the discussion paper and response in the draft regulations is summarised in the table below. The Panel is pleased that the draft regulations have addressed some of the comments that were raised but there appear to be some outstanding matters that are considered to require further consideration.

The Panel seeks the Department's further consideration of these matters and would appreciate understanding how the scheme will address these matters.

CAP comments	Response in draft regulations
Consideration should be given to simplifying this process, perhaps a single registration for both the relevant industry schemes and government scheme	Addressed, but with clarification. A person that is accredited with a recognised industry body can have their application submitted by that body for accreditation under this scheme. It appears however that fees will still be payable to DPTI, potentially requiring duplicate registration & renewal fees.
The accreditation scheme should consider that a person with less experience will inherently require more training as opposed to a person who has demonstrated core competencies through an extensive career	Not Addressed. The scheme does not appear to link the level of training required with a person's experience. The training for accredited professionals is based on the accumulation of prescribed CPD units for the relative level of accreditation.
The fees payable to the scheme should have regard to the moderate sitting fees for members on Assessment Panels.	Not Addressed. The Department has suggested the fee for all evaluations and registrations will be between \$600 - \$800, plus an annual (undisclosed) fee for administration costs. The evaluation fee is approximately 25% of this total annual payment for current Salisbury CAP members, excluding training requirements. (Ave 10 meeting per year

	@ \$320 per meeting). It is noted that Panel Members will not however be required to undergo the audit process.
Clarifying that Local Government Mutual Liability Scheme will cover the professional indemnity insurance for an Assessment Panel Member	Addressed. The Mutual Liability Scheme will cover all decisions made by accredited professionals employed by a council or appointed to an Assessment Panel within a council.
The accreditation scheme should recognise a person with an extensive level of experience and who may not necessarily have a prescribed planning qualification.	Not addressed. The scheme requires a person to hold a recognised qualification.
Furthermore, training should develop the capabilities of a person in making qualitative judgements and distinguishing the key planning issues from other considerations in their role as an Assessment Panel Member	Addressed. Training includes competencies in <i>decision making in development assessment</i> .

Yours sincerely

**Chris Zafiropoulos**  
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